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ASHTON-UNDER-LYNE · AUDENSHAW · DENTON · DROYLSDEN · DUKINFIELD · HYDE · LONGDENDALE · MOSSLEY · STALYBRIDGE

SPEAKERS PANEL (PLANNING)

Day: Wednesday
Date: 23 May 2018
Time: 10.00 am
Place: Lesser Hall - Dukinfield Town Hall

Item No.	AGENDA	Page No
1.	APOLOGIES FOR ABSENCE	
2.	DECLARATIONS OF INTEREST To receive any declarations of interest from Members of the Council.	
3.	MINUTES The Minutes of the meeting of the Speakers' Panel (Planning) held on 25 April 2018, having been circulated, to be signed by the Chair as a correct record.	1 - 6
4.	PLANNING APPLICATIONS To consider the schedule of applications.	
a)	17/00864/FUL - 35 STAMFORD ROAD, MOSSLEY	7 - 32
b)	18/00306/REM - EDWARD STREET, DENTON	33 - 54
c)	18/00307/REM - EDWARD STREET, DENTON	55 - 76
d)	17/00856/FUL - 61 GIBRALTAR LANE, DENTON	77 - 96
e)	17/00949/OUT - EGMONT STREET, MOSSLEY	97 - 120
f)	17/01033/FUL - 81 - 95 RIDGE HILL LANE, STALYBRIDGE	121 - 146
g)	17/01034/FUL - EDGE LANE, DROYLSDEN	147 - 174
h)	18/00035/REM - ST MARY'S ROAD AND TALBOT ROAD, HYDE	175 - 186
i)	17/01058/FUL - 10 - 12 CASTLE STREET, STALYBRIDGE	187 - 208
j)	17/01059/LBC - 10 - 12 CASTLE STREET, STALYBRIDGE	209 - 228
k)	17/01060/REM - 10 - 12 CASTLE STREET, STALYBRIDGE	229 - 262
l)	18/00102/FUL - LAND OFF WINTON AVENUE, AUDENSHAW	263 - 280
5.	URGENT ITEMS To consider any other items, which the Chair is of the opinion should be considered as a matter of urgency.	

From: Democratic Services Unit – any further information may be obtained from the reporting officer or from Carolyn Eaton, Senior Democratic Services Officer, 0161 342 3050 or carolyn.eaton@tameside.gov.uk to whom any apologies for absence should be notified.

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SPEAKERS PANEL (PLANNING)

25 April 2018

Commenced: 10.00am

Terminated: 10.45am

Present: **Councillor McNally (Chair)**
**Councillors P Fitzpatrick, Glover, S Quinn, Ricci, Sweeton,
Travis, Ward, Wild and Dickinson**

Apologies for absence: **Councillors Kinsey and D Lane**

38. MINUTES

The Minutes of the proceedings of the meeting held on 21 March 2018 having been circulated, were taken as read and signed by the Chair as a correct record.

39. DECLARATIONS OF INTEREST

There were no declarations of interest submitted by Members.

40. OBJECTIONS TO PROPOSED CONTROLLED PARKING SCHEME WATERLOO GARDENS, ASHTON-UNDER-LYNE

Consideration was given to a report of the Director of Operations and Neighbourhoods, which explained that in 2013, a Controlled Parking Scheme was introduced on Hurst Bank Road, Crompton Street, Blenheim Way, Wilson Crescent and Midgely Crescent off Mossley Road, Ashton-under-Lyne. It was always made clear that if residents of Waterloo Gardens suffered as a consequence of these restrictions a further scheme would be promoted.

Further development in the area including a new housing estate and school, had exacerbated the current on street parking situation and a petition, signed by the majority of the residents to extend the existing Controlled Parking Scheme was received by the Traffic Operations department.

A consultation with Ward Councillors and residents had taken place in February 2017 and statutory consultation commenced in June 2017 for 28 days. One objection was received.

The objection was made by a resident of the local area. The resident argued that the proposed extension to the existing Controlled Parking Area was necessary and that if the scheme were to be implemented, non permit holders would be forced to park further down Waterloo Gardens. The objector had suggested that waiting restrictions at the junction of Blenheim Way and Waterloo Gardens would be a preferable measure.

The response of the Director of Operations and Neighbourhoods was detailed and it was explained that a questionnaire was sent to the residents who would be affected by the proposals and 69% of the residents were in favour of the scheme extending into Waterloo Gardens. The introduction of waiting restrictions would not be beneficial to the residents as obstructive parking had not been deemed a problem in the vicinity.

The objector was not present at the meeting.

Discussion ensued with regard to the above and consideration was given to the information provided, including the objection raised and response given and it was:

RESOLVED

That authority be given for the necessary action to be taken in accordance with the Road Traffic Regulation Act 1984 to make the following order: **THE TAMESIDE METROPOLITAN BOROUGH (HURST BANK ROAD AREA, ASHTON-UNDER-LYNE (CONTROLLED PARKING AREA) ORDER 2017** as follows:

Introduce a Controlled Parking Zone on Waterloo Gardens, both sides, from its junction with Blenheim Way in a northerly direction for a distance of 11m (the northerly gable end of property number 34 Waterloo Gardens).

41. APPEAL DECISION NOTICES

Application reference/Address of Property.	Description	Appeal Decision
Appeal APP/G4240/D/17/3192055 26 Low Wood Road, Denton, Manchester. M34 2PD	Ref: Proposed dormer to front, rear and side.	Appeal dismissed.
Appeal APP/G4240/D/18/3195689 15 Broadfield Close, Denton, Manchester. M34 6BN	Ref: Proposed front dormer extension.	Appeal dismissed.

42. PLANNING APPLICATIONS

The Panel gave consideration to the schedule of applications submitted and it was:-

RESOLVED

That the applications for planning permission be determined as detailed below:-

Name and Application No:	17/00984/REM BDW Trading Ltd (Barratt Manchester)
Proposed Development:	Application for the approval of reserved matters (means of access, landscaping, layout, scale and appearance) relating to the residential phase of development approved under outline planning permission 16/00962/OUT. Site off Hattersley Road West (North of the junction with Sandy Bank Avenue), Hattersley.
Speaker(s)/Late Representations:	Peter Walker spoke in objection to the application.
Decision:	Approve subject to conditions as set out in the report.

Name and Application No	16/00403/OUT c/o Emery Planning Partnership Ltd												
Proposed Development:	Outline planning application for erection of 25 dwellings with associated works (access to be considered with all other matters reserved). Land formerly Prospect House, Stockport Road, Mossley, Tameside.												
Speaker(s)/Late Representations:	Gareth Salthouse of Emery Planning, spoke in support of the application.												
Additional Comments/Information	<p>The Development Manager, Planning, circulated the following information in respect of 106 contributions.</p> <p>It was explained that, within paragraph 18.2 figures had been quoted incorrectly, with the full 106 contributions quoted as per unit amounts. Paragraph 18.2 should read:</p> <p>In this case, the obligation generator suggests the following amounts per unit:</p> <table border="1" data-bbox="564 887 1439 1155"> <thead> <tr> <th></th> <th>Green Space</th> <th>Education</th> <th>Highways</th> </tr> </thead> <tbody> <tr> <td>Amount per each 2 bed unit</td> <td>£471.52</td> <td>£821.79</td> <td>£584.63</td> </tr> <tr> <td>Amount per each 3 bed unit</td> <td>£707.28</td> <td>£1232.68</td> <td>£876.94</td> </tr> </tbody> </table> <p>It is intended that the Education Contribution be used towards a scheme for increasing admission numbers per year group from 156 to 180 at Mossley Hollins High School. This will be phased over 5 years from September 2018 to remodel classrooms. The suggested highways contribution is to be used towards installing a controlled pedestrian crossing on A670 to improve pedestrian access to both St George's and Livingstone Primary Schools.</p> <p>The Development Manager, Planning, further confirmed that the Green Space contribution would be utilised for enhancements to the infrastructure at Roaches including footpaths.</p>		Green Space	Education	Highways	Amount per each 2 bed unit	£471.52	£821.79	£584.63	Amount per each 3 bed unit	£707.28	£1232.68	£876.94
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Amount per each 3 bed unit	£707.28	£1232.68	£876.94										
Decision:	<p>Approve subject to:</p> <p>The completion of a Section 106 Agreement to secure the above contributions; and</p> <p>the conditions as set out in the report, with the following amended Condition 16:</p> <p>Prior to the commencement of any development a scheme shall first be submitted to an approved in writing by the Local Planning Authority providing details of further ground investigations including as necessary further borehole investigations, soil testing and a watching brief to enable a slope stability assessment to provide evidence to show how the risk of a landslip will be addressed. The scheme shall</p>												

	include details of any retaining structures and foundations and a specific methodology for the structures. The development shall then be carried out, maintained and managed in accordance with the submitted details.
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Name and Application No:	17/00784/FUL Mr Bardsley
Proposed Development:	Conversion of the vacant public house and associated apartment to 5 dwellings and formation of a new car park. The White Hart Inn, 91 \market Street, Mottram
Speaker(s)/Late Representations:	None.
Decision:	Approve subject to the conditions as set out in the report.

Name and Application No:	17/00982/REM BDW Trading Ltd (Barratt Manchester)
Proposed Development:	Application for the approval of reserved matters (means of access, landscaping, layout, scale and appearance) relating to the residential phase of development approved under outline planning permission 16/00959/OUT. Site north of Hattersley Road West (east of Fields Farm Road), Hattersley
Speaker(s)/Late Representations:	None.
Decision:	Approve subject to the conditions as set out in the report.

Name and Application No:	17/00983/REM BDW Trading Ltd (Barratt Manchester)
Proposed Development:	Application for the approval of reserved matters (means of access, landscaping, layout, scale and appearance) relating to the residential phase of development approved under outline planning permission 16/00960/OUT. Site off Hattersley Road West (bound by Hattersley Road West to the north west and Sandy Bank Avenue to the south and west), Hattersley.
Speaker(s)/Late Representations:	None.
Decision:	Approve subject to conditions as set out in the report.

Name and Application No:	17/00985/REM BDW Trading Ltd (Barratt Manchester)
Proposed Development:	Application for the approval of reserved matters (means of access, landscaping, layout, scale and appearance) relating to the residential phase of development approved under outline planning permission 16/00963/OUT. Site off Hattersley Road West (bound by Hattersley Road West to the north west and Sandy Bank Avenue to the south and west), Hattersley.
Speaker(s)/Late Representations:	None.
Decision:	Approve subject to conditions as set out in the report.

43. URGENT ITEMS

The Chair advised that there were no urgent items of business for consideration by the Panel.

CHAIR

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Application Number	17/00864/FUL
Proposal	Change of use from dwelling to 9 bed HMO (Sui generis), including minor elevation change.
Site	35 Stamford Road, Mossley
Applicant	Mr A Rothwell
Recommendation	Grant planning permission subject to conditions
Reason for report	A Speakers Panel decision is required because, in accordance with the Council's Constitution a member of the council has requested the opportunity to address the Panel before a decision is made. Accordingly, the applicant, or their agent, has been given the opportunity to speak also.

REPORT

1. SITE AND SURROUNDINGS

- 1.1 The application site comprises of a large red brick terraced property benefitting from a raised basement, ground and first floor and habitable space in the roof. The property is currently empty and was last used as a single family dwelling. The property is situated in a residential area with residential properties adjoining the site on both sides fronting onto Stamford Street.
- 1.2 Stamford Street rises steeply in a northerly direction from its junction with Manchester Road and the land also falls steeply from Hanover Street to the rear of the application site down to Stamford Road. The property is in very close proximity to Mossley railway station to the south and Mossley town centre to the north-west.

2. THE PROPOSAL

- 2.1 The application seeks change of use of the property to a 9 bedroom House in Multiple Occupation (HMO). Four of the bedrooms will have en-suite facilities and 5 will have access to two shared bathrooms. A kitchen/living/ dining space on the lower ground floor and a separate lounge room on the first floor will be shared by all the residents. There is an internal storage room which will be used for bin storage.
- 2.2 The only external changes proposed that require planning permission is the provision of a rear dormer window to serve a bathroom in the roof-space.

3. RELEVANT PLANNING HISTORY

- 3.1 No relevant Planning History

4. RELEVANT PLANNING POLICIES

- 4.1 Tameside Unitary Development Plan (UDP) Allocation: Unallocated

Part 1 Policies

- 1.3: Creating a Cleaner and Greener Environment

- 1.4: Providing More Choice and Quality Homes.
- 1.5: Following the Principles of Sustainable Development
- 1.12: Ensuring an Accessible, Safe and Healthy Environment

Part 2 Policies

- H7: Mixed Use and Density.
- H10: Detailed Design of Housing Developments.
- T1: Highway Improvement and Traffic Management.
- C1: Townscape and Urban Form
- MW11: Contaminated Land.

National Planning Policy Framework (NPPF)

- Achieving Sustainable Development;
- Section 1 Delivering sustainable development
- Section 6 Delivering a wide choice of high quality homes
- Section 7 Requiring good design
- Section 8 Promoting healthy communities

Planning Practice Guidance (PPG)

This is intended to complement the NPPF and to provide a single resource for planning guidance, whilst rationalising and streamlining the material. Almost all previous planning Circulars and advice notes have been cancelled. Specific reference will be made to the PPG or other national advice in the Analysis section of the report, where appropriate.

Other Policies

- Residential Design Supplementary Planning Document
- Technical Housing Standards - Nationally Described Space Standard.

5. PUBLICITY CARRIED OUT

- 5.1 The application has been advertised by means of neighbour notification letters dispatched to 28 properties on 13 November 2017

6. RESPONSES FROM CONSULTEES

- 6.1 Head of Environmental Services – Highways – It is not considered that the development will have any significant or severe impact on highway safety or operation, such as would warrant refusal.
- 6.2 Head of Environmental Services – Environmental Health, No objections subject to conditions regarding hours of work and refuse/recycling storage.
- 6.3 Greater Manchester Ecology Unit – No objection but suggests a note for applicant reminding them of their obligations under the Wildlife and Countryside Act.
- 6.4 Mossley Town Council - recommends refusal for the following reasons:
 - The proposal is an over-intensive use of the property, which due to its age and position is not suitable for conversion on the scale proposed.
 - The development will have a significant detrimental effect on the residential amenity by reason of noise, general disturbance, waste and intensified use of the residential accommodation There is no off-road parking so the development will negatively impact on traffic and pedestrian safety on the already busy and dangerous Stamford Road.

- The character of the neighbourhood comprises family homes. The level of activity resulting from a group of 10 unconnected people is likely to result in more frequent comings and goings and differing patterns of behaviour.
- Number 35 Stamford Road has passing over rights to the adjoining terraced houses. Residents are concerned that 10 unrelated people and their visitors will use their private back yards as a thoroughfare resulting in serious detriment to amenity and loss of privacy.
- The proposed additional bathrooms will place an unacceptable burden on the existing infrastructure including water pressure and sewer and drainage systems.
- The proposed development will result in a lack of space and accessibility for potential occupants.
- The residential accommodation proposed does not match local housing need. The proposed HMO is not needed, would be an over-intensive development for the area, and there is a continuing need for family homes in this part of Mossley.

6.5 Councillors - A representation offering objection to the proposals has been received from Councillor Sharif on behalf of the residents of Mossley and requested for a determination at Speakers Panel. Concerns raised are size of the development from a home to a 9 bedroom HMO and the affect it will have on neighbours, parking, traffic, no. of bins, noise, etc.

7. SUMMARY OF THIRD PARTY REPRESENTATIONS

7.1 22 letters of objection have been received from neighbouring properties raising the following matters:

- Cause additional pressure on the road in terms of parking
- Lack of parking leads to congestion
- No garage or cycle provision
- Application doesn't comply with Policy in terms of parking standards
- A HMO is counter to the very nature of this residential, family area and be detrimental to the character of the area
- Accommodation for ten persons appears excessive and disproportionate to the house in question
- Additional noise levels that may accompany multiple separate occupants within the same building.
- Concerns about the additional footfall, disturbance, potential intrusion and lack of security across the easement at the back of our terrace owing to increased use of our dual shared access routes through private garden areas,
- No refuse storage capacity.
- The dormer windows proposed will serve only to maximize internal space and will detract from the character of a Victorian terraced property. Loss of traditional character and charm.
- There are already plenty of apartments on Stamford Road including those currently being built, there is no evidence that this property type is needed.
- Noise and disturbance during construction.
- The Development not compliant with The National Space Standards; Single Bedroom - min.2.15m wide and concern that the bedrooms will be double occupancy and therefore contravene "The National Space Standards".
- The financial costs and loss of value
- Human Rights Act, Protocol 1, Article 1, which states that a person has the right to peacefully enjoy all their possessions, which includes the home. Article 8 of the Human Rights Act also states that a person has the substantive right to respect for their private and family life, home and correspondence.
- Increased risk of crime/vandalism/anti social behaviour,

- Bats are often seen flying into and out of the roof space at number 35.
- Fire risk increased by the amount of occupants that could be in the only kitchen space cooking at any one time. Residents smoking in the rooms etc increases the fire risk.
- Additional impact on the drainage, sewage
- Additional impact on local services such as NHS and refuse
- Reduction in local living standards
- Precedent set by refusal of 37 Manchester Road Audenshaw (ref 15/00829/FUL)

8. ANALYSIS

- 8.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 and Section 70(2) of the Town and Country Planning Act 1990 require that applications for planning permission are determined in accordance with the development plan unless material planning considerations indicate otherwise. Material planning considerations include the NPPF.
- 8.2 The site is unallocated on the Proposals Map associated with the Unitary Development Plan for Tameside (2004). Therefore, the main issues to consider are:
1. The principle of the proposed development
 2. Intensity of use
 3. Character of the area
 4. Residential amenity; and,
 5. Parking and highway safety.

9. PRINCIPLE

- 9.1 The application seeks planning permission for the conversion of a residential property which is located within a predominantly residential area.
- 9.2 The NPPF has a presumption in favour of sustainable development and confirms that planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise.
- 9.3 The proposed development would make a positive contribution to the Council's Housing Requirements and provides a sustainable reuse of a vacant building. Furthermore, the site is in a sustainable location adjoining existing residential properties in close proximity to the town centre, bus routes and the railway station. On balance, it is considered that the principle of development is acceptable.

10. INTENSITY OF USE

- 10.1 The application property is a large Victorian Terrace which originally would have provided generous size accommodation and suited a large family. The application was initially submitted with 10 single bedrooms and a communal kitchen/ dining/ living room. This raised concern regarding the potential for a poor standard of living with the only communal living space being located at basement level. During the course of the application negotiation have resulted in the application being amended to overcome this objection and the is now for 9 single bedrooms and includes a communal kitchen/ dining/ living room and separate lounge area. It is therefore considered capable of accommodating the layout and accommodation as shown on the submitted plans and providing an acceptable standard of living.
- 10.2 The proposed room sizes are shown on the submitted plans as single bedrooms comply with the guidance laid out in the 'Technical Housing Standards - Nationally Described

Space Standard' and as a result this part of the proposal is acceptable and there is no evidence to suggest that the living accommodation is sub-standard or of an unsatisfactory quality.

- 10.3 It is acknowledged that within the comments received reference has been drawn to a previous development for an 8 bedroom House of Multiple Occupation in a different part of the borough that was refused due to over intensive use of the property and that this should be refused due to precedent. The two applications are different and should be considered on their own planning merits and the provision of a precedent is not something that can be used as a material planning consideration. The application property for this current scheme is larger in character and set over more levels. The surrounding area is less built up with only one side of Stamford road being developed at that location and with the alterations made to the scheme to reduce one bedroom and use the space as additional living accommodation the proposal is considered to be acceptable in this location.

11. CHARACTER OF THE AREA

- 11.1 The area is predominantly residential and although the tenure is proposed to be changed the general theme will be for residential use albeit within a different use class and the proposal will be the only authorised HMO in this part of the Borough. It is not considered there would be any significant impact on the character of the area resulting from just one HMO. If more HMO proposals follow in the area than the Local Planning Authority would need to consider those against a cumulative impact.
- 11.2 The development would not be contrary with UDP policy C1 as the only external change is minimal located at roof level on the rear of the property and does not detract from the character of a Victorian terraced property.

12. RESIDENTIAL AMENITY

- 12.1 UDP Policy H10 requires new development to be of high quality, provide a good standard of amenity for future occupiers, and for there to be no unacceptable impact on the amenity of neighbouring properties.
- 12.2 In terms of objections raised with regard to noise and disturbance as a result of the number of potential residents, the proposed use is residential and the property has been shown to be sufficient size to provide for acceptable living accommodation for the proposed number of residents. It is a substantial property that could house a large family and it is not therefore considered that the level of noise and disturbance associated with the proposed use would be unacceptable or above the level that could be associated with its current permitted use.
- 12.3 With regard to access over the rear easement this is a private matter for agreement between the property owners. There is no reason to suggest that residents from a HMO would require any more or frequent access than would reasonable be required for the existing dwelling or that the nature of access would cause unreasonable disturbance or reduction in security.
- 12.4 The proposed plans for the building show existing window and door openings to be re-used, the only external change being the addition of a dormer window to facilitate a bathroom within the roof space. The proposed external alterations raise no issues with regard to residential amenity.
- 12.5 Following the above assessment, it is considered that the proposed development would not result in an adverse impact on the residential amenity of any of the neighbouring properties.

13. PARKING AND HIGHWAY SAFETY

- 13.1 The application site fronts Stamford Road where local residents park their cars on the highway or in an extended layby located across Stamford Road. This road is a bus route and also the main access from that direction towards Mossley town centre.
- 13.2 The application proposes no off street car parking facilities. Any demand generated from the proposed use for car parking which would have to be accommodated on street along with the other properties on that stretch of Stamford Road. There are currently no parking restrictions in the vicinity of the application property and during the day this road appears to be relatively quiet and capable of providing adequate on street car parking. It is acknowledged that traffic levels would be greater in the morning and evening peak hours and demand for parking would be greater at the end of the working day.
- 13.3 The site is in a highly sustainable location on a bus route and in close proximity to Mossley Railway Station and Town Centre. There have been no objections raised to the proposal from the Highways Engineer in terms of the impact of the development on highway and pedestrian safety.
- 13.4 Given the highly sustainable location the expected levels of car ownership for occupiers of the scheme would be lower than in other areas. It is considered unlikely that the demand for any car parking generated by the proposed use would result in such severe harm being caused to amenity of existing residents in terms of increased demand for spaces to justifying a refusal of planning permission on this basis. It must also be acknowledged that the occupation of the building for its current lawful use would result in demand for parking.
- 13.5 It is acknowledged that there is currently no cycle storage provision shown on the submitted plans, though as in many domestic properties this could be provided within the rear garden area and as such a condition is recommended requiring this to be provided prior to the occupation of the building and permanently maintained.

14. OTHER MATTERS

- 14.1 There is sufficient space within the curtilage of the site to provide the necessary level of refuse facilities for the building with any further provision to be provided by a private waste contract.
- 14.2 Concern has been raised by objectors that bats are often seen flying into and out of the roof space at the application site. The Greater Manchester Ecology Unit have been consulted and have raised no objection. However they suggest that the applicants should be made aware of their requirements under the Wildlife and Countryside act to protect them should any evidence be found during construction work and that this would be included on the decision notice in the event that the application is approved.
- 14.3 Comments have been received from neighbours regarding the potential for disruption at construction stage whilst the development is undertaken. Unfortunately, some degree of disruption is an inevitable consequence of development and this cannot be used as a reason to resist planning proposals. However a condition will be attached restricting the hours in which construction works can be carried out as recommended by Environmental Services.
- 14.4 Objections regarding the loss of value to existing properties, impact on the drainage and sewage of the additional bathrooms, concern about residents smoking and the character of the residents, increased levels of are not material considerations to be considered when assessing a planning application.

15. CONCLUSION

- 15.1 At the heart of the NPPF is a presumption in favour of sustainable development, this requires planning applications that accord with the development plan to be approved without delay unless the adverse impacts of doing so would significantly and demonstrably outweigh the benefits when assessed against the policies in the framework as a whole or specific policies in the framework indicate that development should be restricted.
- 15.2 In this instance, the application site is currently in residential use and this application is for a different residential use requiring a change of use class. The impacts of this have been shown to not cause substantial harm to the amenity, character or highway safety of the surrounding occupiers and area. The development would bring forward housing provision and associated economic and social benefits and redevelop a vacant neglected building bringing it back into active use. It is considered that the benefits of the scheme outweigh any the identified harm and so the proposal is therefore recommended for approval subject to the conditions set out below.

16. RECOMMENDATION

Grant planning permission subject to the following conditions

1. The development must be begun not later than the expiration of three years beginning with the date of this permission.
2. The materials to be used in the construction of any external work shall match as closely as possible the corresponding materials in the existing building.
3. During demolition / construction / conversion no work (including vehicle and plant movements, deliveries, loading and unloading) shall take place outside the hours of 07:30 and 18:00 Mondays to Fridays and 08:00 to 13:00 Saturdays. No work shall take place on Sundays and Bank Holidays.
4. Notwithstanding the plans hereby approved and prior to the first occupation of any of the residential units a scheme shall be submitted and approved in writing by the Local Planning Authority for the storage of and collection of waste and recyclable materials. The measures within the approved scheme shall be implemented in full and be retained at all times thereafter.
5. Notwithstanding the plans hereby approved and prior to the first occupation of any of the residential units a scheme shall be submitted and approved in writing by the Local Planning Authority for the storage of cycles. The measures within the approved scheme shall be implemented in full and cycle parking be retained and kept available for the parking of cycles at all times thereafter.
6. The development shall be completed in accordance with site location plan and drawing numbers (90)A002, (9)A002 and (9)A001 received on 10 October 2017, (20)A002 rev A received on 24 October 2017 and (20)A001 rev B received on 16 March 2018.

Informatives

1. REASON FOR GRANTING PLANNING PERMISSION, Statement under Article 35 (2) of the Town and Country Planning (Development Management Procedure) England Order 2015 (as amended): The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application

process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the NPPF.

2. CONTAMINATED LAND ADVICE, The responsibility to properly address contaminated land issues, including safe development and secure occupancy, irrespective of any involvement by this Authority, lies with the owner/developer of the site. Should any evidence of contamination be encountered during the development of the site the applicant/developer should contact the Council's Environmental Protection Unit (Tel: 0161 342 2691) as soon as is practicable
3. NOTE FOR APPLICANT, The applicant's attention is drawn to the Wildlife and Countryside Act 1981 which makes provision for the protection of bats and their habitat. As the proposed development might affect such a habitat consideration should be given to establishing whether or not bats are using the premises/site for roosting purposes, prior to any development taking place. More information can be obtained from the Bat Conservation Trust Helpline (Tel: 0845 1300 228) acting on behalf of Natural England.

Application Number: 17/00864/FUL – 35 Stamford Road

Photo 1

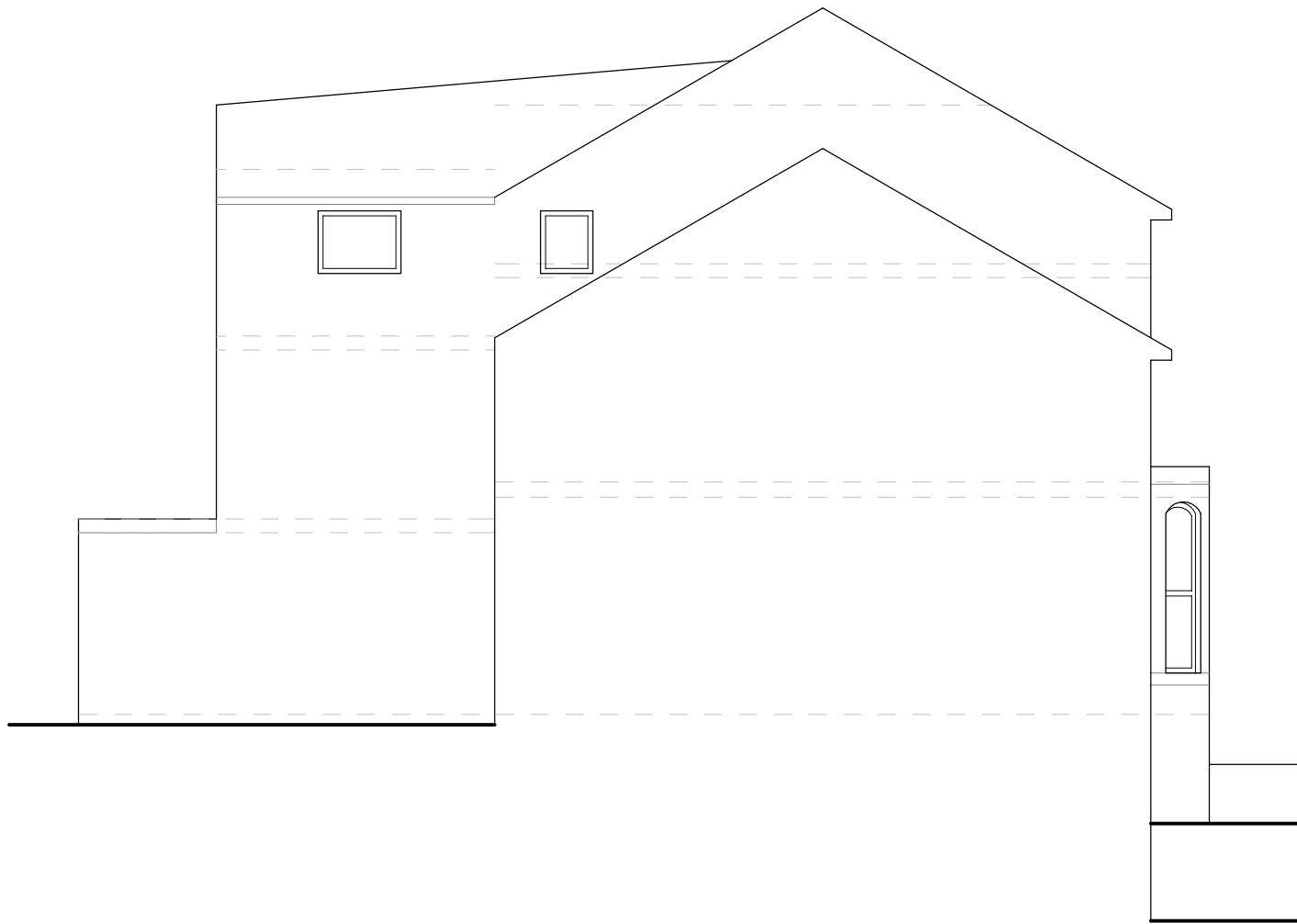
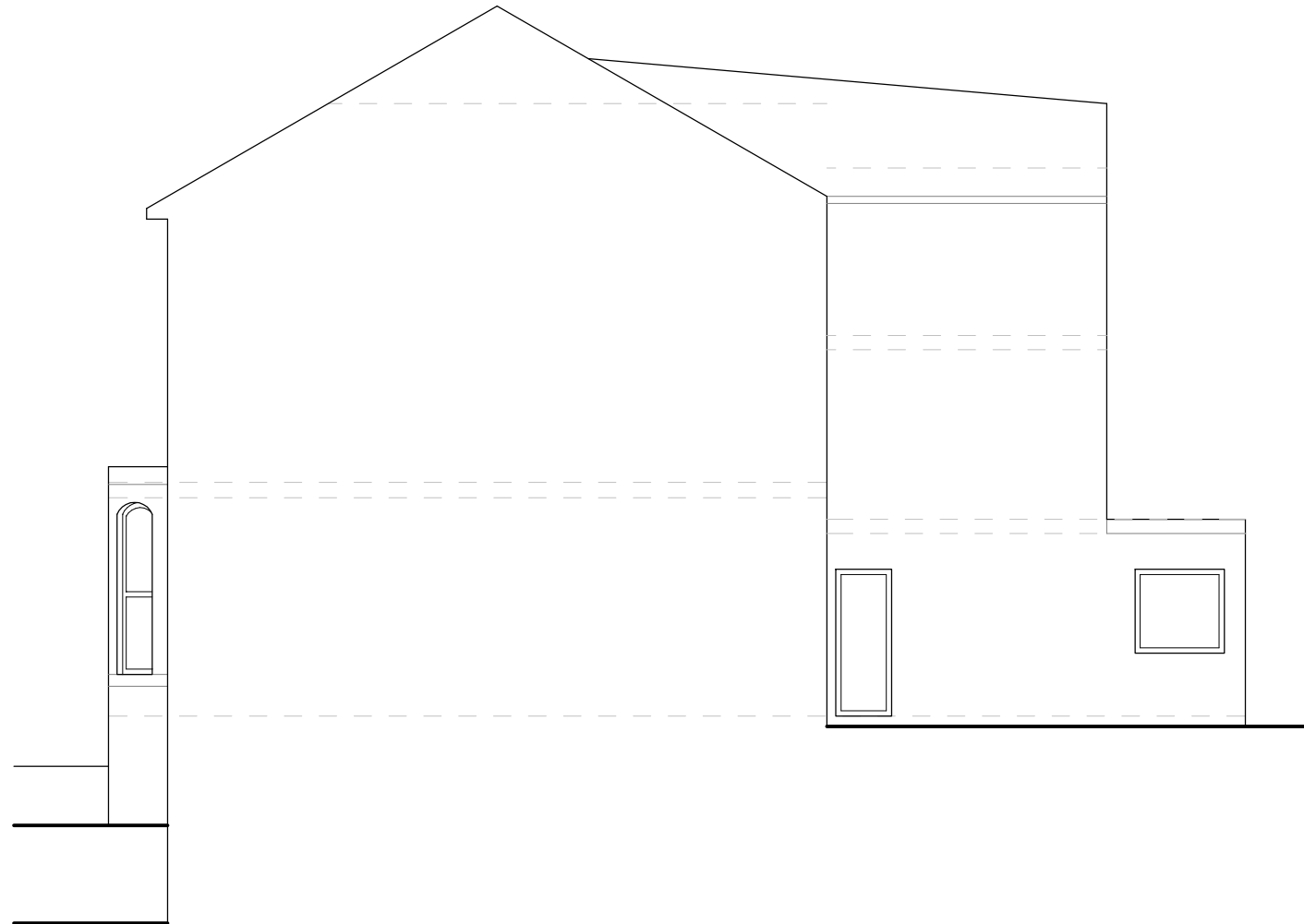


Photo 2



Photo 3





GRANT ERSKINE ARCHITECTS SHALL HAVE NO RESPONSIBILITY FOR ANY USE MADE OF THIS DOCUMENT OTHER THAN FOR THAT WHICH IT WAS PREPARED AND ISSUED.
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 DO NOT SCALE FROM THIS DRAWING.
 ANY DRAWING ERRORS OR DIVERGENCES SHOULD BE BROUGHT TO THE ATTENTION OF GRANT ERSKINE ARCHITECTS AT THE ADDRESS SHOWN BELOW.

NOTES

All work to be carried out in strict accordance with current Codes of Practice, Building Regulations and to the complete satisfaction of the Local Authority. All dimensions to be checked on site prior to work commencing by contractor. All timbers to be treated with wood preservatives before use.

REVISION / DESCRIPTION	DRAWN	CHECKED	DATE
A - Removed dormer	MDS	GPE	24/10/17

GRANT ERSKINE ARCHITECTS
 Suite 5, 3rd Floor
 61 Mosley Street
 Manchester
 M2 3HZ

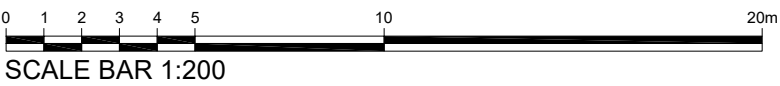
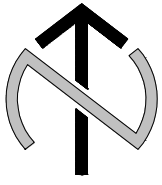
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PROJECT TITLE 35 STAMFORD ROAD, ASHTON SINGLE DWELLING CONVERSION TO HMO	
PROJECT NUMBER 20170926	
DRAWING TITLE PROPOSED ELEVATIONS	SCALE 1:100 @A3
	DATE 04/10/2017
DRAWING No. (20)A002	REVISION A

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PROJECT NUMBER 20170926			
DRAWING TITLE SITE BLOCK PLAN			SCALE 1:200 @ A3
			DATE 09/10/2017
			REVISION -

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REVISION / DESCRIPTION	DRAWN	CHECKED	DATE	PROJECT TITLE
A - Removed bedroom	MDS	GPE	15/03/18	35 STAMFORD ROAD, AHSTON SINGLE DWELLING CONVERSION TO HMO
B - Changed beds	MDS	GPE	16/03/18	
PROJECT NUMBER				SCALE
260917				
DRAWING TITLE				DATE
PROPOSED FLOOR PLANS				28 SEP 17
DRAWING NO				REVISION
(20)A001				B

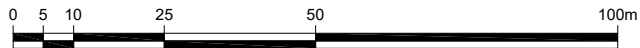
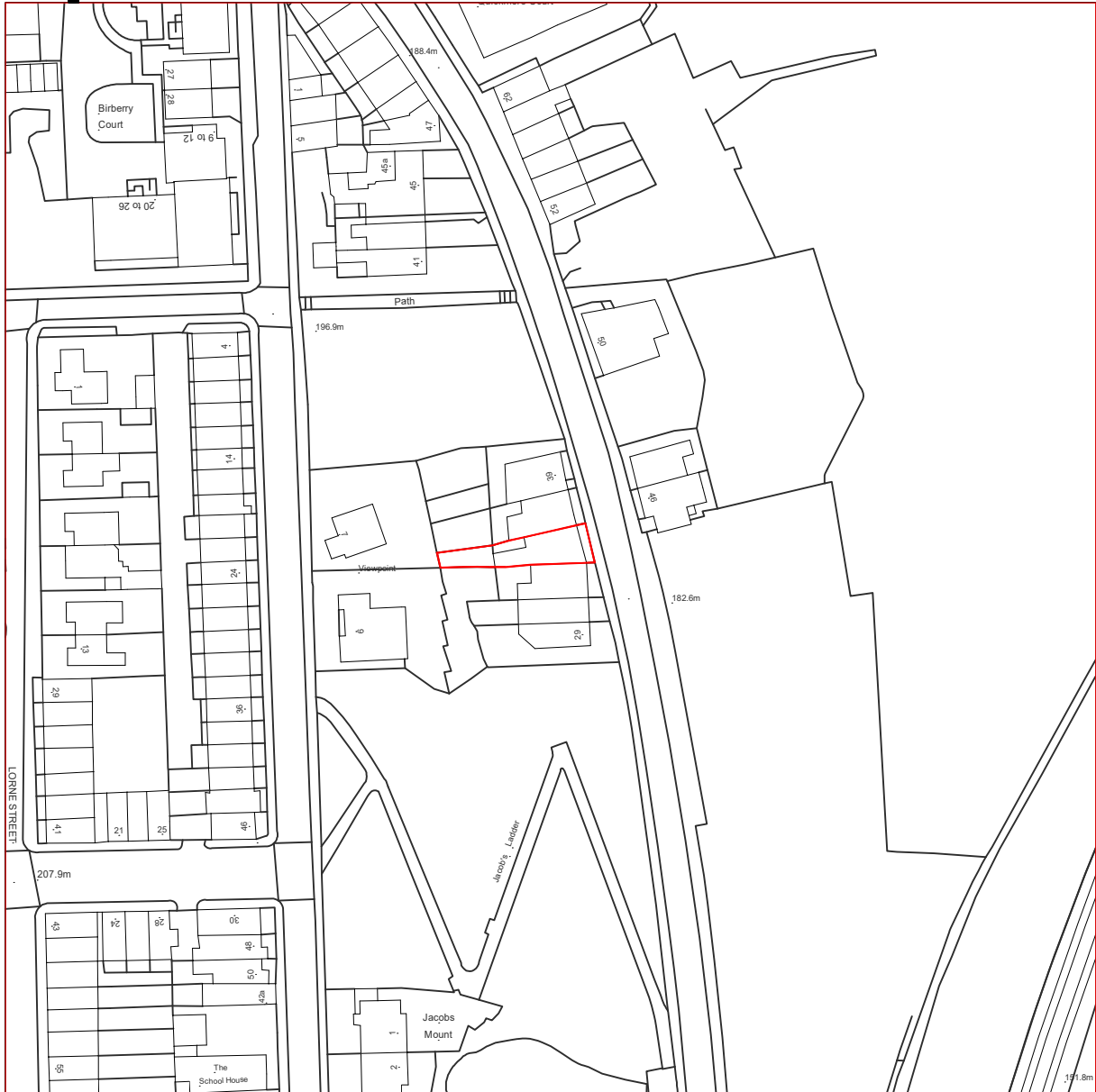
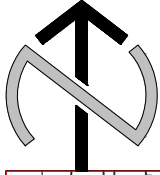
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				PROJECT NUMBER 20170917	
				DRAWING TITLE LOCATION PLAN	SCALE 1:1250 @A4
					DATE 09/10/2017
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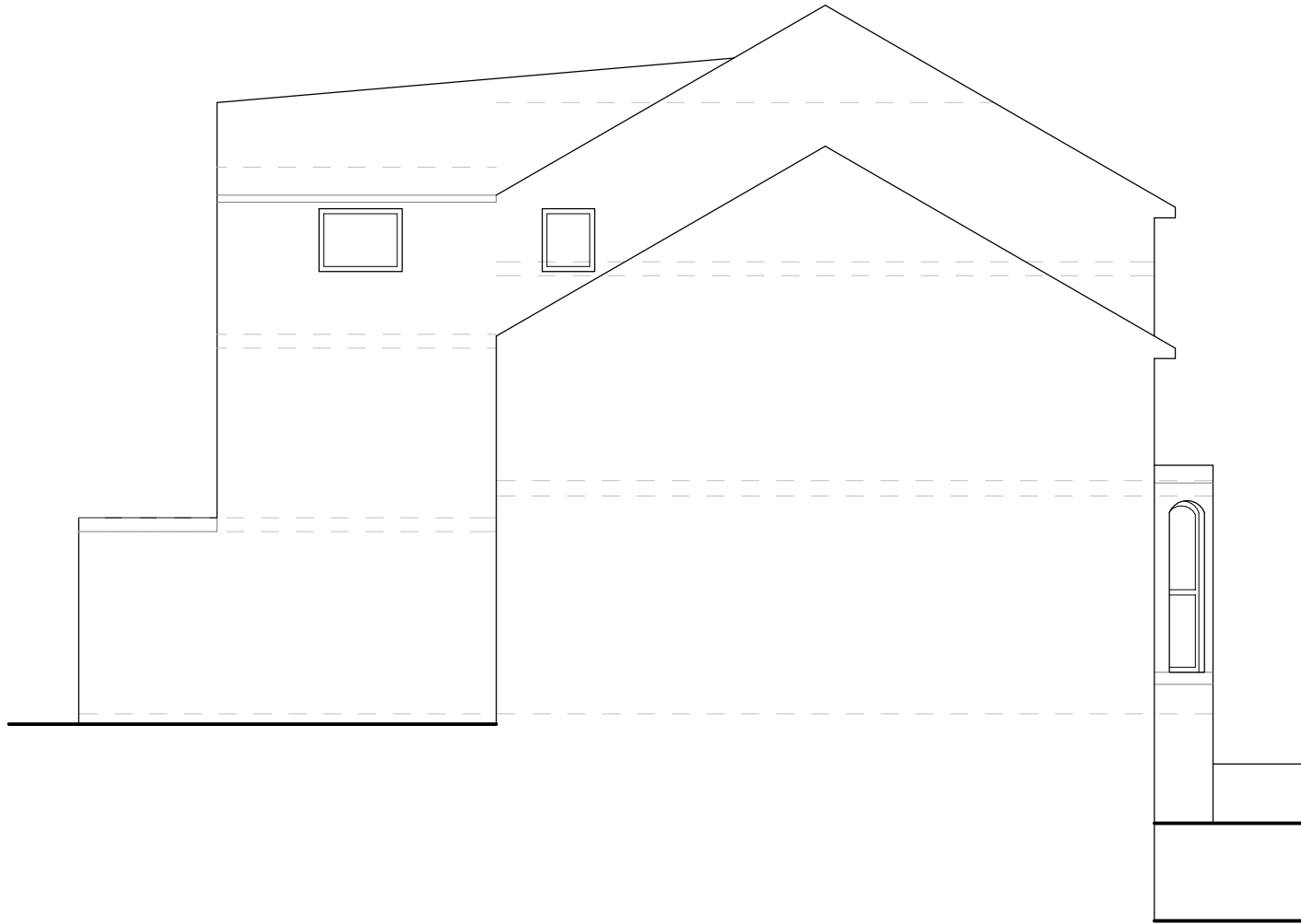
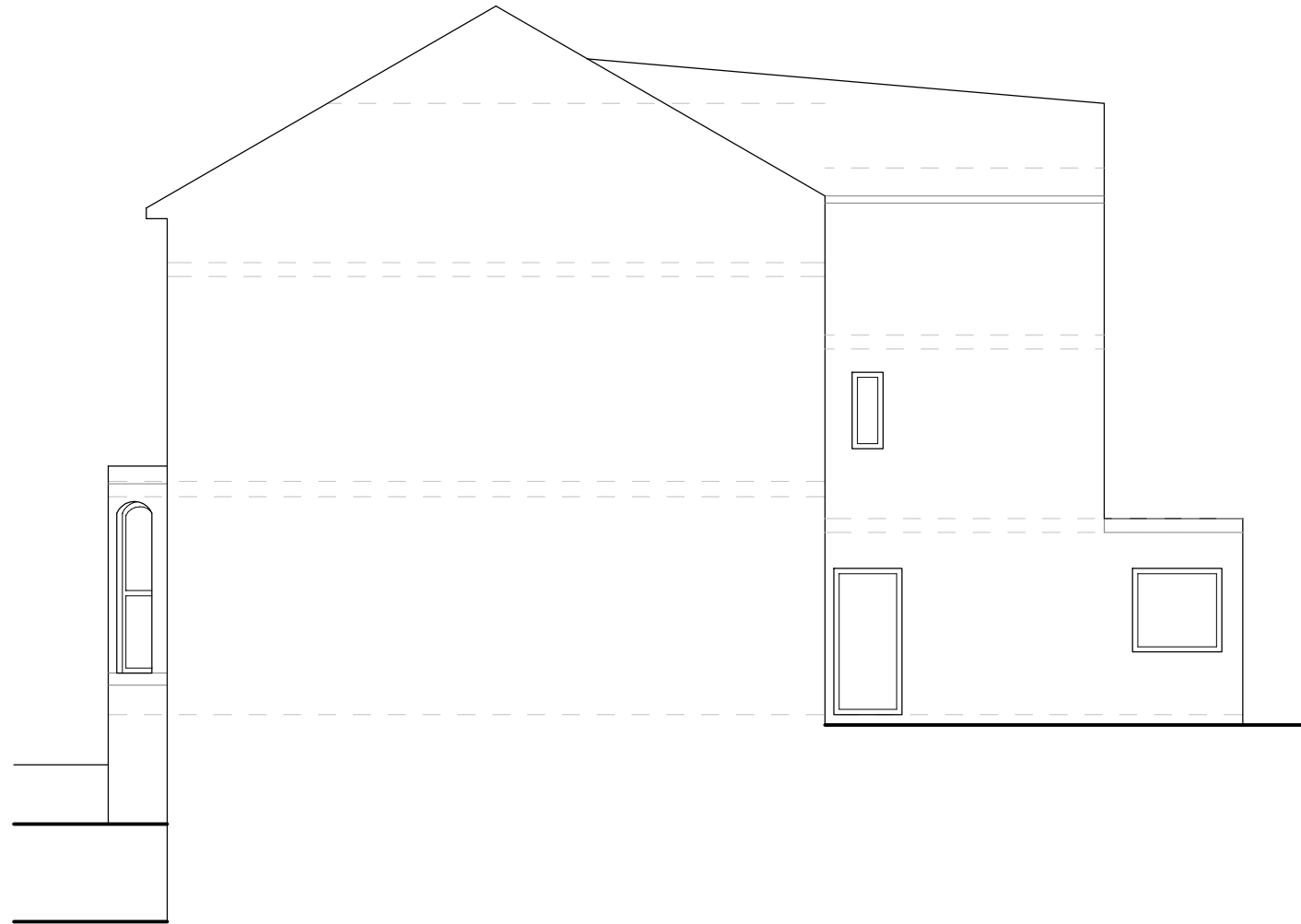
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NOTES

All work to be carried out in strict accordance with current Codes of Practice, Building Regulations and to the complete satisfaction of the Local Authority. All dimensions to be checked on site prior to work commencing by contractor. All timbers to be treated with wood preservatives before use.

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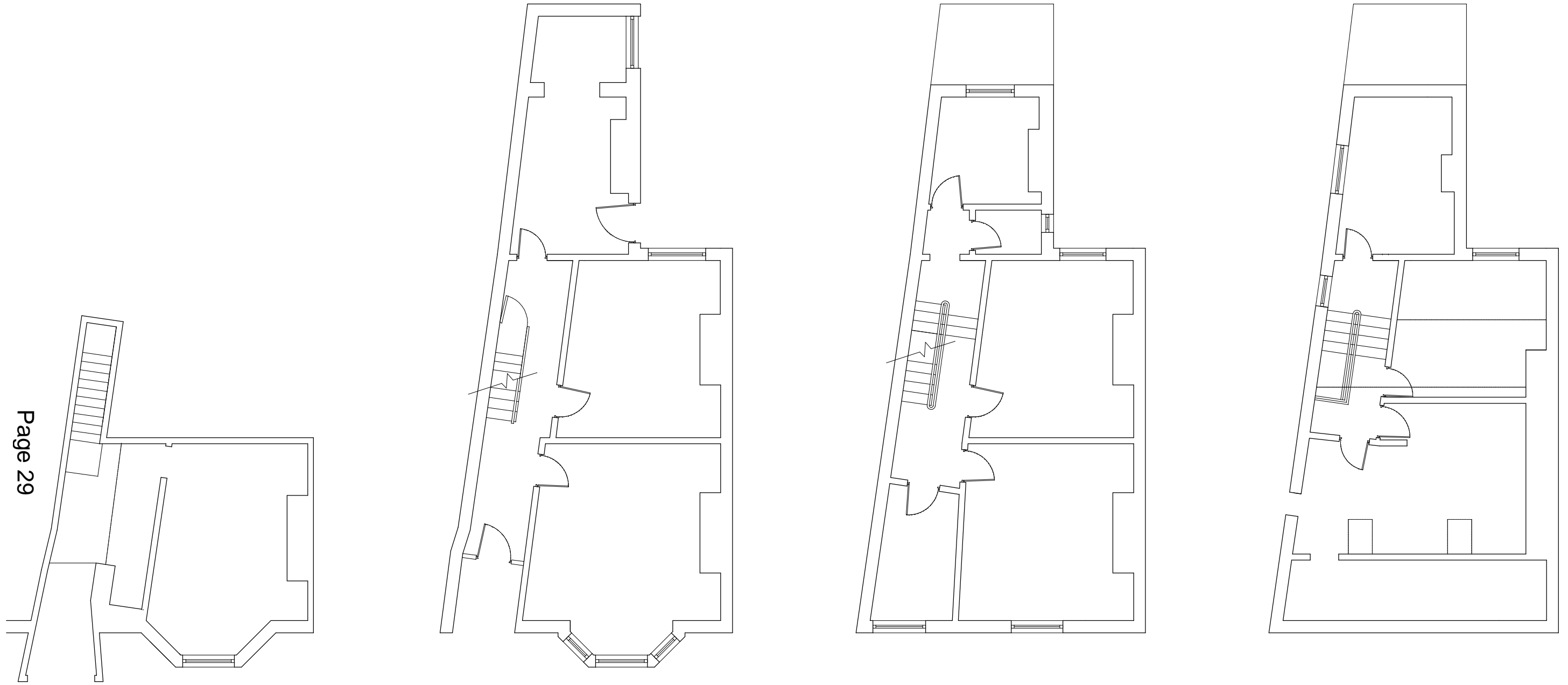
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PROJECT NUMBER 20170926	
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PROJECT TITLE
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 SINGLE DWELLING CONVERSION TO HMO**

PROJECT NUMBER
20170926

DRAWING TITLE EXISTING FLOOR PLANS	SCALE 1/100@A3
	DATE 28 SEP 17

DRAWING No. (-9)A001	REVISION
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HANO

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Application Number 18/00306/REM

Proposal	Reserved matters application for appearance, landscaping, layout and scale for 145 dwellings (following the granting of outline planning permission ref. 15/00704/OUT.)
Site	Former Oldham batteries site, Edward Street, Denton
Applicant	Wainhomes (North West) Ltd, Warrington
Recommendation	Approve, subject to conditions
Reason for report	A Speakers Panel decision is required because the application constitutes a major development.

REPORT

1. APPLICATION DESCRIPTION

- 1.1 The applicant seeks approval for the appearance, landscaping, layout and scale of development following the granting of outline planning permission under reference 15/00704/OUT for a development of 150 houses, in September 2015.
- 1.2 This application relates to land to the west of Edward Street. A parcel of land on the eastern side of Edward Street also has outline planning permission for up to 56 dwellings and is the subject of a separate reserved matters application, reference 18/00307/REM, which is also an item on this agenda.
- 1.3 This is a resubmission following the refusal of planning application reference 17/00513/REM which sought approval for 114 dwellings. That application was refused due to concerns regarding the density and quality of the design of the proposals and is now the subject of an appeal.
- 1.4 The applicant has provided the following documents in support of the planning application:
 - Drainage Statement
 - Noise Assessment
 - Planning and Design and Access Statement

2. SITE & SURROUNDINGS

- 2.1 This application relates to land to the west of Edward Street, which measures approximately 4 hectares and was formerly occupied by the Oldham Batteries factory, which closed in 2002. All buildings have been demolished with only hard standings remaining on site. To the south of the site are a series of two storey terraced residential properties along Patterson Street, Gresham Street and Lime Grove, which are off Hyde Road and the Denton Methodist Church, which fronts Hyde Road. To the west is further land associated with the former use on site and beyond which lies Ashton Road.
- 2.2 The site is located to the east of the commercial element of Denton town centre and is allocated in the UDP as a Development Opportunity Area, emphasising its strategic importance as a regeneration site. Lance Corporal Andrew Breeze Way has been constructed adjacent to the northern boundary of the site and is operational, as required by a condition attached to the outline planning permission.
- 2.3 The principal point of access, as approved at the outline stage, would be taken from Edward Street on the eastern boundary of the site.

3. PLANNING HISTORY

- 3.1 17/00513/REM – reserved matters application for 114 dwellings – refused.
- 3.2 15/00704/OUT - Outline proposal for residential development of up to 150 houses, including means of access, car parking and associated works.(Resubmission of 14/01149/OUT) – approved 09.09.2015.
- 3.3 14/01149/OUT - Outline proposal for residential development of up to 150 houses – refused (the reasons for which addressed by the 2015 application above which was approved).
- 3.4 15/00686/ENV - Request for screening opinion in accordance with Town and Country Planning (Environmental Impact Assessment) Regulations 2011 relating to a proposed residential development – not EIA development.

Nb this Screening Opinion was assessed against the 2011 Environmental Impact Assessment Regulations (as amended). These regulations have since been superseded by the 2017 Regulations. The new Regulations have altered the thresholds in Schedule 2 and a Screening Opinion is no longer required for a development that is below 150 dwellings on a site below 5 hectares in area. This Reserved Matters application proposes less than 150 dwellings

4. RELEVANT PLANNING POLICIES

4.1 Tameside Unitary Development Plan (UDP) Allocation

Allocated under policy E2 (11) as a Development Opportunity Area

4.2 Part 1 Policies

- 1.3: Creating a Cleaner and Greener Environment.
- 1.4: Providing More Choice and Quality Homes.
- 1.5: Following the Principles of Sustainable Development
- 1.6 Securing Urban Regeneration
- 1.12: Ensuring an Accessible, Safe and Healthy Environment

4.3 Part 2 Policies

- E2: Development Opportunity Area (this site being no. 11 of the sites identified in that policy).
- H2: Unallocated Sites (for housing)
- H4: Type, size and affordability of dwellings
- H5: Open Space Provision
- H7: Mixed Use and Density.
- H10: Detailed Design of Housing Developments
- OL10: Landscape Quality and Character
- T1: Highway Improvement and Traffic Management.
- T10: Parking
- T11: Travel Plans.
- C1: Townscape and Urban Form
- N5: Trees Within Development Sites
- N7: Protected Species
- MW11: Contaminated Land
- MW12: Control of Pollution
- MW14: Air Quality
- U3: Water Services for Developments

U4 Flood Prevention
U5 Energy Efficiency

4.4 **Other Policies**

Greater Manchester Spatial Framework - Publication Draft October 2016
Residential Design Supplementary Planning Document

4.5 **National Planning Policy Framework (NPPF)**

Section 1 Delivering sustainable development
Section 2: Ensuring the vitality of town centres
Section 6: Delivering a wide choice of high quality homes
Section 7: Requiring good design
Section 8: Promoting healthy communities

4.6 **Planning Practice Guidance (PPG)**

4.7 This is intended to complement the NPPF and to provide a single resource for planning guidance, whilst rationalising and streamlining the material. Almost all previous planning Circulars and advice notes have been cancelled. Specific reference will be made to the PPG or other national advice in the Analysis section of the report, where appropriate.

5. **PUBLICITY CARRIED OUT**

5.1 Neighbour notification letters were issued, notices displayed on site and an advert published in the press, in accordance with the requirements of the Town and Country Planning (Development Management Procedure) (England) Order 2015.

6. **RESPONSES FROM CONSULTEES**

- 6.1 Highways England – no objections to the proposals in terms of impact on the strategic road network.
- 6.2 Borough Environmental Health Officer – no objections subject to conditions requiring adherence to the contents of the submitted Air Quality Management Plan and a restriction on the hours of activity and deliveries during the construction phase of the development.
- 6.3 Borough Ecologist – no objections to the proposals. A number of conditions were recommended at the outline stage including securing biodiversity enhancements through the redevelopment of the site, control of invasive species and the timing of tree/shrub removal from the site. As a result of the content of the Bat and Great Crested Newt surveys submitted with the outline planning application, no further survey work or mitigation measures is required in relation to the preservation of these species.
- 6.4 Transport for Greater Manchester (TFGM) – no objections to the proposals. The principle of development for up to 150 dwellings was established at the outline planning application stage and the impact on the highway network in terms of trip generation was considered to be acceptable. The site is considered to be within close proximity to sustainable means of transport which would provide an alternative to making journeys via the private car. There are existing bus stops on the A57, within close proximity of the site, which are served by the regular service from Mottram to Manchester Piccadilly. Hyde Central Train Station is also within a bus journey and short walk from the site. The layout of the development should create a safe and attractive environment for pedestrians and cyclists and opportunities for connection through to surrounding streets should be provided. No further conditions

suggested as a condition requiring the submission and approval of a Travel Plan for the development was attached to the outline planning permission.

- 6.5 Local Highway Authority – no objection was raised to the principle of development at the outline stage, subject to the commencement of construction of the Denton Link Road prior to the occupation of any of the dwellings. This has now been constructed and is open to traffic.
- 6.6 United Utilities – did not object to the principle of development at the outline stage, subject to the means of drainage being laid out in accordance with the details submitted with that application.
- 6.7 Greater Manchester Police – raised no objection to the outline application indicating that a Crime impact Statement should be submitted and that the scheme should be designed to meet Secured by Design standards.

7. SUMMARY OF THIRD PARTY RESPONSES RECEIVED

- 7.1 Andrew Gwynne MP raised concerns in relation to the previous application regarding the impact of potential ground contamination on the site on the future occupants of the development. A condition requiring the submission of an investigation into potential sources of contamination on the site, the agreement of a remediation strategy and the implementation of this strategy prior to the commencement of development was attached to the outline planning application. This matter does not therefore need to be revisited at this reserved matters stage.
- 7.2 No representations have been received from any other local residents in relation to the reserved matters application.

8. ANALYSIS

- 8.1 The issues to be assessed in the determination of this planning application are:

- 1) The principle of development
- 2) The impact of the proposed layout, design and scale of the development on the character of the site and the surrounding area)
- 3) The impact upon the residential amenity of neighbouring properties
- 4) The impact on highway safety
- 5) The impact on flood risk and environmental health
- 6) The acceptability of the proposed landscaping scheme

9. PRINCIPLE OF DEVELOPMENT

- 9.1 The principle of residential development and means of access to the site have already been approved by the grant of outline planning permission in 2015. The key issues for consideration now are detailed matters relating to layout, scale, design and appearance of the proposed new homes and this is discussed in more detail below.

10. CHARACTER OF THE SITE AND SURROUNDING AREA

- 10.1 Section 7 of the NPPF requires development to achieve good standards of design and states at paragraph 56 that 'Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for

people.’ National policy also emphasises the importance of new development effectively responding to the character of the surrounding area. Paragraph 58 of the NPPF sets out a number of criteria stating that decisions should aim to ensure that developments (those criteria relevant to residential development):

- will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;

- establish a strong sense of place, using streetscapes and buildings to create attractive and comfortable places to live, work and visit;- respond to local character and history, and reflect the identity of local surroundings and materials, while not preventing or discouraging appropriate innovation;

- create safe and accessible environments where crime and disorder, and the fear of crime, do not undermine quality of life or community cohesion; and

- are visually attractive as a result of good architecture and appropriate landscaping

10.2 This site is considered to be in a prominent location and is therefore an important regeneration site within Denton, a characteristic emphasised by its designation as a Development Opportunity Area. Policy E2 of the UDP states that in these areas ‘the Council will permit redevelopment or refurbishment schemes which include uses likely to create higher levels or quality of.....residential provision and bring about significant improvements in overall appearance.’

10.3 In order to achieve the quality required by the NPPF and policy E2, it is considered that the scheme would need to be designed to connect to the wider regeneration of this part of Denton, recognising that the designated Opportunity Area extends beyond the boundaries of the application site. These connections should be evidenced in the scale, density, siting and detailed design of the buildings, how public and private spaces are treated and how routes through the site are defined. In relation to the previous scheme which was refused, officers had the following concerns:

- a. the layout was typical of a suburban area, on a site that sits within an urban context.
- b. the treatment of the northern boundary of the site, facing Lance Corporal Andrew Breeze Way was considered to lack the strength and quality to adequately define a sense of place.
- c. there was a lack of a strong built form in a number of key areas and a lack of scale and presence to the development.
- d. the house types were considered to emphasise the suburban form and poor relationship between the development and the urban character of the surrounding area.

10.4 The following paragraphs assess how the revised application has addressed each of these points in turn:

Overall density and form

10.5 The revised scheme now proposes 145 units over a developable area of 3.27 hectares. That equates to a density of 44 dwellings per hectare, the equivalent to the density approved at the outline stage (the application site at this reserved matters stage has reduced due to the extent of the highway works involved in the creation of Lance Corporal Andrew Breeze way on the northern edge of the site). This increase in density has been brought about by a far stronger grid form of development in this revised proposal, when compared to the refused scheme and the incorporation of flatted development in 3 storey buildings.

- 10.6 The closure of gaps along the internal access roads through a strong building line results in a development that has a linear emphasis, which respects the urban context of the site and surroundings. The spine route through the site from the entrance on Edward Street is more consistently lined with properties fronting the route, creating a more legible development and defining a sense of place, leading to the public open space in the form of the square in the north western corner of the development. In addition, there is a more consistent frontage to the north-south aligned internal access roads within the scheme, which emphasise how the increase in density has resulted in a much stronger grid pattern, which is characteristic of urban residential development in the surrounding area.

Lance Corporal Andrew Breeze Way

- 10.7 The treatment of the northern edge of the development, fronting onto Lance Corporal Andrew Breeze Way has significantly improved in this revised scheme. The introduction of house type 'c' ensures that 3 storey units would occur at regular intervals along that boundary, providing a physical presence that was not evident in the refused scheme. The introduction of these taller units ensures that the proposal counteracts the fact that the units are set back from the northern boundary and provides strength to this frontage.
- 10.8 This addresses a significant weakness of the previous design, where the height along this boundary was limited to 2 storey with rooms in the roof of some of the units. By increasing the scale of buildings on this frontage, the scheme now has far greater definition in views from Lance Corporal Andrew Breeze Way, giving the development a stronger sense of place and in turn a stronger relationship with the wider area, recognising the regeneration role of the key site close to the centre of Denton.

Strength of built form and addressing key frontages

- 10.9 The previous scheme did not include any apartments. That provided a limitation as to how the south eastern and north eastern corners of the site were treated. These corners contained mainly single units, which resulted in gaps at key points, reducing the strength of the frontage at either end of Edward Street. This failed to recognise the importance of these prominent corners of the development as key connections to the wider area and highly visible points at which the scheme would be defined in public views.
- 10.10 The revised scheme has addressed this weakness through the provision of a continuous 3 storey block of apartments in the south eastern corner and sets of 3 units on the north eastern corner which include a 3 storey middle unit. Whilst the proposed treatment of the north eastern corner would still result in gaps between buildings, these gaps are fewer in number and reduced in prominence by the use of part 3 storey development in that location in the revised scheme.
- 10.11 The revised proposals present a stronger frontage to the large square in the north western corner, which provides the main area of public open space within the development. The dwellings on the eastern edge of that square now engage with the space as opposed to being set back within the plots. This provides a greater sense of surveillance of the public space and at the same time further reinforces the urban grain of the layout.

Improvement in mix of house types

- 10.12 The treatment of the corners at either end of Edward Street have also become more coherent through the use of gable features to define the 3 storey dwellings in the north eastern corner and on 3 of the outer facing wings of the apartments now proposed in the south eastern corner. This is a significant improvement on the previous scheme in terms of creating a sense of place and an identity to the development, resulting in a scheme that will improve the appearance of the area, as required by policy E2.

- 10.13 This improvement, alongside the use of 3 storey units along the northern boundary, fronting on to Lance Corporal Andrew Breeze Way results in a proposal that includes house types that respond to the size and shape of the site. This addresses the fundamental weakness of the previous scheme, in which a range of 'standard' house types were imposed on the site in a way which failed to respond positively to the specific characteristics of the site, thereby undermining the quality of the scheme.

Summary in relation to design:

- 10.14 To summarise, the proposals are considered to be a significant improvement on the refused scheme. The density of the development has increased substantially and is now equivalent to that approved at the outline stage. In addition to the benefit of a greater supply of housing, the higher density of the development has resulted in a development that more effectively respects the urban grain of the surrounding area. The revised scheme is far more legible due to stronger frontages to the key routes through the site and clearer framing of the areas of open space.
- 10.15 An increase in the height of development along the northern and eastern edges of the site, including the sensitive north eastern and south eastern corners, has resulted in a scheme which would have a stronger presence and clearly defines the character of the development from key public views. The revised scheme therefore contributes positively to the regeneration of the surrounding area and would result in a significant improvement in the appearance of the area, meeting the requirements of policy E2.

11. RESIDENTIAL AMENITY

- 11.1 The outline planning permission established the principle of development for up to 150 dwellings on the site. This reserved matters application proposes a lower density of development, which, indicates that sufficient separation distances could be retained between the edge of the development and the neighbouring residential properties on the terraced streets to the south to achieve the space standards set out in the Residential Design Guide (RDG). The fact that Edward Street bisects the wider site previously occupied by Oldham Batteries ensures that sufficient separation would also be provided between the eastern edge of the site that is the subject of this application and the western edge of the scheme that this the subject of application ref. 18/00307/REM. The arrangement of plots within the development would allow separation distances to be retained that would achieve the guidance within the RDG (21 metres between elevations with habitable room windows and 14 metres between blank elevations and elevations with openings.)
- 11.2 Following the above assessment it is considered that the proposals would not have an adverse impact on the residential amenity of the neighbouring properties and would adequately preserve the amenity of future occupants of the development.

12. HIGHWAY SAFETY

- 12.1 The Local Highway Authority raised no objection to the principle of the development of the site, subject to the commencement of development of the Denton Link Road prior to the occupation of any of the dwellings. This requirement was secured by condition 10 on the outline planning permission. At the outline stage, the Transport Assessment submitted with the application was considered sufficient to demonstrate that the number of trips generated by 150 dwellings would not have a severe impact on the capacity of the road network and would therefore not be prejudicial to highway safety. Given that this scheme equates to the same density but actually proposes less dwellings, thereby generating fewer trips, it is considered that the same assessment would apply.

- 12.2 The proposed plans indicate that 2 car parking spaces would be provided for most dwellings, some limited to 1 space although the scheme includes **some 1 bed units**. The proposals would therefore meet the requirements of the RDG. As highlighted by the response from TfGM to the outline planning application, the site is considered to be within close proximity to sustainable means of transport which would provide an alternative to making journeys via the private car. There are existing bus stops on the A57, within close proximity of the site, which are served by the regular service from Mottram to Manchester Piccadilly. Hyde Central Train Station is also within a bus journey and short walk from the site.
- 12.3 Given this context, it is considered that the level of parking to be provided within the development would not result in a reliance on on-street parking, ensuring that there would not be an unreasonable impact on highway safety in this regard. In terms of connectivity to the surrounding area, pedestrian links would be provided to Gresham Street and Lime Grove on the southern boundary of the development and footway links would be provided from the development along Edward Street, Hyde Road and Lance Corporal Andrew Breeze Way. These factors, alongside the information required to discharge the Travel Plan condition attached to the outline planning permission would ensure that the development would include provision for sustainable modes of transport.
- 12.4 Highways England has not objected to the application and did not object to the principle of development at the outline stage. Conditions requiring details of the surfacing of driveways, the provision of pedestrian visibility splays and requiring all car parking to be laid out prior to the occupation of the development, can be added to the decision notice.

13. FLOOD RISK

- 13.1 The site is in Flood Zone 1 and is therefore considered to be at a lower risk of flooding. United Utilities have raised no objections to the proposals subject to the means of drainage being laid out in accordance with the details submitted with that application. Condition 4 of the planning permission requires a surface water drainage strategy to be submitted and approved prior to the first occupation of the development. The applicant confirmed on the application form at the outline stage that foul water would be drained from the development via a connection to the existing main sewer.
- 13.2 It is considered that the proposed development would not result in a harmful impact in relation to flood risk.

14. ENVIRONMENTAL HEALTH

- 14.1 The Borough's Environmental Health Officer has raised no objections to the proposals, subject to the imposition of conditions limiting the hours of operation and deliveries during the construction phase of the development and compliance with the mitigation measures detailed in the Air Quality Assessment submitted with the outline planning application. These conditions were imposed on the outline planning permission (conditions 7 and 11) and therefore would not need to be re-imposed on a reserved matters approval, had all other material considerations been satisfied.
- 14.2 The applicant has submitted a Noise Assessment with the reserved matters application. This report indicates that a number of plots would require mitigation to be installed in order to minimise the impact of the noise generated by the traffic on the M67 and Lance Corporal Andrew Breeze way to the north of the site, Edward Street to the east and Hyde Road to the south. Within the parcel to the west of Edward Street that is the subject of this application, the report suggests that 2.5 metre high acoustic fencing would be required to the rear of plots in the south eastern corner, the north western part of the site and a number

of plots along the northern edge of the site, to mitigate the impact of noise generated by communal parking areas.

- 14.3 Furthermore, a number of the plots would need to be fitted with high specification glazing and mechanical ventilation systems that would allow air circulation without the need to open windows, to mitigate the impact of noise sources beyond the boundaries of the site, listed above. Further details of the specifications of the glazing and the acoustic fencing can be required by condition, to ensure that the residential amenity of the future occupiers of the development would be adequately preserved.
- 14.3 The MP raised concerns in relation to the previous application regarding the history of ground contamination on the site and the impact that disturbance of the contamination may have on the living conditions of the future occupants of the development. A condition requiring the submission of an investigation into potential sources of contamination on the site, the agreement of a remediation strategy and the implementation of this strategy prior to the commencement of development was attached to the outline planning application. This matter does not therefore need to be revisited at this reserved matters stage.

15. LANDSCAPING

- 15.1 The improvements to the layout in this revised scheme allow the landscaping details to far more effectively delineate between public and private space, with both hard and soft boundary treatments helping to provide legibility and permeability to the development. This is a result of strengthening the front building line and alignment of plots in a stronger 'grid' formation. Soft landscaping would be used to provide a feature at the centre of public square in the north western part of the site and also to break up the parking courtyard area to the rear of the units in the north eastern corner.
- 15.2 Overall, it is considered that the proposed hard and soft landscaping scheme would enhance the quality of the development and would be appropriate for the urban setting of the site.

16. OTHER MATTERS

- 16.1 As a result of the content of the Bat and Great Crested Newt surveys submitted with the outline planning application, no further survey work or mitigation measures is required in relation to the preservation of these species. A number of conditions were recommended by GMEU including securing biodiversity enhancements through the redevelopment of the site, control of invasive species and the timing of tree/shrub removal from the site. These can be attached to the decision notice.
- 16.2 In relation to crime impact, whilst the scheme does include some communal car parking areas, these are considered to be well surveyed and details of external lighting of these areas can be secured by condition. Areas of public space would be overlooked by the principal elevations of dwellings. Greater Manchester Police have not raised any objections to the proposals. A condition can be attached to the planning permission requiring the submission of details to be incorporated within the development to reduce the risk of crime and requiring Secured by Design accreditation to be achieved.

17. CONCLUSION

- 17.1 The site plays a key role in the regeneration of Denton, as demonstrated by the designation of the site as a Development Opportunity Area. The revised proposal is considered to address the weaknesses of the previously refused scheme by increasing the density of

development, increasing the scale of buildings in key locations and presenting a higher quality layout which better reflects the urban grain of the surrounding area. As a result, the proposals are considered to achieve the objectives of policy E2 of the UDP.

- 17.2 There are no objections from any of the statutory consultees and the proposals are considered to preserve the residential amenity of neighbouring properties as well as the future occupants of the development, maintain highway safety and be acceptable in relation to all other material considerations.
- 17.3 The revised proposals are therefore considered to comply with the relevant national and local planning policies quoted above.

RECOMMENDATION

Grant planning permission, subject to the following conditions:

1. The development hereby approved shall be carried out in accordance with the following approved plans/details:

1:1250 Site location plan (Drawing no. 1536WHD/ESD/LP01 Rev. A)
Proposed site layout plan (Drawing no. 1536WHD/ESD/PL01 Rev. W)
Proposed landscape layout plan (Drawing no. 105 Rev. D)
Proposed Planting Plan (sheet 1 of 3) (Drawing no. 201)
Proposed Planting Plan (sheet 2 of 3) (Drawing no. 202) (insofar as it relates to this application.)
Proposed Planting Plan (sheet 3 of 3) (Drawing no. 203) (insofar as it relates to this application.)
Proposed hardworks plan (sheet 1 of 4) (Drawing number 101 Rev. D)
Proposed hardworks plan (sheet 2 of 4) (Drawing number 102 Rev. D)
Proposed hardworks plan (sheet 3 of 4) (Drawing number 103 Rev. D)
Proposed apartments plans and elevations (plots 3-14) (Drawing no. 1000-3-14)
Proposed house types plots 1 and 2 floor plans and elevations (Drawing no. 100-1&2)
Proposed house types A and B plans and elevations (Drawing no. 1000-A&B)
Proposed house type C floor plans and elevations (Drawing no. 1000-C)
Proposed house types D and E floor plans and elevations (Drawing no. 1000-D&E)
Proposed house type Langley A floor plans and elevations (1536/WHD/ESD/LA01 Rev. A)
Proposed house type Langley B floor plans and elevations (1536/WHD/ESD/LA02 Rev. C)
Proposed 'Baird' contemporary (Gable fronted) plans and elevations (Drawing no. 1536WHD/ESD/BA01 Rev. A)
Proposed 'Bell' contemporary (Gable fronted) plans and elevations (Drawing no. 1536WHD/ESD/BE02 Rev. A)
Proposed 'Brancaster' contemporary (Side aspect) plans and elevations (Drawing no. 1536WHD/ESD/BR01 Rev. B)
Proposed 'Claydon' contemporary plans and elevations (Drawing no. 1536WHD/ESD/CL01 Rev. A)
Proposed 'Haversham' + Bay plans and elevations (Drawing no. 1536WHD/ESD/HA02 Rev. A)
Proposed 'Haversham' (Side aspect) plans and elevations (Drawing no. 1536WHD/ESD/HA03 Rev. A)
Proposed 'Haversham' contemporary plans and elevations (Drawing no. 1536WHD/ESD/HA01 Rev. A)
Proposed 'Jenner' contemporary plans and elevations (Drawing no. 1536WHD/ESD/JE01 Rev. A)
Proposed 'Jenner' contemporary (Gable fronted) plans and elevations (Drawing no. 1536WHD/ESD/JE02 Rev. A)

Proposed 'Newton' contemporary plans and elevations (Drawing no. 1536WHD/ESD/NW01 Rev. A)
Proposed 'Telford' contemporary plans and elevations (Drawing no. 1.136/P/B/G)
Proposed 'Telford' contemporary (with Dummy front) plans and elevations (Drawing no. 1.136/P/B/G/2)
Proposed 'Wordsworth' contemporary plans and elevations (Drawing no. 1536WHD/ESD/WO02 Rev. A)
Proposed 'Wordsworth' (Side aspect- contemporary) plans and elevations (Drawing no. 1536WHD/ESD/WO01 Rev. A)
Proposed 'Wren' contemporary plans and elevations (Drawing no. 1536WHD/ESD/WR01 Rev. A)

2. No development shall commence until the following details have been submitted to and approved in writing by the Local Planning Authority:
 - scaled plans showing the exact location and elevations of the acoustic fencing to be installed within the development site and a manufacturers specification of the fencing; and
 - scaled plans showing the location of windows to be treated with high specification glazing and mechanical ventilation and manufacturers specifications of each of the noise mitigation measures to be installed.

The noise mitigation measures shall be implemented in accordance with the approved details, prior to the first occupation of any of the dwellings and shall be retained as such thereafter.

3. Notwithstanding any description of materials in the application, no above ground construction works shall take place until samples and/or full specification of materials to be used: externally on the buildings; in the construction of all boundary walls, fences and railings; and, in the finishes to all external hard-surfaces have been submitted to, and approved in writing by, the local planning authority. Such details shall include the type, colour and texture of the materials. Development shall be carried out in accordance with the approved details.
4. The car parking spaces to serve the development hereby approved shall be laid out as shown on the approved Proposed site layout plan (Drawing no. 1536WHD/ESD/PL01 Rev. W), prior to the occupation of any of the dwellings and shall be retained free from obstruction for their intended use at all times thereafter.
5. Boundary treatments to be installed in accordance with the details as shown on the following approved plans (insofar as they relate to the application site):
 - Proposed hardworks plan (sheet 1 of 4) (Drawing number 101 Rev. D)
 - Proposed hardworks plan (sheet 2 of 4) (Drawing number 102 Rev. D)
 - Proposed hardworks plan (sheet 3 of 4) (Drawing number 103 Rev. D)

prior to the first occupation of any of the dwellings hereby approved. Such boundary treatments shall be retained at all times thereafter.

6. Hard and soft landscaping to be installed in accordance with the details as shown on the following approved plans (insofar as they relate to the application site):

Proposed landscape layout plan (Drawing no. 105 Rev. D)
Proposed Planting Plan (sheet 1 of 3) (Drawing no. 201)

Proposed Planting Plan (sheet 2 of 3) (Drawing no. 202) (insofar as it relates to this application.)

Proposed Planting Plan (sheet 3 of 3) (Drawing no. 203) (insofar as it relates to this application.)

Proposed hardworks plan (sheet 1 of 4) (Drawing number 101 Rev. D)

Proposed hardworks plan (sheet 2 of 4) (Drawing number 102 Rev. D)

Proposed hardworks plan (sheet 3 of 4) (Drawing number 103 Rev. D)

prior to the first occupation of any of the dwellings hereby approved.

7. The approved scheme of landscaping scheme shall be implemented before the first occupation of any part of the development or in accordance with a programme agreed previously with the local planning authority. Any newly planted trees or plants forming part of the approved scheme which, within a period of 5 years from the completion of the planting, are removed, damaged, destroyed or die shall be replaced in the next appropriate planting season with others of similar size and species.
8. Prior to the occupation of any part of the development hereby approved, a scheme detailing the measures to be incorporated in an application to obtain Secured by Design accreditation to Greater Manchester Police shall be submitted to and approved in writing by the Local Planning Authority. Written confirmation that the accreditation has been achieved shall be submitted to and approved in writing by the Local Planning Authority and the approved measures shall be implemented prior to the first occupation of any part of the development. The development shall be retained as such thereafter.
9. No part of the development hereby approved shall be occupied until details of the means of storage and collection of refuse generated by the development have been submitted to and approved in writing by the Local Planning Authority. The details shall include scaled plans showing the location of storage and the means of enclosure. The bin storage arrangements for each dwelling shall be implemented in accordance with the approved details prior to the occupation of that dwelling and shall be retained as such thereafter.
10. None of the dwellings hereby approved shall be occupied until details of the provision of secured storage for bicycles within each of the plots (minus those plots with garages) of the development hereby approved have been submitted to and approved in writing by the Local Planning Authority. The cycle storage shall be provided for each dwelling in accordance with the approved details prior to the occupation of that dwelling and shall be retained as such thereafter.
11. No development above ground level shall commence until details of Biodiversity enhancement measures to be installed as part of the development hereby approved has been submitted to and approved in writing by the Local Planning Authority. The details shall include a specification of the installations and scaled plans showing their location within the development. The approved enhancement measures shall be installed in accordance with the approved details, prior to the first occupation of any of the dwellings and shall be retained as such thereafter.
12. Prior to the first occupation of any of the dwellings hereby approved, details of a scheme for external lighting to serve the development shall be submitted to and approved in writing by the Local Planning Authority. The details shall include a scale plan indicating the location of the lighting to be installed, a LUX contour plan indicating the levels of light spillage and scaled elevations of lighting columns/supporting structures. The external lighting scheme

shall be implemented in accordance with the approved details prior to the first occupation of any of the dwellings and shall be retained as such thereafter.

13. No tree felling or vegetation removal shall take place during the optimum period for bird nesting (March to July inclusive) unless otherwise agreed in writing with the Local Planning Authority.
14. No development shall commence unless and until a Method Statement is submitted to and approved in writing by the Local Planning Authority detailing how Japanese Knotweed and any other invasive species on the site will be removed from the site. The development shall thereafter proceed in strict accordance with the approved Method Statement.
15. The driveways to serve the dwellings hereby approved shall be constructed from a bound material and shall be constructed on a level that prevents the displacement of material and surface water onto the highway. The development shall be retained as such thereafter.
16. Prior to the occupation of any part of the development hereby approved, visibility splays shall be provided on both sides of the site access where it meets the footway. The visibility splays shall measure 2.4 metres along the edge of the site access and 2.4 metres along the footway. It must be clear of anything higher than 600mm above ground level. The visibility splays shall be retained as such thereafter.

Reasons for conditions:

1. For the avoidance of doubt
2. To protect the amenities of future occupants from external noise in accordance with UDP policy H10.
3. To ensure that the appearance of the development reflects the character of the surrounding area.
4. To ensure that the development is served by adequate parking provision.
5. To protect the amenities of occupants of nearby properties/dwelling houses in accordance with UDP policies 1.12 and E6.
6. To ensure that sufficient hard and soft landscaping are implemented to ensure that the overall development respects the character of the surrounding area.
7. To ensure that the approved landscaping scheme is adequately maintained.
8. To ensure that the development is designed to minimise opportunities for crime.
9. To provide adequate secure bin storage to serve the development and to safeguard the general amenity of the area in accordance with UDP policy 1.12/1.13/H10.
10. To ensure cycle storage is provided to enhance the environmental sustainability of the development.

11. To ensure biodiversity enhancements are secured to mitigate the environmental impacts of the scheme.
12. To ensure that adequate lighting is provided to public areas
13. In order to prevent any habitat disturbance to nesting birds in accordance with the National Planning Policy Framework.
14. To ensure that invasive species are removed from the site appropriately.
15. To ensure that the development maintains highway safety.
16. To ensure that the development maintains highway safety.

Application Number: 17/00513/REM Land East of Edward street Denton

Photo 1 – south eastern corner of site from Hyde Road



Photo 2 – central part of the site from Edward Street

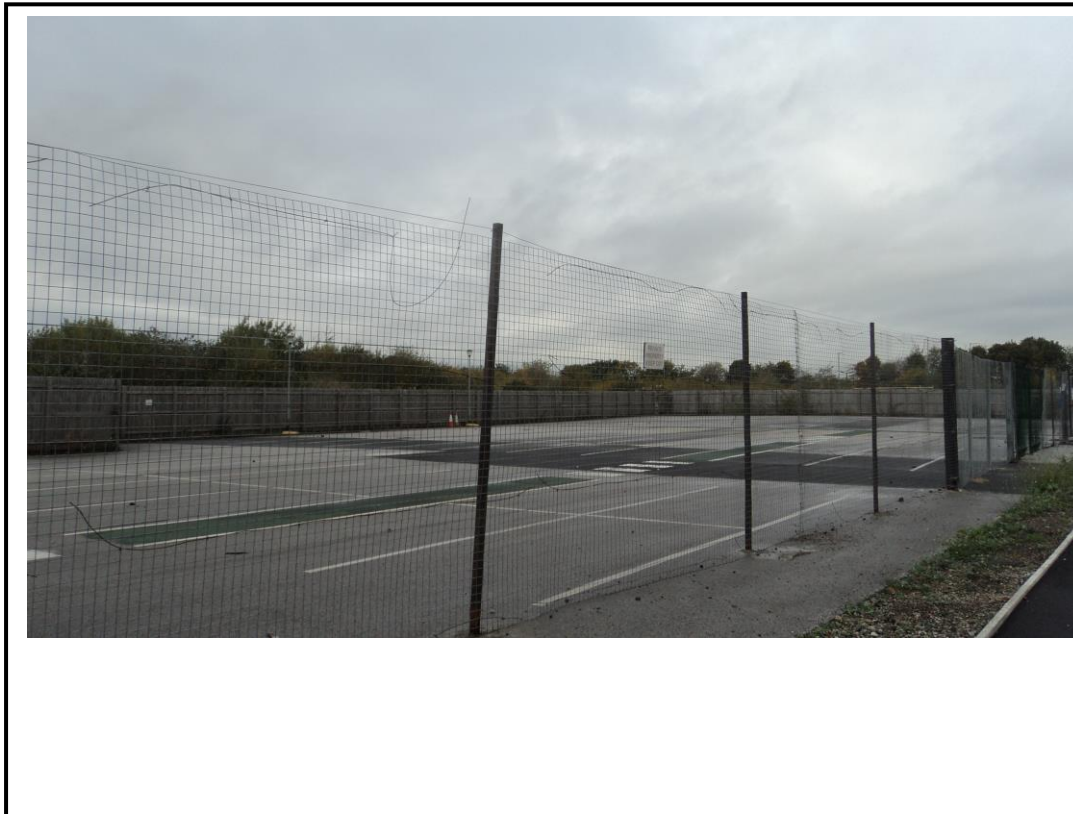


Photo 3 – north eastern corner of the site



Photo 4 – looking south down Edward Street from north eastern corner of the site



Photo 5- looking southwards through application site from Lance Corporal Andrew Breeze Way on northern boundary



Photo 6 – looking along northern boundary of the site from north eastern corner

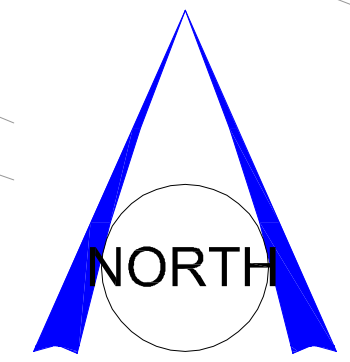


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Edward Street, Denton

SITE LAYOUT

LANCE CORPORAL ANDREW BREEZE WAY



Mix Schedule	
Plots 146 - 200 (Phase 2)	
Baird (BAI)	6
Bell (BEL)	20
Brancaster-SA (BRA-SA)	2
Chinley Apartments (CHI)	6
Claydon-SA (CLA-SA)	1
Jenner Gable (JEG)	4
Jenner Special (JES)	1
Wordsworth (WOR)	1
Wordsworth-SA (WOR-SA)	4
Wren (WRE)	3
Langley (LAN)	3
Telford (TEL)	1
Type A (A)	2
Type B (B)	1
TOTAL	55

Mix Schedule	
Plots 1 to 145 (Phase 1)	
Apartments 1-14 (Ap 1-14)	14
Baird (BAI)	25
Bell (BEL)	14
Brancaster (BRA)	7
Brancaster-SA (BRA-SA)	1
Claydon (CLA)	1
Claydon-SA (CLA-SA)	2
Haversham (HAV)	2
Haversham-SA (HAV-SA)	3
Jenner Gable (JEG)	18
Jenner (JEN)	2
Newton (NEW)	3
Telford (TEL)	6
Wordsworth (WOR)	8
Wordsworth-SA (WOR-SA)	5
Wren (WRE)	1
Langley (LAN)	4
Type A (A)	7
Type B (B)	4
Type C (C)	10
Type D (D)	4
Type E (E)	4
TOTAL	145

Rev.	Date	Revision Description	Drawn
V	13-4-18	Amendments following Case Officer Comments / Meeting with applicant 12/4	DGL
T	19-3-18	Amendments throughout the site, 145 Plots	DGL
S	23-1-18	Minor amendments following client comments to finalise for re-submission to LA	DGL
Rv5	19-1-18	North Eastern Corner of Phase 1 amended + Further tweaks to mix	DGL
P	8-1-18	Preliminary Issue Rev/P	DGL
Nv2	5-1-18	Preliminary Issue Rev/Nv2	DGL
M	25-10-17	Redesigns to plots 71/72, 10, 23-26, 93 to 107 and 156/157	DGL
Lv2	13-10-17	Various amendments in dealing with comments 31/10/17 from SW	DGL
K	05-09-17	2nd Major re-plan following discussions with OH	DGL
Nv2	17-7-17	Major re-plan following second Planning Meeting (Draft Rev J V2)	DGL
H	2-7-17	Various re-design amendments following meeting with LA / discussions with the applicant	DGL
Qv2	16-4-17	Jenner (Special) and Claydon SA types clarified on Layout and Mix, Scott Type updated	DGL
F	24-05-17	Further amendments to parking courts following comments from Landscape Arch / Handings checked and SA / DA types clarified on layout and schedule	DGL
E	09-05-17	Railings / Railings with Brick Piers added to site frontages and footpath links	DGL
D	05-05-17	Highway amendments prior to submission	DGL
C	24-04-17	Parking Courts revised, additional screening added	DGL
B	16-03-17	Northern Access Point added	DGL
A	01-02-17	Nelson house type bay position updated	DGL
Rev.	Date	Revision Description	Drawn
W	24-4-18	Plots 147 to 149 changed to types AB	DGL

Legend

- 68 Site Boundary (Red Edge)
- Existing Trees & Hedges Retained
- Existing Trees & Hedges Removed/Reduced shown Dashed
- Indicative New Tree Planting and Hedgerows
- 1.8m High Timber Screen Fencing (Double Line)
- 900mm High Brick Wall with Brick Piers and Timber Infill Panels (Thick Dashed Line)
- 1.2m High Metal Railings between Brick Piers
- 1.2m High Metal Railings alongside proposed footpath links as shown
- 1.8m High Brick Wall with Brick Piers (Solid Line)
- 1.8m High Timber Close Boarded Plot Divisional Fencing (Dashed Line)
- Gated Access to Rear of Properties (Lockable) Arrows Denote Door Entry Positions into Dwellings
- Shared Private Drive Areas (Bitmac with Rumble Strip Sets)
- Key Junctions and Speed Tables (Red Block paved - Transitional elements between highway materials can be tegula or cobbled sets) Key spaces - Materials to be agreed with Tree Grids as shown
- Refer to Hardworks Plans produced by tpm landscape for specific design detail to these areas



Barn Meadow House
 Southfield Fold Farm
 Southfield
 Burnley
 Lancashire
 BB10 3RH
 Tel : 01282 601157
 Mbl : 07976 782876

Client	Wainhomes
Project	Edward Street, Denton
Scale	1:1000 @ A3 1:500 @ A1
Drawn	24th Jan 2017
Drawing No.	1536WHD/ESD/PL01
Revision	W
Drawn	D.S. Leaver
Scale	1:1000 @ A3 1:500 @ A1
Drawn	24th Jan 2017
E-mail	darren.dgla@gmail.com
Checked	Approved
	A1

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STREET SCENE TO LANCE CORPORAL ANDREW BREEZE WAY



Plot 105 Type E

Plot 107 Baird

Plot 108 Type C

Plot 109 Type C

Plot 110 Type C

Plot 111 Jenner Gable

Plot 85 Jenner Gable

Plot 86 Type C

Plot 87 Type C

Plot 88 Type C

Plot 89 Baird

Plot 90 Type E

STREET SCENE TO LANCE CORPORAL ANDREW BREEZE WAY / EDWARD STREET

Page 53



Plot 130 Type Claydon

Plot 131 Baird

Plot 132 Baird

Plot 133 Baird

Plot 134 Baird

Plot 135 Type B

Plot 136 Type A

Plot 137 Type A

Plot 138 Type B

Plot 139 Type A

Plot 140 Type A

Plot 141 Type B

Plot 142 Type A

Plot 143 Type A

Plot 144 Type B

Plot 145 Type A

Plot 112 Wordsworth

STREET SCENE TO HYDE ROAD / EDWARD STREET



Apartments Plots 1 - 2

Apartments Plots 2 - 14

STREET SCENE TO HYDE ROAD / EDWARD STREET



Apartments Plots 2 - 14

Plot 15 Bell

Plot 16 Bell

Plot 17 Bell

Plot 18 Bell

Plot 20 Type E

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Application Number 18/00307/REM

Proposal Reserved matters application for appearance, landscaping, layout and scale pursuant to outline consent 15/00081/OUT (55 dwellings proposed)

Site Former Oldham batteries site, Edward Street, Denton

Applicant Wainhomes (North West) Ltd, Warrington

Recommendation Approve, subject to conditions

Reason for report A Speakers Panel decision is required because the application is a major.

REPORT

1. APPLICATION DESCRIPTION

- 1.1 The applicant seeks approval for the appearance, landscaping, layout and scale of development following the granting of outline planning permission under reference 15/00081/OUT for a development of up to 56 houses.
- 1.2 This application relates to land to the east of Edward Street. A parcel of land on the western side of Edward Street also has outline planning permission for up to 150 dwellings and is the subject of a separate reserved matters application, reference 18/00306/REM, which is also on this agenda.
- 1.3 This is a resubmission following the refusal of planning application reference 17/00512/REM which sought approval for 54 dwellings. That application was refused due to concerns regarding the density and quality of the design of the proposals and is now the subject of an appeal.
- 1.4 The applicant has provided the following documents in support of the planning application:
 - Drainage Statement
 - Noise Assessment
 - Planning and Design and Access Statement

2. SITE & SURROUNDINGS

- 2.1 The application site is to the east of Edward Street and is located within Denton Town Centre. The site was part of the former Oldham Batteries factory site, which closed in 2002. The buildings have since been demolished and the application site consists largely of rubble and hardstandings awaiting redevelopment. The site is faced by the rear of the two storey housing fronting onto Osborne Road to the east, the M67 in a cutting to the north, Edward Street to the west and the rear of properties on Hyde Road to the south. The site will also have a part frontage onto Hyde Road.

3. PLANNING HISTORY

- 3.1 17/00512/REM – reserved matters application for 54 dwellings - refused
- 3.2 15/00081/OUT - Outline proposal for residential development of up to 56 houses, including means of access, car parking and associated works – approved 07.05.2015.

- 3.3 15/00150/ENV - Request for screening opinion in accordance with Town and Country Planning (Environmental Impact Assessment) Regulations 2011 relating to a proposed residential development – not EIA development.

Nb this Screening Opinion was assessed against the 2011 Environmental Impact Assessment Regulations (as amended). These regulations have since been superseded by the 2017 Regulations. The new Regulations have altered the thresholds in Schedule 2 and a Screening Opinion is no longer required for a development that is below 150 dwellings on a site below 5 hectares in area. This Reserved Matters application proposes less than 150 dwellings and the site area is below 5 hectares.

4. RELEVANT PLANNING POLICIES

4.1 Tameside Unitary Development Plan (UDP) Allocation

Allocated under policy E2 (11) as a Development Opportunity Area

4.2 Part 1 Policies

- 1.3: Creating a Cleaner and Greener Environment.
- 1.4: Providing More Choice and Quality Homes.
- 1.5: Following the Principles of Sustainable Development
- 1.6 Securing Urban Regeneration
- 1.12: Ensuring an Accessible, Safe and Healthy Environment

4.3 Part 2 Policies

- E2: Development Opportunity Area (this site being no. 11 of the sites identified in that policy).
- H2: Unallocated Sites.
- H4: Type, size and affordability of dwellings
- H5: Open Space Provision
- H7: Mixed Use and Density.
- H10: Detailed Design of Housing Developments
- OL10: Landscape Quality and Character
- T1: Highway Improvement and Traffic Management.
- T10: Parking
- T11: Travel Plans.
- C1: Townscape and Urban Form
- N5: Trees Within Development Sites
- N7: Protected Species
- MW11: Contaminated Land
- MW12: Control of Pollution
- MW14 Air Quality
- U3: Water Services for Developments
- U4 Flood Prevention
- U5 Energy Efficiency

4.4 Other Policies

- Greater Manchester Spatial Framework - Publication Draft October 2016
- Residential Design Supplementary Planning Document
- Trees and Landscaping on Development Sites SPD adopted in March 2007

4.5 National Planning Policy Framework (NPPF)

Section 1 Delivering sustainable development
Section 6 Delivering a wide choice of high quality homes
Section 7 Requiring good design
Section 8 Promoting healthy communities

4.6 **Planning Practice Guidance (PPG)**

4.7 This is intended to complement the NPPF and to provide a single resource for planning guidance, whilst rationalising and streamlining the material. Almost all previous planning Circulars and advice notes have been cancelled. Specific reference will be made to the PPG or other national advice in the Analysis section of the report, where appropriate.

5. **PUBLICITY CARRIED OUT**

5.1 Neighbour notification letters were issued in accordance with the requirements of the Town and Country Planning (Development Management Procedure) (England) Order 2015.

6. **RESPONSES FROM CONSULTEES**

6.1 Highways England – no objections to the proposals.

6.2 Borough Environmental Health Officer – no objections subject to conditions requiring adherence to the contents of the submitted Air Quality Management Plan and a restriction on the hours of activity and deliveries during the construction phase of the development.

6.3 Borough Ecologist – no objections to the proposals. A number of conditions were recommended at the outline stage including securing biodiversity enhancements through the redevelopment of the site, control of invasive species and the timing of tree/shrub removal from the site. As a result of the content of the Bat and Great Crested Newt surveys submitted with the outline planning application, no further survey work or mitigation measures is required in relation to the preservation of these species.

6.4 Transport for Greater Manchester – no objections to the proposals. The principle of development for up to 56 dwellings was established at the outline planning application stage and the impact on the highway network in terms of trip generation was considered to be acceptable. The site is considered to be within close proximity to sustainable means of transport which would provide an alternative to making journeys via the private car. There are existing bus stops on the A57, within close proximity of the site, which are served by the regular service from Mottram to Manchester Piccadilly. Hyde Central Train Station is also within a bus journey and short walk from the site. The layout of the development should create a safe and attractive environment for pedestrians and cyclists and opportunities for connection through to surrounding streets should be provided. No further conditions suggested as a condition requiring the submission and approval of a Travel Plan for the development was attached to the outline planning permission.

6.5 Local Highway Authority – no objection was raised to the principle of development at the outline stage.

6.6. Greater Manchester Archaeological Advisory Service – raised no objection to the principle of development at the outline stage, subject to the imposition of a condition requiring an archaeological investigation to be undertaken and any necessary mitigation measures implemented prior to the commencement of development. Such a condition was added to the outline planning permission.

- 6.7 United Utilities – did not object to the principle of development at the outline stage, subject to the means of drainage being laid out in accordance with the details submitted with that application.
- 6.8 Greater Manchester Police – raised no objection to the outline application indicating that a Crime impact Statement should be submitted and that the scheme should be designed to meet Secured by Design standards.

7. SUMMARY OF THIRD PARTY RESPONSES RECEIVED

- 7.1 Andrew Gwynne MP raised concerns in relation to the previous application regarding the impact of potential ground contamination on the site on the future occupants of the development. A condition requiring the submission of an investigation into potential sources of contamination on the site, the agreement of a remediation strategy and the implementation of this strategy prior to the commencement of development was attached to the outline planning application. This matter does not therefore need to be revisited at this reserved matters stage.
- 7.2 No other third party representations have been received in relation to this revised reserved matters application. 3 letters of representation were received from neighbouring residents in relation to the previously refused scheme. All acknowledged the potential benefits of the redevelopment of the site but also raised the following concerns (summarised):
- Parking is already a problem on Lupton Street, Nelson Street and Lime Grove. This will be made worse by the proposed development. Residents' only permits should be considered to address this issue.
 - How will the boundaries of the existing properties on Osborne Road (which back on to the eastern boundary of the application site) be protected?

8. ANALYSIS

- 8.1 The issues to be assessed in the determination of this planning application are:

- 1) The principle of development
- 2) The impact of the proposed layout, design and scale of the development on the character of the site and the surrounding area)
- 3) The impact upon the residential amenity of neighbouring properties
- 4) The impact on highway safety
- 5) The impact on flood risk and environmental health
- 6) The acceptability of the proposed landscaping scheme

9. PRINCIPLE OF DEVELOPMENT

- 9.1 The principle of residential development and means of access to the site have already been approved by the grant of outline planning permission. The key issues for consideration now are detailed matters relating layout, scale, design and appearance of the proposed new homes and this is discussed in more detail below.

10. CHARACTER OF THE SITE AND SURROUNDING AREA

- 10.1 Section 7 of the NPPF requires development to achieve good standards of design and states at paragraph 56 that 'Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for

people.’ National policy also emphasises the importance of new development effectively responding to the character of the surrounding area. Paragraph 58 of the NPPF sets out a number of criteria stating that decisions should aim to ensure that developments (those criteria relevant to residential development):

- will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
- establish a strong sense of place, using streetscapes and buildings to create attractive and comfortable places to live, work and visit;
- respond to local character and history, and reflect the identity of local surroundings and materials, while not preventing or discouraging appropriate innovation;
- create safe and accessible environments where crime and disorder, and the fear of crime, do not undermine quality of life or community cohesion; and
- are visually attractive as a result of good architecture and appropriate landscaping

10.4 This site is considered to be in a prominent location and is therefore an important regeneration site within Denton, a characteristic emphasised by its designation as a Development Opportunity Area. Policy E2 of the UDP states that in these areas ‘the Council will permit redevelopment or refurbishment schemes which include uses likely to create higher levels or quality of.....residential provision and bring about significant improvements in overall appearance.’

10.5 In order to achieve the quality required by the NPPF and policy E2, it is considered that the scheme would need to be designed to connect to the wider regeneration of this part of Denton, recognising that the designated Opportunity Area extends beyond the boundaries of the application site. These connections should be evidenced in the scale, density, siting and detailed design of the buildings, how public and private spaces are treated and how routes through the site are defined. In relation to the previous scheme which was refused, officers had the following concerns:

- a. Dominance of external parking areas
- b. Lack of strong built form to define the character of the development
- c. Inappropriateness of the proposed house types.

10.6 The following paragraphs assess how the revised application has addressed each of these points in turn:

Dominance of external parking areas

10.7 The revised layout reduces the prominence of the car parking area in the northern section of the site through positioning of units 151 and 152 so that they front onto the internal access road into that part of the development. This is considered to be an improvement on the refused scheme, where the two equivalent plots were surrounded by hardstanding creating a less coherent layout which lacked the strength of frontage required to reflect the urban setting of the site.

10.8 This revised scheme has introduced a landscape buffer to the pedestrian link which runs through the central part of the site, following improvements to parking courts. In the refused scheme, the pedestrian link ran immediately adjacent to the access to the communal parking areas, resulting in an environment dominated by hardstanding. By redesigning the parking courts so that the majority of the bays are aligned east-west, the revised proposals show a tree lined pedestrian route through the eastern edge of the site, which runs in a continuous line. This is a far more legible arrangement than the refused scheme, allowing a permeable connection from the northern end of the site through to Taylor Street on the

southern boundary, a route that was disjointed as a result of the previous car parking layout.

- 10.9 The introduction of a greater amount of soft landscaping on southern edge of the development following improvements to parking layout is an improvement which further enhances the environment around the footpath link. This enhancement represents a further improvement in the quality of place provided by the revised scheme in relation to the refused development and reduces the dominance of hardstanding areas.

Lack of strong built form to define the character of the development

- 10.10 The revised proposal includes an additional unit, providing active frontage to Taylor Street. This would provide a stronger frontage to the southern end of the development and alongside the improvements to the pedestrian route through the site (as discussed previously), would improve the legibility and surveillance of the connection from the development, on to Taylor Street and through to Hyde Road. The enclosure of this space would also create a sense of a higher density scheme, reflecting the urban grain of surrounding development more effectively than the previously refused scheme.
- 10.11 The revised scheme also proposes a tighter building line on the western edge of the development. The location of the single storey garage on the frontage of the refused scheme undermined the ability to relate to the urban context at this prominent part of the site. The amended scheme proposes a terrace of 3 units and a unit fronting Edward Street on the corner with the access road into the northern part of the site, resulting in a development of a consistent scale, with no significant gaps. This is considered to be an improvement in design terms over the refused scheme which would contribute to the development achieving the quality required by policy E2.
- 10.12 The revised scheme proposes a stronger spine of green space (aligned east-west) in the centre of site, providing a higher quality public realm than the previously refused scheme. The layout now presented results in soft landscaping along the full length of this route and provides greater separation between this area and the car parking spaces at the eastern end of the route. This revision ensures that the purpose of this space is more clearly defined and would be more effective at reducing the prominence of hardstanding from this entrance into the development. The amended scheme would also provide a far more coherent relationship between this space and the north/south spine link through the site, further enhancing the legibility and quality of the public realm.

Inappropriateness of the proposed house types

- 10.13 In addition to the improvements in the layout on the Taylor Street frontage and on the north western edge, the scheme has been amended to include a part 3 storey terrace of 3 units in the north western corner of the site. This would provide a greater scale and presence at a prominent location and would provide a clear visual link to the development on the western side of Edward Street, where the same unit would be used to treat the north eastern corner of that plot. This revision is considered to represent a significant improvement on the previously refused scheme
- 10.14 Following the above assessment, it is considered that the design of the revised scheme has addressed the weaknesses of the refused scheme and would be of a standard that would comply with policy E2 and the requirements of the NPPF in regard to design.

11. RESIDENTIAL AMENITY

- 11.1 The outline planning permission established the principle of development for up to 56 dwellings on the site. This reserved matters application proposes a slightly lower density of development and indicates that sufficient separation distances could be retained between

the edge of the development and the neighbouring residential properties on Osborne Road to the east to achieve the space standards set out in the Residential Design Guide (RDG). The fact that Edward Street bisects the wider site previously occupied by Oldham Batteries ensures that sufficient separation would also be provided between the western edge of the site that is the subject of this application and the eastern edge of the scheme that this the subject of application ref. 18/00306/REM.

- 11.2 The arrangement of plots within the development would allow separation distances to be retained that would achieve the guidance within the RDG (21 metres between elevations with habitable room windows and 14 metres between blank elevations and elevations with openings.) There is no reason to suggest that the boundary treatments of the properties on Osborne Road would be affected by the proposals. Retention of the existing treatment along the eastern boundary of the site can be secured by condition.
- 11.3 Following the above assessment it is considered that the proposals would not have an adverse impact on the residential amenity of the neighbouring properties and would adequately preserve the amenity of future occupants of the development.

12. HIGHWAY SAFETY

- 12.1 At the outline stage, the Transport Assessment submitted with the application was considered sufficient to demonstrate that the number of trips generated by 56 dwellings would not have a severe impact on the capacity of the road network and would therefore not be prejudicial to highway safety. Given that this scheme proposed 1 dwelling less, it is considered that the same assessment would apply.
- 12.2 The proposed plans indicate that 2 car parking spaces would be provided for most dwellings, some limited to 1 space although the scheme includes some 1 bed units. The proposals would therefore meet the requirements of the RDG. As highlighted by the response from TfGM to the outline planning application, the site is considered to be within close proximity to sustainable means of transport which would provide an alternative to making journeys via the private car. There are existing bus stops on the A57, within close proximity of the site, which are served by the regular service from Mottram to Manchester Piccadilly. Hyde Central Train Station is also within a bus journey and short walk from the site.
- 12.3 Given this context, it is considered that the level of parking to be provided within the development would not result in a reliance on on-street parking, ensuring that there would not be an unreasonable impact on highway safety in this regard. In terms of connectivity to the surrounding area, pedestrian links would be provided via the footway connections to Edward Street, in the north western and south western sections of the development and to Taylor Street on the southern boundary (which is considered to have improved significantly in this revised scheme). These factors, alongside the information required to discharge the Travel Plan condition attached to the outline planning permission would ensure that the development would include provision for sustainable modes of transport.
- 12.4 Highways England has not objected to the application and did not object to the principle of development at the outline stage. Conditions requiring details of the surfacing of driveways, the provision of pedestrian visibility splays and requiring all car parking to be laid out prior to the occupation of the development can be added to the decision notice.

13. FLOOD RISK

- 13.1 The site is in Flood Zone 1 and is therefore considered to be at a lower risk of flooding. United Utilities have raised no objections to the proposals subject to the means of drainage

being laid out in accordance with the details submitted with that application. Condition 4 of the planning permission requires a surface water drainage strategy to be submitted and approved prior to the first occupation of the development. The applicant confirmed on the application form at the outline stage that foul water would be drained from the development via a connection to the existing main sewer.

- 13.2 It is considered that the proposed development would not result in a harmful impact in relation to flood risk.

14. ENVIRONMENTAL HEALTH

- 14.1 The Borough's Environmental Health Officer has raised no objections to the proposals, subject to the imposition of conditions limiting the hours of operation and deliveries during the construction phase of the development and compliance with the mitigation measures detailed in the Air Quality Assessment submitted with the outline planning application. These conditions were imposed on the outline planning permission (conditions 7 and 11) and therefore would not need to be re-imposed on a reserved matters approval, had all other material considerations been satisfied.
- 14.2 The applicant has submitted a Noise Assessment with the reserved matters application. This report indicates that a number of plots would require mitigation to be installed in order to minimise the impact of the noise generated by the traffic on the M67 and Lance Corporal Andrew Breeze way to the north of the site, Edward Street to the west and Hyde Road to the south. Within the parcel to the east of Edward Street that is the subject of this application, the report suggests that 3 metre high acoustic fencing would be required along the northern boundary of the site, adjacent to the motorway. Furthermore, a number of the plots would need to be fitted with high specification glazing and mechanical ventilation systems that would allow air circulation without the need to open windows. Further details of the specifications of the glazing and the acoustic fencing can be attached to the decision notice.
- 14.3 The MP raised concerns in relation to previous application the regarding the history of ground contamination on the site and the impact that disturbance of the contamination may have on the living conditions of the future occupants of the development. A condition requiring the submission of an investigation into potential sources of contamination on the site, the agreement of a remediation strategy and the implementation of this strategy prior to the commencement of development was attached to the outline planning application. This matter does not therefore need to be revisited at this reserved matters stage.

15. OTHER MATTERS

- 15.1 As a result of the content of the Bat and Great Crested Newt surveys submitted with the outline planning application, no further survey work or mitigation measures is required in relation to the preservation of these species. A number of conditions are recommended by GMEU including securing biodiversity enhancements through the redevelopment of the site, control of invasive species and the timing of tree/shrub removal from the site. These can be added to the decision notice.
- 15.2 The proposed species mix to be planted in the soft landscaping scheme and the materials to be used in relation to hard landscaping are considered to be acceptable following revisions to the parking arrangements and the quality of the public realm as discussed in detail above.
- 15.3 In relation to crime impact, whilst the scheme does include some communal car parking areas, these are considered to be well surveyed and the submission and approval of the

details of external lighting of these areas is required by condition 18 of the outline planning permission. Areas of public space would be overlooked by the principal elevations of dwellings. Greater Manchester Police have not raised any objections to the proposals. A condition can be attached to the planning permission requiring the submission of details to be incorporated within the development to reduce the risk of crime and requiring Secured by Design accreditation to be achieved.

16. CONCLUSION

- 16.1 The site plays a key role in the regeneration of Denton, as demonstrated by the designation of the site as a Development Opportunity Area. The revised proposal is considered to address the weaknesses of the previously refused scheme by introducing an additional unit on the Taylor Street frontage, increasing the scale of buildings in key locations and presenting a higher quality layout which better reflects the urban grain of the surrounding area. As a result, the proposals are considered to achieve the objectives of policy E2 of the UDP.
- 16.2 There are no objections from any of the statutory consultees and the proposals are considered to preserve the residential amenity of neighbouring properties as well as the future occupants of the development, maintain highway safety and be acceptable in relation to all other material considerations.
- 16.3 The revised proposals are therefore considered to comply with the relevant national and local planning policies quoted above.

RECOMMENDATION

Grant planning permission, subject to the following conditions:

1. The development hereby approved shall be carried out in accordance with the following approved plans/details:

1:1250 Site location plan (Drawing no. 1536WHD/ESD/LP02 Rev. A)
Proposed site layout plan (Drawing no. 1536WHD/ESD/PL02 Rev. W)
Proposed landscape layout plan (Drawing no. 105 Rev. D)
Proposed Planting Plan (sheet 2 of 3) (Drawing no. 202) (insofar as it relates to this application.)
Proposed Planting Plan (sheet 3 of 3) (Drawing no. 203) (insofar as it relates to this application.)
Proposed hardworks plan (sheet 2 of 4) (Drawing number 102 Rev. D)
Proposed hardworks plan (sheet 4 of 4) (Drawing number 104 Rev. D)
Proposed house types A and B plans and elevations (Drawing no. 1000-A&B)
Proposed house type Langley B floor plans and elevations (1536/WHD/ESD/LA02 Rev. C)
Proposed 'Baird' contemporary (Gable fronted) plans and elevations (Drawing no. 1536WHD/ESD/BA01 Rev. A)
Proposed 'Bell' contemporary (Gable fronted) plans and elevations (Drawing no. 1536WHD/ESD/BE02 Rev. A)
Proposed 'Bell' contemporary (End Gable) plans and elevations (Drawing no. 1536WHD/ESD/BE01 Rev. A)
Proposed 'Baird' contemporary (End gable) plans and elevations (Drawing no. 1536WHD/ESD/BA02 Rev. A)
Proposed 'Claydon' contemporary plans and elevations (Drawing no. 1536WHD/ESD/CL01 Rev. A)
Proposed 'Brancaster' Contemporary (Side aspect) plans and elevations (Drawing no. 1536WHD/ESD/BR01 Rev. B)

Proposed 'Chinley' apartments plans and elevations (Drawing no. 1536WHD/ESD/CH02)
Proposed 'Telford' contemporary plans and elevations (Drawing no. 1.136/P/B/G)
Proposed 'Jenner' contemporary plans and elevations (Drawing no. 1536WHD/ESD/JE01 Rev. A)
Proposed 'Jenner' contemporary (Gable fronted) plans and elevations (Drawing no. 1536WHD/ESD/JE02 Rev. A)
Proposed 'Wordsworth' contemporary plans and elevations (Drawing no. 1536WHD/ESD/WO02 Rev. A)
Proposed 'Wordsworth' (Side aspect- contemporary) plans and elevations (Drawing no. 1536WHD/ESD/WO01 Rev. A)
Proposed 'Wren' contemporary plans and elevations (Drawing no. 1536WHD/ESD/WR01 Rev. A)

2. No development shall commence until the following details have been submitted to and approved in writing by the Local Planning Authority:
 - scaled plans showing the exact location and elevations of the acoustic fencing to be installed within the development site and a manufacturers specification of the fencing; and
 - scaled plans showing the location of windows to be treated with high specification glazing and mechanical ventilation and manufacturers specifications of each of the noise mitigation measures to be installed.

The noise mitigation measures shall be implemented in accordance with the approve details, prior to the first occupation of any of the dwellings and shall be retained as such thereafter.

3. Notwithstanding any description of materials in the application, no above ground construction works shall take place until samples and/or full specification of materials to be used: externally on the buildings; in the construction of all boundary walls, fences and railings; and, in the finishes to all external hard-surfaces have been submitted to, and approved in writing by, the local planning authority. Such details shall include the type, colour and texture of the materials. Development shall be carried out in accordance with the approved details.
4. The car parking spaces to serve the development hereby approved shall be laid out as shown on the approved Proposed site layout plan (Drawing no. 1536WHD/ESD/PL02 Rev. W), prior to the occupation of any of the dwellings and shall be retained free from obstruction for their intended use at all times thereafter.
5. Boundary treatments to be installed in accordance with the details as shown on the following approved plans (insofar as they relate to the application site):

Proposed hardworks plan (sheet 2 of 4) (Drawing number 102 Rev. D)
Proposed hardworks plan (sheet 4 of 4) (Drawing number 104 Rev. D)

prior to the first occupation of any of the dwellings hereby approved. Such boundary treatments shall be retained at all times thereafter.

6. Hard and soft landscaping to be installed in accordance with the details as shown on the following approved plans (insofar as they relate to the application site):
 - Proposed landscape layout plan (Drawing no. 105 Rev. D)
 - Proposed hardworks plan (sheet 2 of 4) (Drawing number 102 Rev. D)
 - Proposed hardworks plan (sheet 4 of 4) (Drawing number 104 Rev. D)
 - Proposed Planting Plan (sheet 2 of 3) (Drawing no. 202) (insofar as it relates to this application.)

Proposed Planting Plan (sheet 3 of 3) (Drawing no. 203) (insofar as it relates to this application.)

prior to the first occupation of any of the dwellings hereby approved.

7. The approved scheme of landscaping scheme shall be implemented before the first occupation of any part of the development or in accordance with a programme agreed previously with the local planning authority. Any newly planted trees or plants forming part of the approved scheme which, within a period of 5 years from the completion of the planting, are removed, damaged, destroyed or die shall be replaced in the next appropriate planting season with others of similar size and species.
8. Prior to the occupation of any part of the development hereby approved, a scheme detailing the measures to be incorporated in an application to obtain Secured by Design accreditation to Greater Manchester Police shall be submitted to and approved in writing by the Local Planning Authority. Written confirmation that the accreditation has been achieved shall be submitted to and approved in writing by the Local Planning Authority and the approved measures shall be implemented prior to the first occupation of any part of the development. The development shall be retained as such thereafter.
9. No part of the development hereby approved shall be occupied until details of the means of storage and collection of refuse generated by the development have been submitted to and approved in writing by the Local Planning Authority. The details shall include scaled plans showing the location of storage and the means of enclosure. The bin storage arrangements for each dwelling shall be implemented in accordance with the approved details prior to the occupation of that dwelling and shall be retained as such thereafter.
10. None of the dwellings hereby approved shall be occupied until details of the provision of secured storage for bicycles within each of the plots (minus those plots with garages) of the development hereby approved have been submitted to and approved in writing by the Local Planning Authority. The cycle storage shall be provided for each dwelling in accordance with the approved details prior to the occupation of that dwelling and shall be retained as such thereafter.
11. No development above ground level shall commence until details of Biodiversity enhancement measures to be installed as part of the development hereby approved has been submitted to and approved in writing by the Local Planning Authority. The details shall include a specification of the installations and scaled plans showing their location within the development. The approved enhancement measures shall be installed in accordance with the approved details, prior to the first occupation of any of the dwellings and shall be retained as such thereafter.
12. The existing boundary treatment on the eastern boundary of the site (with Osborne Road) on the date of this shall be retained unless a replacement boundary treatment and details of timescales for installation have been first submitted to and approved in writing by the Local Planning Authority. Any replacement boundary treatment shall be implemented in accordance with the approved details and retained as such thereafter.
13. No tree felling or vegetation removal shall take place during the optimum period for bird nesting (March to July inclusive) unless otherwise agreed in writing with the Local Planning Authority.

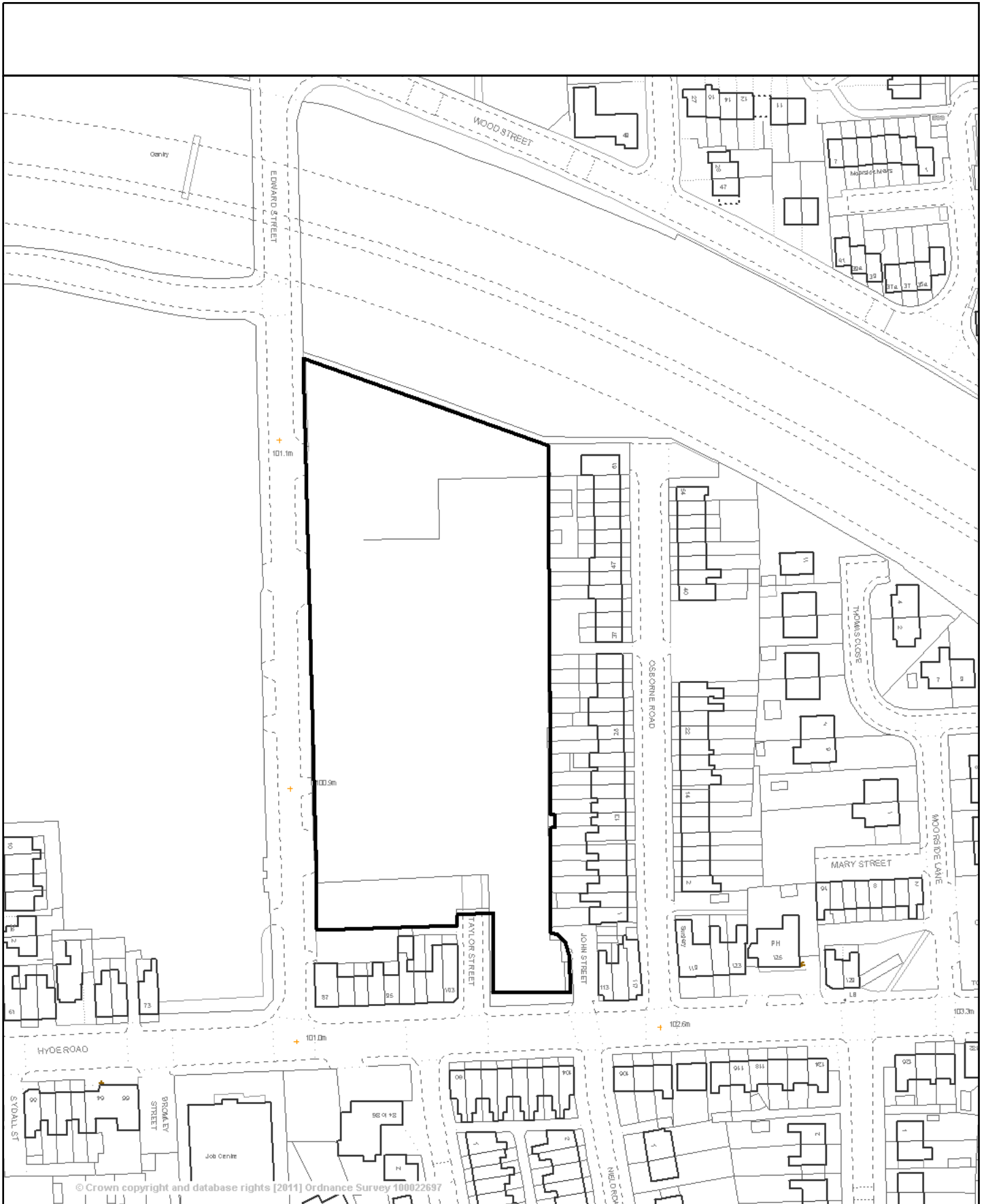
14. No development shall commence unless and until a Method Statement is submitted to and approved in writing by the Local Planning Authority detailing how Japanese Knotweed and any other invasive species on the site will be removed from the site. The development shall thereafter proceed in strict accordance with the approved Method Statement.
15. The driveways to serve the dwellings hereby approved shall be constructed from a bound material and shall be constructed on a level that prevents the displacement of material and surface water onto the highway. The development shall be retained as such thereafter.
16. Prior to the occupation of any part of the development hereby approved, visibility splays shall be provided on both sides of the site access where it meets the footway. The visibility splays shall measure 2.4 metres along the edge of the site access and 2.4 metres along the footway. It must be clear of anything higher than 600mm above ground level. The visibility splays shall be retained as such thereafter.

Reasons for conditions:

1. For the avoidance of doubt
2. To protect the amenities of future occupants from external noise in accordance with UDP policy H10.
3. To ensure that the appearance of the development reflects the character of the surrounding area.
4. To ensure that the development is served by adequate parking provision.
5. To protect the amenities of occupants of nearby properties/dwelling houses in accordance with UDP policies 1.12 and E6.
6. To ensure that sufficient hard and soft landscaping are implemented to ensure that the overall development respects the character of the surrounding area.
7. To ensure that the approved landscaping scheme is adequately maintained.
8. To ensure that the development is designed to minimise opportunities for crime.
9. To provide adequate secure bin storage to serve the development and to safeguard the general amenity of the area in accordance with UDP policy 1.12/1.13/H10.
10. To ensure cycle storage is provided to enhance the environmental sustainability of the development.
11. To ensure biodiversity enhancements are secured to mitigate the environmental impacts of the scheme.

12. To ensure that the proposals preserve the residential amenity of neighbouring properties.
13. In order to prevent any habitat disturbance to nesting birds in accordance with the National Planning Policy Framework.
14. To ensure that invasive species are removed from the site appropriately.
15. To ensure that the development maintains highway safety.
16. To ensure that the development maintains highway safety.

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Scale 1/1500 Date 15/5/2018

Centre = 392924 E 395631 N

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Application Number: 17/00512/REM Land East of Edward street Denton

Photo 1 – south western corner of site from Hyde Road



Photo 2 – view looking south from north western corner of the site along Edward Street



Photo 3 – central part of the site from Edward Street (along western boundary)



Photo 4 – central part of the site from Edward Street



Photo 5 – looking north along Edward Street (towards north western corner of the site)



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Edward Street, Denton

SITE LAYOUT



Mix Schedule	
Plots 146 - 200 (Phase 2)	
Baird (BAI)	6
Bell (BEL)	20
Brancaster-SA (BRA-SA)	2
Chinley Apartments (CHI)	6
Claydon-SA (CLA-SA)	1
Jenner Gable (JEG)	4
Jenner Special (JES)	1
Wordsworth (WOR)	1
Wordsworth-SA (WOR-SA)	4
Wren (WRE)	3
Langley (LAN)	3
Telford (TEL)	1
Type A (A)	2
Type B (B)	1
TOTAL	55

Mix Schedule	
Plots 1 to 145 (Phase 1)	
Apartments 1-14 (Ap 1-14)	14
Baird (BAI)	25
Bell (BEL)	14
Brancaster (BRA)	7
Brancaster-SA (BRA-SA)	1
Claydon (CLA)	1
Claydon-SA (CLA-SA)	2
Haversham (HAV)	2
Haversham-SA (HAV-SA)	3
Jenner Gable (JEG)	18
Jenner (JEN)	2
Newton (NEW)	3
Telford (TEL)	6
Wordsworth (WOR)	8
Wordsworth-SA (WOR-SA)	5
Wren (WRE)	1
Langley (LAN)	4
Type A (A)	7
Type B (B)	4
Type C (C)	10
Type D (D)	4
Type E (E)	4
TOTAL	145

Rev.	Date	Revision Description	Drawn
V	13-4-18	Amendments following Case Officer Comments / Meeting with applicant 12/4	DGL
T	19-3-18	Amendments throughout the site, 145 Plots	DGL
S	23-1-18	Minor amendments following client comments to finalise for re-submission to LA	DGL
Rv5	19-1-18	North Eastern Corner of Phase 1 amended + Further tweaks to mix	DGL
P	8-1-18	Preliminary Issue Rev/P	DGL
Nv2	5-1-18	Preliminary Issue Rev/Nv2	DGL
M	25-10-17	Redesigns to plots 71/72, 10, 23-26, 93 to 107 and 156/157	DGL
Lv2	13-10-17	Various amendments in dealing with comments 31/10/17 from SW	DGL
K	05-09-17	2nd Major re-plan following discussions with OH	DGL
Nv2	17-7-17	Major re-plan following second Planning Meeting (Draft Rev J V2)	DGL
H	2-7-17	Various re-design amendments following meeting with LA / discussions with the applicant	DGL
Qv2	16-4-17	Jenner (Special) and Claydon SA types clarified on Layout and Mix, Scott Type updated	DGL
F	24-05-17	Further amendments to parking courts following comments from Landscape Arch / Handings checked and SA / DA types clarified on layout and schedule	DGL
E	09-05-17	Railings / Railings with Brick Piers added to site frontages and footpath links	DGL
D	05-05-17	Highway amendments prior to submission	DGL
C	24-04-17	Parking Courts revised, additional screening added	DGL
B	16-03-17	Northern Access Point added	DGL
A	01-02-17	Nelson house type bay position updated	DGL
Rev.	Date	Revision Description	Drawn
W	24-4-18	Plots 147 to 149 changed to types AB	DGL

Legend

- 68 Site Boundary (Red Edge)
- Existing Trees & Hedges Retained
- Existing Trees & Hedges Removed/Reduced shown Dashed
- Indicative New Tree Planting and Hedgerows
- 1.8m High Timber Screen Fencing (Double Line)
- 900mm High Brick Wall with Brick Piers and Timber Infill Panels (Thick Dashed Line)
- 1.2m High Metal Railings between Brick Piers
- 1.2m High Metal Railings alongside proposed footpath links as shown
- 1.8m High Brick Wall with Brick Piers (Solid Line)
- 1.8m High Timber Close Boarded Plot Divisional Fencing (Dashed Line)
- Gated Access to Rear of Properties (Lockable) Arrows Denote Door Entry Positions into Dwellings
- Shared Private Drive Areas (Bitmac with Rumble Strip Sets)
- Key Junctions and Speed Tables (Red Block paved - Transitional elements between highway materials can be tegula or cobbled sets) Key spaces - Materials to be agreed with Tree Grids as shown
- Refer to Hardworks Plans produced by tpm landscape for specific design detail to these areas



Barn Meadow House
 Southfield Fold Farm
 Southfield
 Burnley
 Lancashire
 BB10 3RH
 Tel : 01282 601157
 Mbl : 07976 782876

Revision	Drawn	Checked
W	D.S.Lever	Approved
	Scale: 1:1000 @ A3 1:500 @ A1	24th Jan 2017
	Drawing No. 1536WHD/ESD/PL01	
	E-mail: darren.dgla@gmail.com	A1

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- Application No:** 17/00856/FUL
- Proposal:** Erection of a detached dwelling house following demolition of existing dormer bungalow, garage and car port.
- Site:** Spring Haven, 61 Gibraltar Lane, Denton
- Applicant:** Mr Mustapha Matib
- Recommendation:** Grant Planning permission subject to conditions
- Reason for report:** Councillor Claire Reid has requested that the application be determined by Members of the Speakers Panel (Planning) on behalf of a local resident who is concerned regarding loss of privacy and visibility.

REPORT

1. APPLICATION DESCRIPTION

- 1.1 The application seeks planning permission to erect a detached dwellinghouse following the demolition of the existing bungalow, garage and car port.
- 1.2 Since the original submission, the application has been amended to include a revised siting and set back from Gibraltar Lane, the removal of several windows at first floor level and an amended 'hipped' roof design on the single storey part of the scheme.
- 1.3 The proposed dwelling house (as amended) has an 'L' shaped footprint and is part two storey and part single storey in height. The accommodation comprises of a kitchen, family lounge, guest lounge, hall, utility, w.c, gym/pool and double garage at ground floor with 4 bedrooms (all en suite) and a study at first floor level. The proposed materials of the dwelling house are rendering with exposed stone quoins at corners and returns with a tiled roof.
- 1.4 The proposal will utilise the existing access from Gibraltar Lane into the site and a new driveway in front of the proposed dwelling house will provide parking for two vehicles.
- 1.5 A bin storage area will be provided close to the site frontage on Gibraltar Lane.
- 1.6 The applicant has provided the following documents in support of the planning application:

Proposed and Existing Plans and Elevations
A Coal Mining Risk Assessment
Design and Access Statement

2. SITE AND SURROUNDINGS

- 2.1 61 Gibraltar Lane comprises of a detached bungalow with detached double garage sited on a roughly rectangular shaped parcel of land some 0.11 hectares in area. The property is sited in the north western part of the site area and close to the side and rear boundaries of the site. The detached double garage is sited close to the south eastern boundary of the site. There is a landscaped garden area in front of the property with a hedge boundary adjoining the road frontage. A separate access point from Gibraltar Lane currently provides vehicular access into the site.

- 2.2 The application site is sloping and site falls by approximately 3 – 5 metres from north to south. The highest levels are in the north, east and western parts of the site whilst the lowest ones are along the southern parts (close to the site access).
- 2.3 The site lies within a predominately residential area and is adjoined by 59 and 63 Gibraltar Lane and properties on Sandown Drive and Newport Road to the rear. Residential properties also lie opposite the site.

3. PLANNING HISTORY

- 3.1 16/00784/FUL – Existing Dormer Bungalow roof raised to form 1st floor accommodation, 2 storey side extension and construction of Granny Annexe to replace existing Garage and Carport. Withdrawn on 31.10.16.

4. RELEVANT PLANNING POLICIES

- 4.1 National Planning Policy Framework (NPPF)

- 4.2 Planning Practice Guidance (PPG)

4.3 Tameside Unitary Development Plan (UDP) Allocation

Unallocated, within the settlement of Haughton Green, Denton.

4.4 Part 1 Policies

- 1.3: Creating a Cleaner and Greener Environment.
- 1.4: Providing More Choice and Quality Homes.
- 1.5: Following the Principles of Sustainable Development
- 1.10 Protecting and Enhancing the Natural Environment
- 1.12: Ensuring an Accessible, Safe and Healthy Environment

4.5 Part 2 Policies

- H2: Unallocated Sites
- H4: Type, size and affordability of dwellings
- H10: Detailed Design of Housing Developments
- T1: Highway Improvement and Traffic Management
- T10: Parking
- C1: Townscape and Urban Form
- N4: Trees and Woodland
- N5: Trees Within Development Sites
- MW11: Contaminated Land

4.6 Other Policies

Greater Manchester Spatial Framework - Publication Draft October 2016;
Residential Design Supplementary Planning Document; and,
Trees and Landscaping on Development Sites SPD adopted in March 2007.

4.7 National Planning Policy Framework (NPPF)

- Section 1 Delivering sustainable development
- Section 6 Delivering a wide choice of high quality homes

Section 7 Requiring good design
Section 8 Promoting healthy communities
Section 11: Conserving and enhancing the natural environment

4.8 **Planning Practice Guidance (PPG)**

4.9 This is intended to complement the NPPF and to provide a single resource for planning guidance, whilst rationalising and streamlining the material. Almost all previous planning Circulars and advice notes have been cancelled. Specific reference will be made to the PPG or other national advice in the Analysis section of the report, where appropriate.

5. **PUBLICITY CARRIED OUT**

5.1 Neighbour notification letters were issued in accordance with the requirements of the Town and Country Planning (Development Management Procedure) (England) Order 2015 and the Council's adopted Statement of Community Involvement.

6. **RESPONSES FROM CONSULTEES**

6.1 Borough Contaminated Land Officer: Recommends that a standard contaminated land condition is attached to any planning approval granted for development at the site, requiring the submission and approval of an assessment into potential sources of contamination and a remediation strategy.

6.2 Borough Environment Health Officer: Raises no objections to the proposed development subject to the imposition of conditions limiting the hours of works and deliveries during the construction process.

6.3 Local Highway Authority: Raises no objections to the proposals subject to the imposition of conditions requiring the laying out of the car parking spaces prior to the first occupation of the development, details of measures necessary within the highway to ensure that the proposed access is constructed satisfactorily.

6.4 The Coal Authority: The application has been supported by a Coal Mining Risk Assessment. The Coal Authority concurs with the recommendations of the Coal Mining Risk Assessment Report; that coal mining legacy potentially poses a risk to the proposed development and that intrusive site investigation works should be undertaken prior to development in order to establish the exact situation regarding coal mining legacy issues on the site. The Coal Authority recommends that the LPA impose a Planning Condition should planning permission be granted for the proposed development requiring these site investigation works prior to commencement of development.

6.5 Arboricultural Officer (TMBC): There is a Cherry tree to be removed that is relatively low value. The more significant fir tree and boundary hedge are to be retained and should be protected to BS5837 during works.

7. **SUMMARY OF THIRD PARTY RESPONSES RECEIVED**

7.1 Councillor Claire Reid has contacted the Council on behalf of residents and requested that the dwelling house be '1 metre shorter' than described on the plans.

7.2 4 letters of objection have been received. Main points raised:

- Concern regarding noise and disturbance from plant and machinery associated with the swimming pool;
- General disturbance during the construction period;
- Increased congestion and noted that Gibraltar Lane is well used by pedestrians;
- Intrude on privacy and result in overlooking;
- Noise;
- Light and lack of light;
- Property value will be reduced by 25 – 30% if development goes ahead; and,
- Request visit from Planning Department to discuss concerns.

8. ANALYSIS

8.1 In accordance with the NPPF and Tameside UDP policies H2 and H10, the main issues raised by the application relate to the following:

- The principle of development;
- Impact on the character and appearance of the surrounding area;
- Impact on residential amenity; and,
- Impact on highway safety

8.2 The above matters are considered in more detail below.

9. PRINCIPLE OF DEVELOPMENT

9.1 With regard to the principle of development, the site is unallocated on the UDP Proposals Map and lies within a predominately residential area. Given that that the application proposes a replacement dwelling house, the site falls within the definition of 'previously developed land' as prescribed by the NPPF and as such it is not considered that there are any objections to the redevelopment of the site in broad land use terms. The proposal accords with national and local planning policy advice in this regard.

10. IMPACT ON CHARACTER AND APPEARANCE OF SURROUNDING AREA

10.1 Saved Tameside UDP Policies C1 and H10 together with the NPPF all seek to ensure that any new development respects or improves the character of the surrounding area and adjacent properties in terms of scale, massing, materials, layout, bulk and height.

10.2 The area surrounding the application site is characterised by a mix of detached and semi-detached dwellings which are predominately two stories in height with examples of single storey bungalows and dormer bungalows along parts of Gibraltar Lane. Properties on this part of Gibraltar Lane generally present a linear pattern of development facing towards the highway and tend to be sited within relatively spacious plots.

10.3 The proposed development fronts onto Gibraltar Lane and is considered to be consistent with the existing linear pattern of development in the locality. The proposal would also achieve an appropriate visual relationship with the adjacent properties given its size, scale and design. The scale and mass of the building and particularly the proposed finished floor levels and resultant overall height would mean that the dwelling would not appear prominent in the street scene or in views from the existing residential properties to the rear or side of the site. As a result it is considered that the property can be accommodated on the site without detriment to the character and appearance of the surrounding area.

10.4 The proposed design of the dwelling and materials (including the use of render) are also considered to be acceptable.

11. RESIDENTIAL AMENITY

11.1 The NPPF outlines the importance of planning in securing good standards of amenity for future and existing occupiers of land and buildings. Saved UDP Policy H10 seeks to ensure that new development does not result in any detrimental impact on the residential amenities of existing occupiers through loss of privacy, overshadowing or traffic.

11.2 The Residential Design SPD expands on issues covered by criteria under Policy H10 with, amongst other matters, the requirement of new development to maintain adequate separation distances between proposed and existing dwellings in order to protect the amenities of future and existing occupiers.

11.3 In this particular case, the main impact from the proposal would be on the occupiers of 59 and 63 Gibraltar Lane and properties to the rear of the site on Sandown Drive. The impact from the proposal on each of these properties is considered in more detail below.

11.4 59 Gibraltar Lane is a detached dormer bungalow which lies adjacent to the site. The rear elevation of this property faces the side (western) elevation of the proposed dwelling house. There are main windows in the rear elevation of 59 Gibraltar Lane and at ground floor level in the side (western) elevation of the proposed dwelling. The three windows at first floor level in the side (western) elevation of the proposed dwelling house serve two en suite bathrooms and a study (high level window). The proposed dwelling house is sited at a lower level to 59 Gibraltar Lane with the finished floor levels being approximately 4 metres below that of this property.

11.5 The separation distance between the proposed dwelling house and 59 Gibraltar Lane is approximately 17 metres. Whilst the Council's guidelines laid down in SPD Policy RED2 would normally require a separation distance of 21 metres, given the site levels and proposed window arrangement at first floor level, it is not considered that there will be any undue overlooking or loss of privacy resulting from this part of the proposal. It is recommended that a condition be attached requiring the use of obscure glazing to all en suite bathroom to protect the privacy of adjoining occupiers.

11.6 63 Gibraltar Lane is a detached dormer bungalow which lies adjacent to the side boundary of the application site. There is a main window in the side gable of this property at first floor level. The separation distance between this window and windows in the side (eastern) elevation of the proposed dwelling house is approximately 18 metres. Given the angle between these windows, this relationship is acceptable.

11.7 The rear boundary of the site adjoins properties on Sandown Drive. The rear (northern) elevation of the proposed dwelling house contains windows at ground and first floor level which directly face the rear elevation of properties on Sandown Drive. The separation distance between these properties and the proposed dwelling house is 25 metres which is acceptable and in complete accordance with the Council's guidelines.

11.8 Part of the side (western) boundary of the site adjoins the rear garden areas of properties on Newport Road. The separation distance between these properties and the proposed dwelling house is 23 metres which is acceptable and in complete accordance with the Council's guidelines.

11.9 The proposed property is more than 14 metres away from properties opposite the site on Gibraltar Lane which is acceptable in accordance with the Council's guidelines.

11.10 The proposal will be served by a separate means of access from Gibraltar Lane. There will be a parking area in front of the dwelling. This arrangement is similar to surrounding properties and is not considered to adversely impact on the amenities of surrounding residents.

11.11 The development proposes adequate private amenity space.

12. HIGHWAY SAFETY

12.1 The application proposes to utilise the existing driveway from Gibraltar Lane which will provide off street car parking for approximately two vehicles. In addition, the proposed dwelling has an integral double garage which will provide space for a further two vehicles. The proposed access and parking arrangements are acceptable and no objections have been raised to this part of the proposal from the Head of Environmental Services (Highways). It is recommended that the driveway be constructed in permeable materials. This can be secured via a planning condition.

13. CONCLUSION

13.1 At the heart of the NPPF is a presumption in favour of sustainable development which should be seen as a golden thread running through decision taking. Paragraph 7 of the NPPF establishes three dimensions to sustainable development – economic, social and environmental and paragraph 8 of the NPPF indicates that these should be sought jointly and simultaneously through the planning system.

13.2 The site is unallocated on the UDP Proposal Map and within the defined urban area with an accessible and sustainable location. As such the principle of residential development on the site is considered acceptable. The layout and design of the proposed development has been assessed and is considered acceptable, in terms of its impact on the visual amenity of the area and the residential amenity of surrounding properties. The traffic generation, access and parking arrangements are also considered to be acceptable.

13.3 In view of the above, the proposal is considered to comply with relevant saved UDP Policies and the SPD on Residential Design. In considering the planning merits of the proposal against the requirements of the NPPF, the proposal is considered to represent sustainable development.

13.4 On this basis in accordance with the requirements of Section 38 (6) of the Planning and Compulsory Purchase Act 2004, the application is recommended for approval.

14. RECOMMENDATION

Grant planning permission subject to the following conditions:

- 1) The development must be begun not later than the expiration of three years beginning with the date of this permission.
- 2) Samples of all external materials shall be submitted to and approved in writing by the Local Planning Authority before any work commences. The development shall be constructed with such approved materials.
- 3) No works other than the excavation of the foundations and/or piling works for the development shall be undertaken at the site until the CLS2A Contaminated Land

Screening Form has been submitted to, and approved in writing by, the Councils Environmental Protection Unit (EPU). Where necessary, a scheme to deal with any contamination / potential contamination shall be submitted to, and approved in writing by the EPU. The scheme shall be appropriately implemented and a completion report demonstrating this and that the site is suitable for its intended use will be approved in writing by the EPU prior to occupation. The discharge of this planning condition will be given in writing by the Local Planning Authority (LPA) on completion of the development and once all information specified in this condition has been provided to the satisfaction of the EPU.

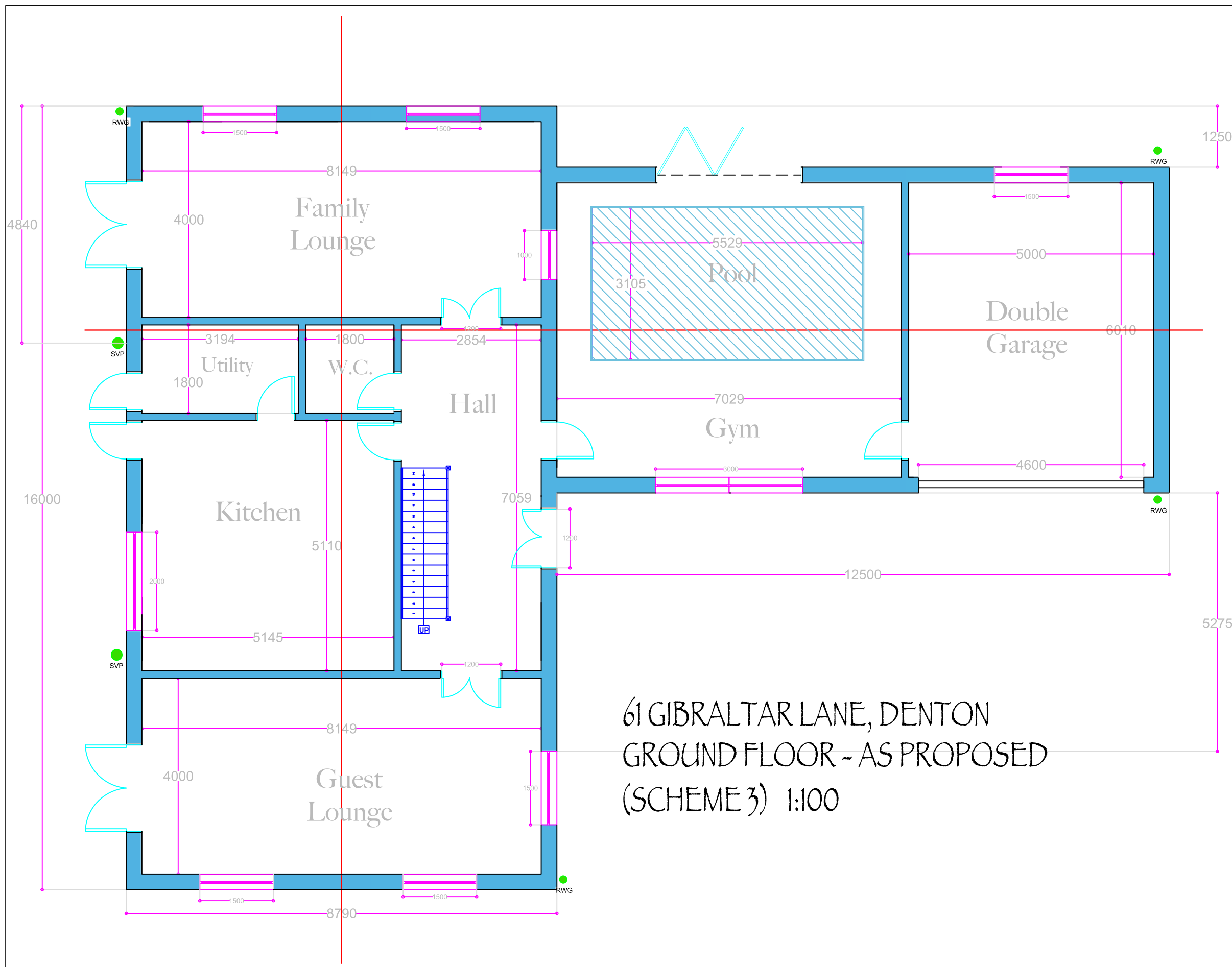
- 4) Prior to occupation, the car parking indicated on the approved plan shall be provided to the full satisfaction of the Local Planning Authority and thereafter kept unobstructed and available for its intended purpose. The areas shall be maintained and kept available for the parking of vehicles at all times.
- 5) Prior to the commencement of any development:

details of a scheme of intrusive site investigations, including gas monitoring, in order to establish the exact situation regarding coal mining legacy issues on the site shall be submitted to, and approved by, the local planning authority;

any approved scheme shall then be undertaken and a report of findings arising from the investigation, including a scheme of any necessary remedial works, shall be submitted to, and approved by, the local planning authority.
- 6) During demolition/construction no work (including vehicle and plant movements, deliveries, loading and unloading) shall take place outside the hours of 07:30 and 18:00 Mondays to Fridays and 08:00 to 13:00 Saturdays. No work shall take place on Sundays and Bank Holidays.
- 7) The dwelling hereby approved shall not be occupied unless and until adequate facilities for the storage and collection of refuse and recyclable materials have been physically provided in accordance with the details indicated on the approved plans.
- 8) Notwithstanding the submitted details and prior to the commencement of any site works for the development hereby approved, details of all existing and proposed slab and site levels (including any re grading proposed to the site) shall be submitted to and approved in writing by the Local Planning Authority and the development shall only be carried out in accordance with those details so approved.
- 9) Notwithstanding the provisions of Classes A, B, C, D and E of Schedule 2, Part 1 of the Town and Country Planning (General Permitted Development) Order 2015 (as amended), or any Order revoking and re-enacting that Order, the dwelling hereby approved shall not be altered or extended (including any alterations to its roof), no new windows shall be inserted, and no buildings or structures shall be erected within the curtilage of the dwelling unless planning permission has first been approved by the Local Planning Authority.
- 10) All en suite bathroom windows shall be fitted and retained with obscure glass which shall achieve a level of obscurity equivalent to Grade 4 or 5 on the Pilkington Scale. Any subsequent re-glazing shall be in obscure glass which shall achieve the same level of obscurity.
- 11) The new hard surface in front of the dwelling house hereby approved shall be constructed of porous materials or else provision shall be made to direct run-off water from the hard surface to a permeable or porous area or surface within the curtilage of the property.

- 12) Prior to the commencement of the development, a detailed scheme of landscaping shall be submitted to and approved in writing by the Local Planning Authority.
- 13) All planting, seeding or turfing comprised in the approved details of landscaping shall be carried out in the first planting and seeding season following the first occupation of the dwelling or the completion of the development, whichever is sooner: and any trees or plants which within a period of 5 years from the completion of the development die, are removed, or become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species, unless the Local Planning Authority gives written consent to any variation.
- 14) Prior to commencement of work on site the applicant shall undertake a condition and dilapidations survey of the highway fronting the site and giving access to the site and prepare and submit a report to the Engineering Operations Manager. The developer will be responsible for making good any damage caused to the highway by the development works or by persons working on or delivering to the development. Any damage caused to the street during the development period shall be reinstated to the full satisfaction of the Highway Authority prior to the occupation of any part of the development.
- 15) The development hereby approved shall be carried out in accordance with the amended drawings received by the Council on 20 April 2018

130102/001 (Ground Floor Plan As Proposed)
130102/002A (First Floor Plan As Proposed)
130102/003B (SW & SE Elevations As Proposed)
130102/004B (NE & NW Elevations As Proposed)
130102/005 A (Location Plan As Proposed)
130102/006 B (Site Block Plan)
130102/007 B (Site Sections)
130102/008A (Site Layout Plan)



General Notes

**Oldham & Rochdale
Stockport & Tameside**

www.architectural.uk.com
07872 577 565

Project Description:
**New two storey
house**

For:
Mr Mustapha Matib

Site Address:
**61 Gibraltar Lane
Denton, M34 7PY**

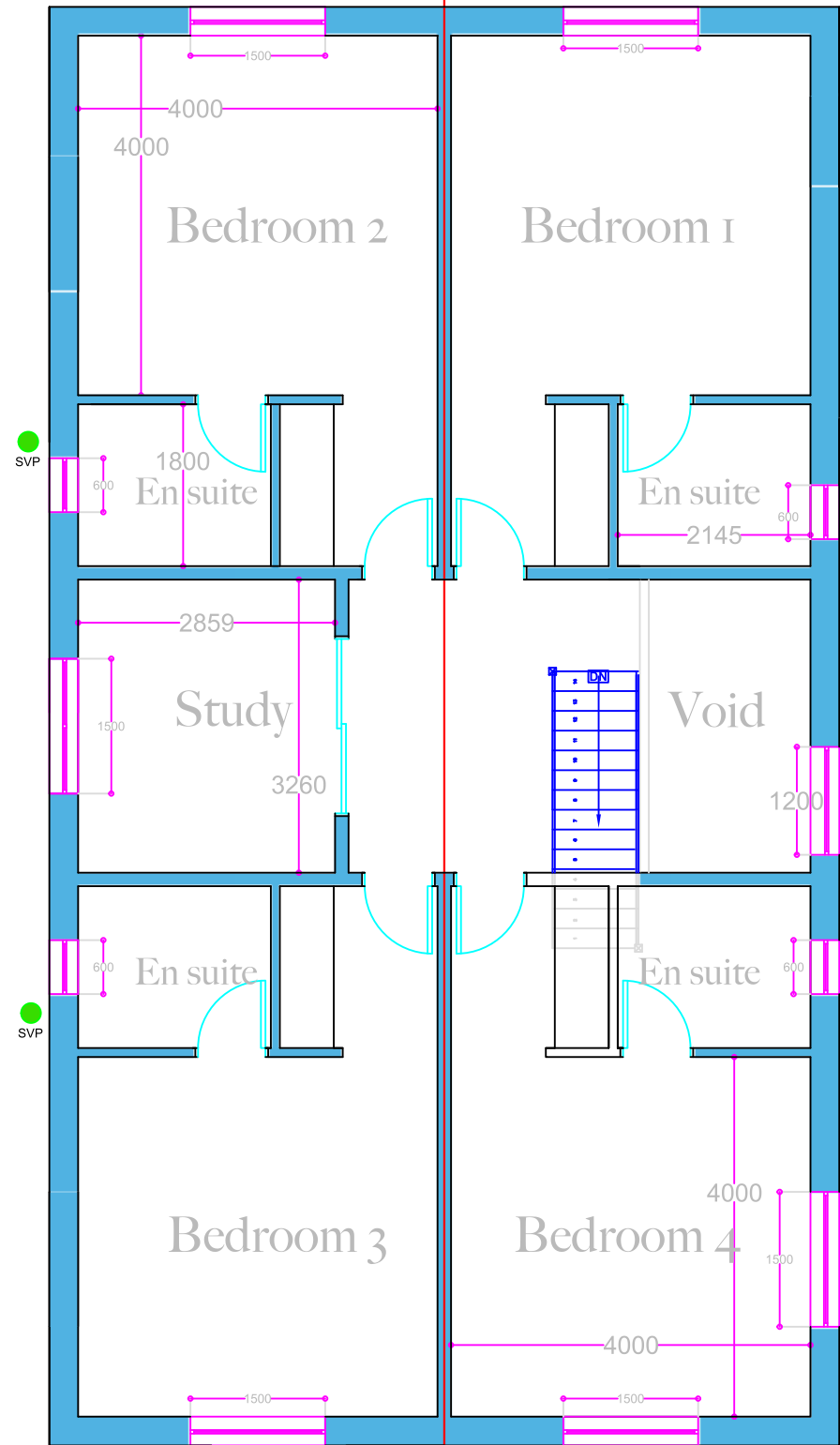
Date: **October 2017**

Scale: **1:100 @ A3**

Sheet Description:
**Ground Floor Plan
As Proposed**

No.	Revision /Issue	Date

Drawing Number:
130102/001



61 GIBRALTAR LANE, DENTON
 FIRST FLOOR - AS PROPOSED
 (SCHEME 3) 1:100

General Notes

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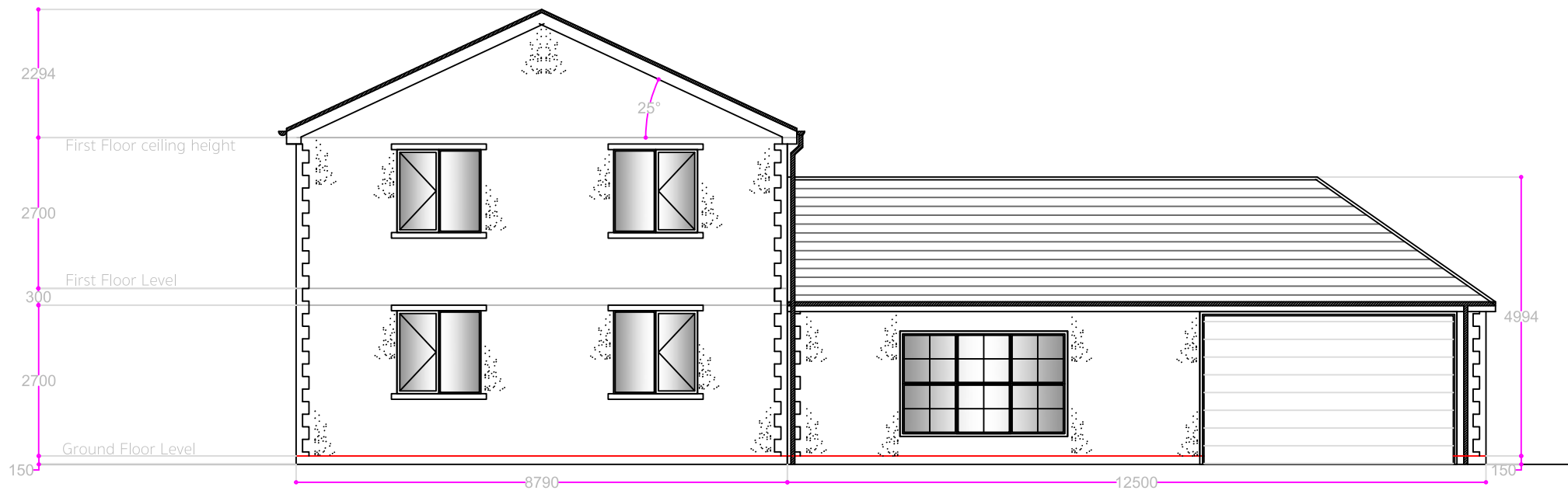
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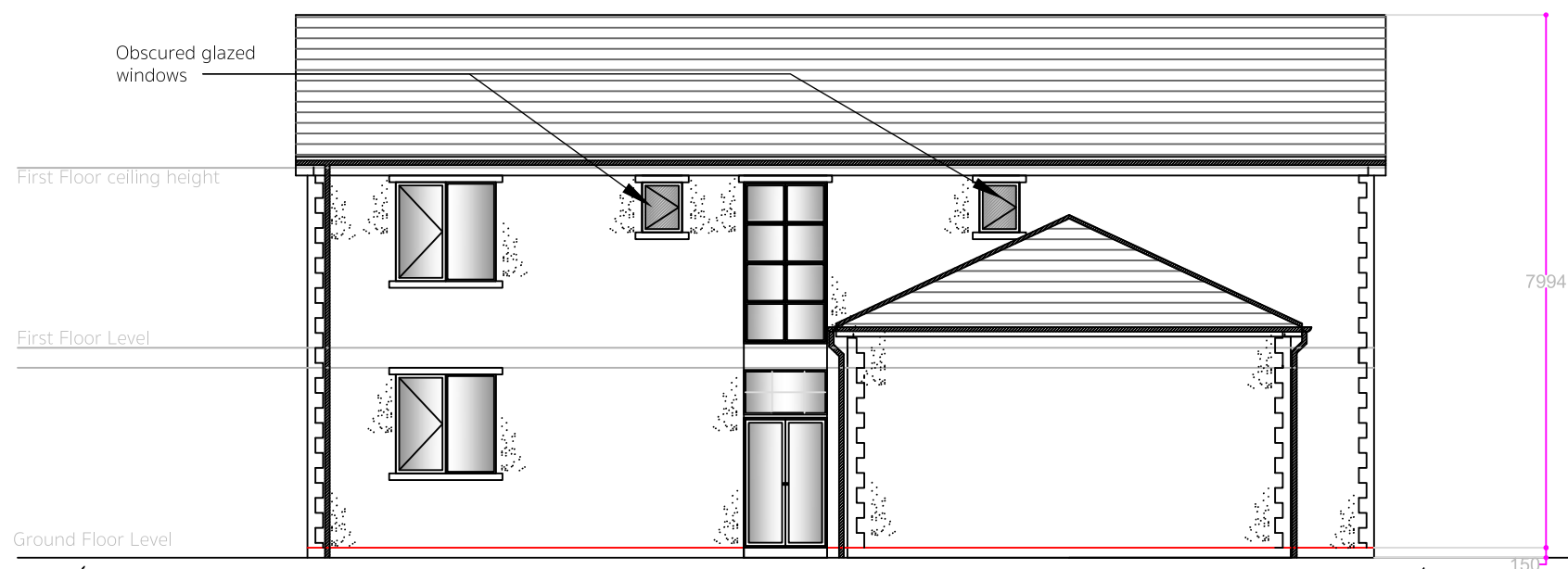
Sheet Description:
 First Floor Plan
 As Proposed

No.	Revision /Issue	Date
A	Window revs. (LPA comments)	20/4/18

Drawing Number:
 130102/002A



61 GIBRALTAR LANE, DENTON SOUTH-WEST ELEVATION - AS PROPOSED - (SCHEME 3)
1:100



61 GIBRALTAR LANE, DENTON SOUTH-EAST ELEVATION - AS PROPOSED - (SCHEME 3) 1:100

General Notes

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Stockport & Tameside**

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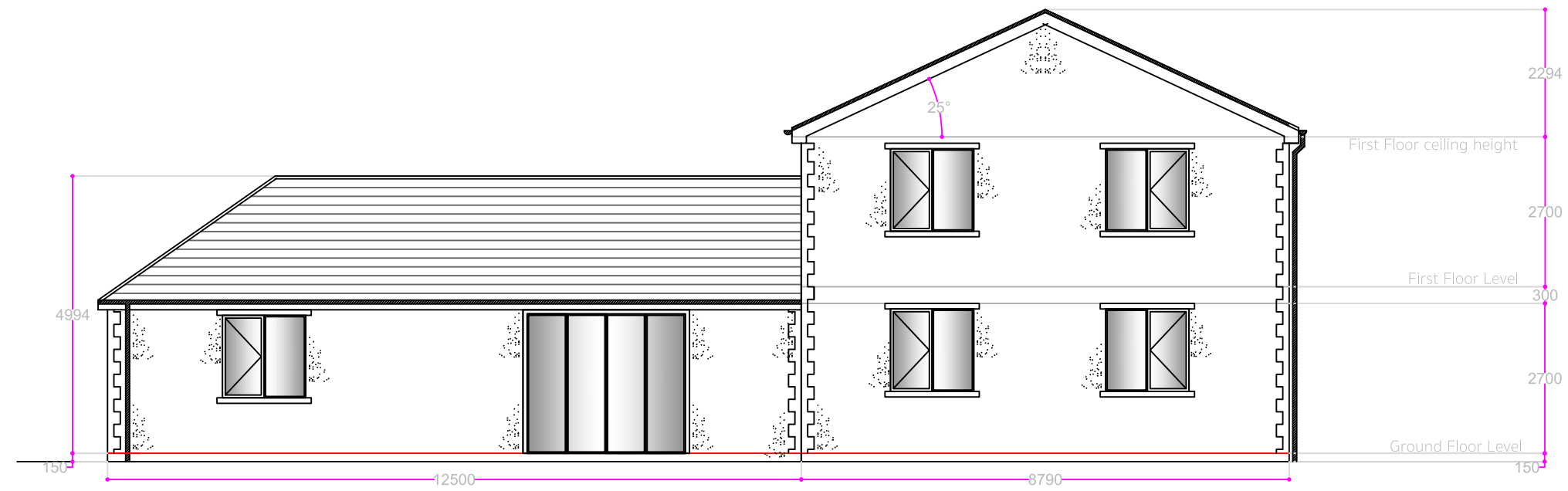
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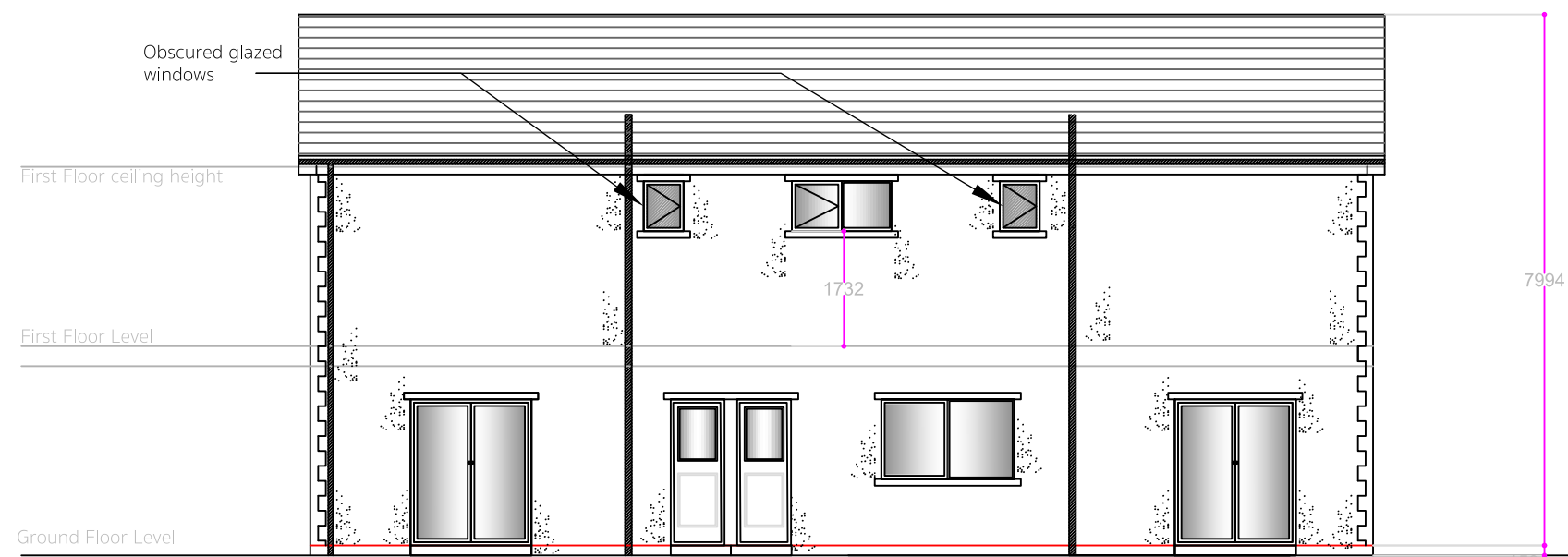
Sheet Description:
**SW & SE Elevations
As Proposed**

No.	Revision /Issue	Date
B	Window revs. (LPA comments)	20/4/18
A	New hipped roof shown	26/3/18

Drawing Number:
130102/003B



61 GIBRALTAR LANE, DENTON NORTH-EAST ELEVATION - AS PROPOSED - (SCHEME 3)
1:100



61 GIBRALTAR LANE, DENTON NORTH-WEST ELEVATION - AS PROPOSED - (SCHEME 3) 1:100

General Notes

**Oldham & Rochdale
Stockport & Tameside**

www.architectural.uk.com
07872 577 565

Project Description:

New two storey house

For:

Mr Mustapha Matib

Site Address:

61 Gibraltar Lane
Denton, M34 7PY

Date:

October 2017

Scale:

1:100 @ A3

Sheet Description:

NE & NW Elevations
As Proposed

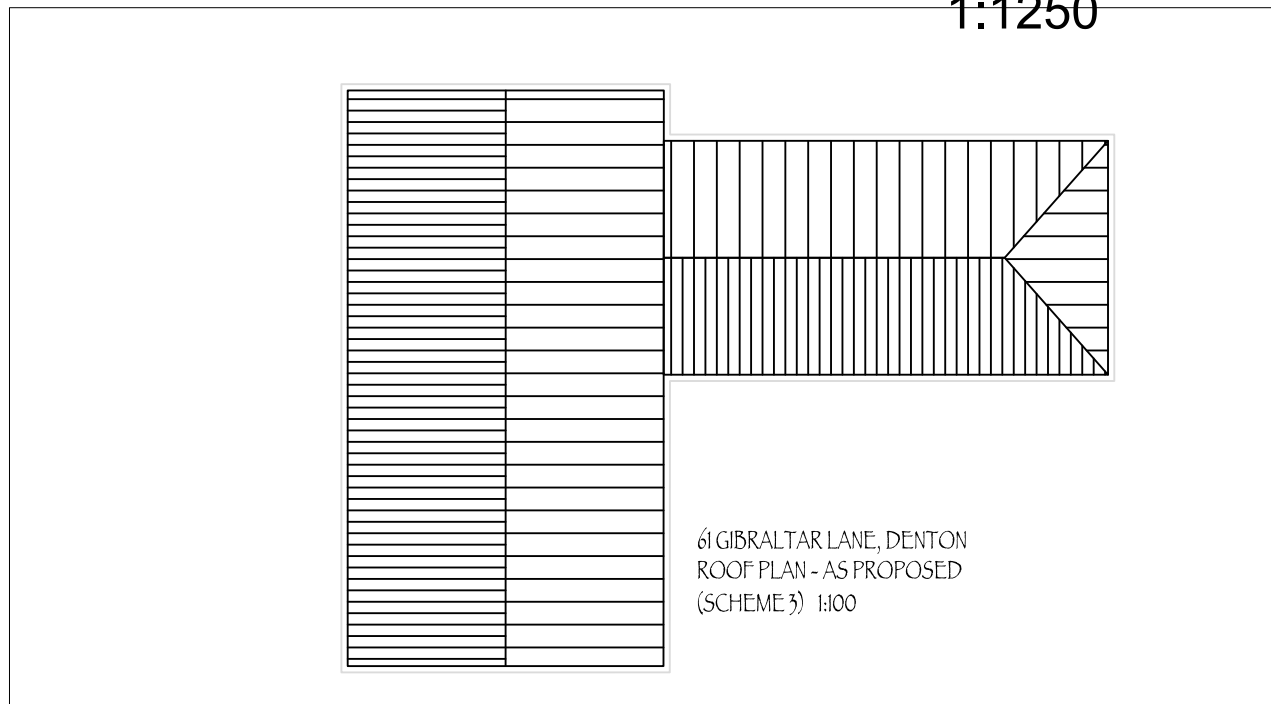
No.	Revision /Issue	Date
A	Window revs, (LPA comments)	20/4/18
A	New hipped roof shown	26/3/18

Drawing Number:

130102/004B



Location Plan
1:1250



General Notes

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Stockport & Tameside**
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07872 577 565

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For:
Mr Mustapha Matib

Site Address:
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Denton, M34 7PY

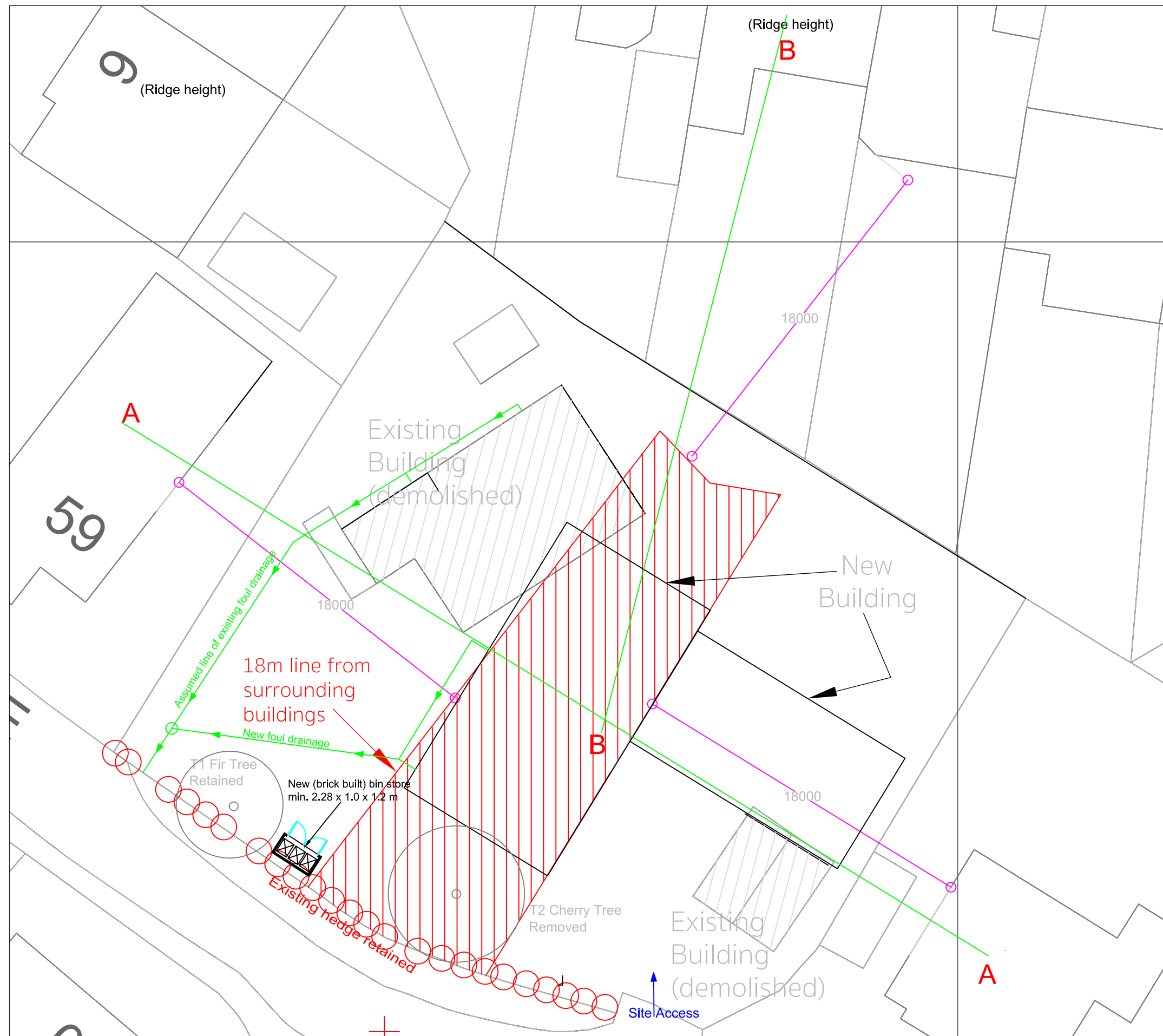
Date: October 2017

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Sheet Description:
Location & Roof Plans
As Proposed

No.	Revision /Issue	Date
A	Revised house posn. & new hipped roof shown	26/3/18

Drawing Number:
130102/005A



General Notes

**Oldham & Rochdale
Stockport & Tameside**
www.architectural.uk.com
07872 577 565

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New two storey house

For:
Mr Mustapha Matib

Site Address:
**61 Gibraltar Lane
Denton, M34 7PY**

Date: **October 2017**

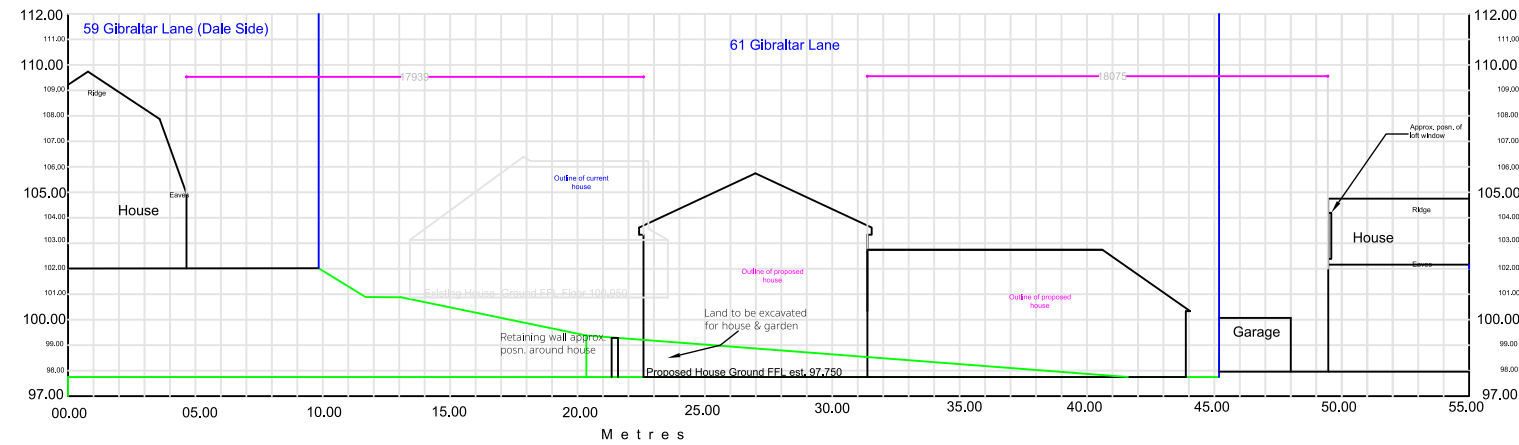
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Sheet Description:
Site Block Plan

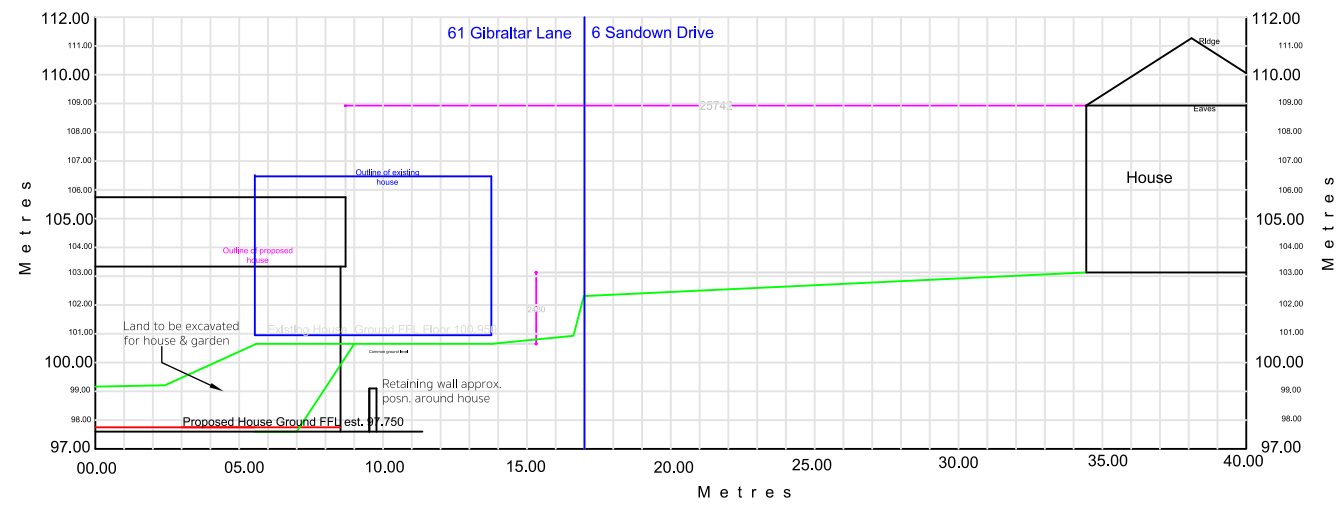
No.	Revision /Issue	Date
B	Revised house posn shown	26/3/18
A	Site section lines added	12/10/17

Drawing Number:
130102/006B

Section 1: A-A 1:250



Section 2: B-B 1:250



General Notes

**Okham & Rochdale
Stockport & Tameside**

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07872 577 565

Project Description:
**New two storey
house**

For:
Mr Mustapha Matib

Site Address:
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Denton, M34 7PY**

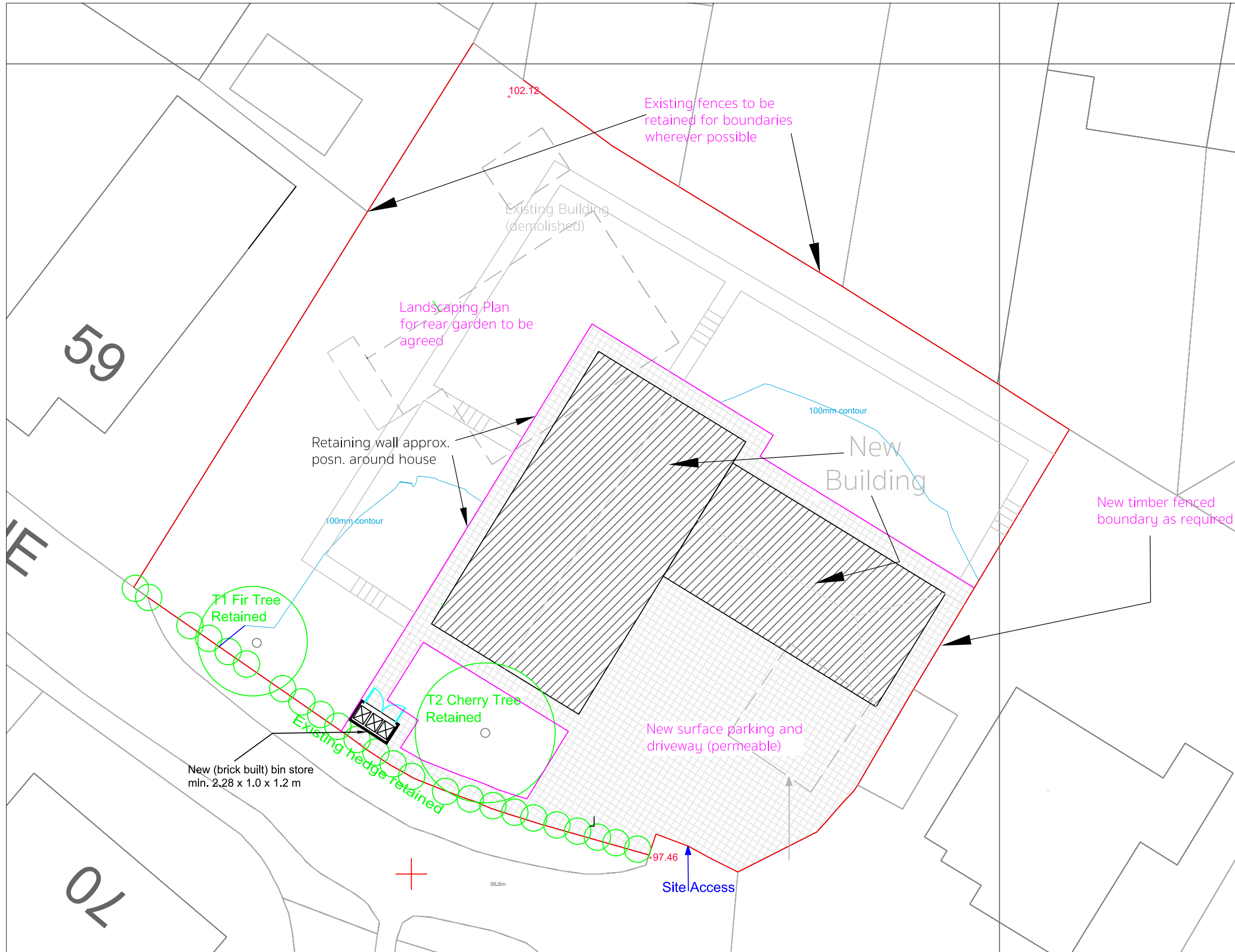
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Sheet Description:
Site Sections

No.	Revision /Issue	Date
B	Wall & excavation comments (LPA)	20/4/18
A	Revised house posn shown	26/3/18

Drawing Number:
130102/007B



General Notes

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Stockport & Tameside**
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07872 577 565

Project Description:
**New two storey
house**

For:
Mr Mustapha Matib

Site Address:
**61 Gibraltar Lane
Denton, M34 7PY**

Date: **March 2018**

Scale: **1:200 @ A3**

Sheet Description:
Site Layout Plan

No.	Revision /Issue	Date
A	Wall & fences comments (LPA)	20/4/18

Drawing Number:
130102/008A



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Scale 1/500 Date 15/5/2018

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Application Number: 17/00856/FUL

Photo 1



Photo 2



Photo 3



Photo 4



Application Number 17/00949/OUT

Proposal Outline planning application for the demolition of the existing industrial buildings on the site and the erection of 14 dwellings (8 townhouses and a block of 6 apartments)

Site Unit 14 Glover Centre Egmont Street Mossley

Applicant McKay Homes Ltd

Recommendation Grant planning permission, subject to the prior completion of a Section 106 agreement, subject to conditions

Reason for report A Speakers Panel decision is required because the application constitutes major development and includes the requirement of a Section 106 legal agreement.

1. APPLICATION DESCRIPTION

1.1 The applicant seeks outline planning permission for the erection of 14 dwellings on the site, in the form of 8 dwellings and 6 apartments in one block in the north western corner of the site. The applicant seeks approval for the means of access and layout. The matters of appearance, landscaping and the scale of development are reserved.

1.2 The applicant has provided the following documents in support of the planning application:

- Flood Risk and Drainage Assessment;
- Planning Statement;
- Topographical survey;
- Crime Impact Survey

2. SITE & SURROUNDINGS

2.1 The application site is occupied by buildings and hardstanding associated with the previous industrial use of the land. A brick building with a profile sheet roof fronts onto Egmont Street on the northern boundary, with smaller structures in the central and southern parts of the site. The Huddersfield Narrow Canal runs adjacent to the western boundary of the site, with a dense belt of trees running the length of the eastern boundary.

3. RELEVANT PLANNING HISTORY

3.1 16/01163/OUT - Outline Application (access only) attached to 0.63Ha of land for proposed residential development including access from Cheshire Street (the application site included the land that is the subject of 17/00949/OUT and additional land to the south) – resolution to approve at June 2017 meeting of Speakers Panel – decision not issued as Section 106 was not signed by the applicant. As such, this is not an extant planning permission.

- 3.2 13/00169/OUT – Proposed Residential Development - approved
- 3.3 07/00050/OUT - Proposed Residential Development – approved

4. RELEVANT PLANNING POLICIES

4.1 National Planning Policy Framework (NPPF)

4.2 Planning Practice Guidance (PPG)

4.3 Tameside Unitary Development Plan (UDP) Allocation

Unallocated, within the settlement of Mossley.

4.4 Part 1 Policies

- 1.3: Creating a Cleaner and Greener Environment.
- 1.4: Providing More Choice and Quality Homes.
- 1.5: Following the Principles of Sustainable Development
- 1.6: Securing Urban Regeneration
- 1.10 Protecting and Enhancing the Natural Environment
- 1.12: Ensuring an Accessible, Safe and Healthy Environment

4.5 Part 2 Policies

- H2: Unallocated Sites.
- H4: Type, size and affordability of dwellings
- H5: Open Space Provision
- H7: Mixed Use and Density (Density being relevant to this proposal)
- H10: Detailed Design of Housing Developments
- OL4: Protected Green Space
- OL7: Potential of Water Areas
- OL10: Landscape Quality and Character
- T1: Highway Improvement and Traffic Management.
- T10: Parking
- C1: Townscape and Urban Form
- N5: Trees Within Development Sites.
- N6: Protection and Enhancement of Waterside Areas
- N7: Protected Species
- MW11: Contaminated Land
- MW14 Air Quality
- U3: Water Services for Developments
- U4 Flood Prevention
- U5 Energy Efficiency

4.6 Other Policies

Greater Manchester Spatial Framework - Publication Draft October 2016;
Residential Design Supplementary Planning Document; and,
Trees and Landscaping on Development Sites SPD adopted in March 2007.

4.7 National Planning Policy Framework (NPPF)

Section 1 Delivering sustainable development
Section 6 Delivering a wide choice of high quality homes
Section 7 Requiring good design
Section 8 Promoting healthy communities
Section 11: Conserving and enhancing the natural environment

4.8 Planning Practice Guidance (PPG)

4.9 This is intended to complement the NPPF and to provide a single resource for planning guidance, whilst rationalising and streamlining the material. Almost all previous planning Circulars and advice notes have been cancelled. Specific reference will be made to the PPG or other national advice in the Analysis section of the report, where appropriate.

5. PUBLICITY CARRIED OUT

5.1 Neighbour notification letters were issued in accordance with the requirements of the Town and Country Planning (Development Management Procedure) (England) Order 2015 and the Council's adopted Statement of Community Involvement. This is in addition to a site notice and press notice.

6. RESPONSES FROM CONSULTEES

6.1 Borough Tree Officer: Raises no objections to the proposals. Details of the proposed landscaping scheme should be secured by condition.

6.2 United Utilities: No objection to the proposed development subject to conditions relating to the details of foul and surface water drainage (including management of maintenance of sustainable drainage systems to be installed) being attached to any approval.

6.3 Greater Manchester Ecological Unit (GMEU): No objections to the proposals subject to the imposition of a condition requiring the submission and approval of an Environmental Construction Method Statement, including details of how pollution of the Canal waters is to be avoided during the construction phase of the development.

6.4 Borough Contaminated Land Officer: Recommends that a standard contaminated land condition is attached to any planning approval granted for development at the site, requiring the submission and approval of an assessment into potential sources of contamination and a remediation strategy.

6.5 Borough Environment Health Officer: Raises no objections to the proposed development subject to the imposition of conditions limiting the hours of works and deliveries during the construction process, requiring the submission and approval of bin storage arrangements serving the development and the approval of a soundproofing scheme to mitigate the impact of noise associated with adjacent uses on the residential amenity of the future occupants of the development.

6.6 Local Highway Authority: Raises no objections to the proposals subject to the imposition of conditions requiring the laying out (and retention free from obstruction thereafter) of the car parking spaces prior to the first occupation of the development, the retention of pedestrian visibility splays on either side of the proposed access arrangements, the submission of a survey of the condition of the highway and the

submission of a Construction Environment Management Plan prior to the commencement of development.

- 6.7 Greater Manchester Police (Design Out Crime Officer): (comments on the Impact Statement submitted with the application) – no objection in principle subject to further details being provided at the reserved matters stage.
- 6.8 Environment Agency: No objections to the proposals subject to a condition securing compliance with the mitigation measures detailed in the Flood Risk Assessment submitted with the planning application.
- 6.9 Mossley Town Council: considered the application at the meeting held on 13 December 2017. No observations to make.

7. SUMMARY OF THIRD PARTY RESPONSES RECEIVED

- 7.1 No third party representations have been received.

8. ANALYSIS

- 8.1 The issue to be assessed in the determination of this planning application are:
- 1) The principle of development;
 - 2) The impact of the design and scale of the development on the character of the site and the surrounding area;
 - 3) The impact upon the residential amenity of neighbouring properties;
 - 4) The impact on highway safety;
 - 5) The impact on flood risk;
 - 6) The impact on ecology; and,
 - 7) Other matters

9. PRINCIPLE OF DEVELOPMENT

- 9.1 The site is located adjacent to two Established Employment Areas (one to the west and one to the south) but the site itself is not subject to this (or any other) designation. It is clear however that the last use of the site was for employment purposes. Therefore, it does constitute an employment site and therefore the provisions of policy E3 of the UDP apply.
- 9.2 The policy states that the conversion of such sites to residential or mixed use development will not be permitted unless it is considered that the need for housing and the regeneration benefits of such development outweigh the need to retain the site for employment purposes. The policy states that, in making this assessment, the following factors should be considered:
- (a) The quality and type of employment sites and premises available in the area
 - (b) Evidence of demand for employment sites and premises in the area
 - (c) The suitability of the site for further employment use in terms of size, physical characteristics, access, traffic impact, and sensitivity of surrounding land uses

- (d) The opportunity which may be presented for new forms of employment as part of a mixed use scheme
- 9.3 In relation to criterion (a) and (b), the presence of two Established Employment Areas within close proximity of the site indicates that significant areas of land in this part of Mossley are protected as sites for employment use. The Employment Land Review of 2013 indicated that the supply of industrial use properties was greater than demand for such premises in the Borough. The surplus of employment land has been reduced through the inclusion of some allocated employment land within the Strategic Housing Land Availability Assessment (SHLAA) (i.e. there is an assumption that some of these sites would be redeveloped for residential use).
- 9.4 In relation to alternative employment uses, offices are classified as a town centre use and sites for such development should be considered within designated town centres first. This site does not fall within the town centre. In relation to less intensive industrial uses, there is a Designated Opportunity Area to the north of the site, where a number of former mill buildings once stood. That land is considered suitable under policy E2 as a site for residential and some business uses, which could include mixed use development covered by criteria (d) of policy E3. This allocation represents an alternative site for employment uses within close proximity of the site. It is therefore considered that the benefit of boosting the supply of housing in the Borough would outweigh the retention of part of the site for employment purposes in a mixed use development, addressing the requirements of criteria (d) of policy E3. These factors are considered to reduce the weight to be attributed to the harm arising from the loss of this unallocated employment site.
- 9.5 The site is considered to be suitable for employment uses (albeit that the proximity of no. 60 Egmont street to the north east would render a number of B2 and potentially B8 uses unsuitable), given the commercial nature of a number of adjacent uses and the separation distance to other neighbouring residential properties. However, in the broader context, there is a need to boost the supply of housing in the Borough and a number of the allocated employment sites included within the SHLAA have not yet been granted planning permission for housing. There is support in the NPPF for the redevelopment of brownfield sites for housing in sustainable locations. This site is considered to meet this definition, with regular public transport connections via buses on Egmont Street to and from Oldham and Mossley train station is within reasonable walking distance of the site.
- 9.6 On balance, following the above assessment, it is considered that the harm arising from the loss of the employment site is outweighed by the benefits of boosting the supply of housing in the Borough on a brownfield site in a sustainable location. The principle of development is therefore considered to be acceptable, subject to all other material considerations being satisfied.

10. CHARACTER OF THE SURROUNDING AREA

- 10.1 The scheme has been amended to significantly improve the layout, which is one of the matters to be determined at this outline stage. The original submission proposed a number of dwellings backing on to the western boundary of the site, adjacent to the

canal. Officers were concerned that this approach compromised one of the opportunities of the site and weakened how the development would relate to the surrounding area. There was also no activity at the end of the access road into the development, weakening the sense of place created by the scheme.

- 10.2 The amended layout now proposes a block of apartments fronting onto the canal, with a row of terraced units backing on to the eastern boundary of the site and 3 units located at the southern end of the site, facing the access road and providing a terminating vista on entry into the development. This amendment has resulted in a more positive development, presenting outward facing units along the western boundary with the canal, which is visible from the bridge immediately north west of the site, ensuring that the scheme will result in an environmental improvement, given the lack of quality in the existing built environment from that vantage point.
- 10.3 The revised proposals would respect the linear form of buildings in the surrounding area. There would be potentially some flexibility in terms of scale, given the opportunity presented by the canal frontage. This, along with the appearance of the buildings and details of landscaping to be incorporated within the development, is a matter to be determined at the reserved matters stage.
- 10.4 Following the above assessment, it is considered that the proposals would preserve the character of the site and the surrounding area.

11. RESIDENTIAL AMENITY

- 11.1 The property at 60 Egmont Street to the north east of the site would be separated from the apartment block in the north western corner and the north eastern gable of plot 8 by the internal access road and parking provision within the proposed development. Given the separation distances to be retained and the fact that there are no windows in the western elevation of that neighbouring property, it is considered that the proposals would not result in a detrimental impact on the residential amenity of the occupiers of that dwelling, subject to a condition requiring any first floor windows in the gable of plot 8 to be obscurely glazed.
- 11.2 The next closest residential properties are located on the opposite side of Egmont Street to the north east of the site. The proposed apartment block in the north western corner of the development would retain an oblique relationship with the corresponding elevations of those neighbouring properties.
- 11.3 Given this situation and the fact that the highway intervenes the separation distance, it is considered that the proposals would not have an adverse impact on the residential amenity of those neighbouring properties, subject to an appropriate scale and appearance to the development, to be determined at the reserved matters stage.
- 11.4 Given the extent of the separation distance to the properties on Cheshire Street to the east of the site and the density of the intervening tree belt, it is considered that the proposals would not result in any adverse impact on the residential amenity of those properties through unreasonable overlooking or overshadowing.

- 11.5 Adequate separation distances would be retained between plots to ensure that unreasonable overlooking and overshadowing would be avoided between dwellings within the proposed development. Where the units at plots 6-8 face the rear elevation of the apartment block in the north western corner, the separation distance would be 21 metres, meeting the requirements of the Residential Design Guide where habitable room windows face each other. Subject to the obscure glazing of any windows in the southern gable of the apartment building, the oblique relationship between that unit and plot 1 would be sufficient to preserve the amenity of those properties. This requirement can be secured by condition.
- 11.6 Part of the development would extend right up to the northern boundary of the site and a number of the neighbouring sites include commercial uses. It is therefore considered necessary to condition the details of a scheme to soundproof the elevations of a number of the dwellings proposed. This would ensure that the noise associated with traffic on Egmont Street and the adjacent uses would not result in an adverse impact on the living conditions of the future occupants of the development.
- 11.7 On the basis of the above assessment, it is considered that the proposals would not result in a detrimental impact on the residential amenity of any of the neighbouring properties or the future occupiers of the proposed development.

12. HIGHWAY SAFETY

- 12.1 Access to the proposed development would be taken from Egmont Street on the northern boundary of the site, the boundary on which the existing access is located. It is considered that sufficient visibility splays would be achievable in a north easterly and south westerly direction from the proposed access, given the relatively straight nature of this part of Egmont Street.
- 12.2 Given that the width of the access is considered to be sufficient to allow cars to pass each other at the entrance to the development, it is considered that the additional car movements would not result in an increase in congestion that could be considered to be severely detrimental to highway safety. On the basis of the guidance in paragraph 32 of the NPPF, planning permission should not be refused on this basis.
- 12.3 The scheme makes provision for 1 car parking spaces per dwelling, with 3 additional communal spaces shown on the proposed layout. The scale of development (including bedroom sizes of the proposed properties) is not being determined at this outline stage but the Residential Design Guide requires 2 parking spaces per dwelling for properties over 1 bedroom in size in this location.
- 12.4 The level of parking provision would fall short of the maximum standards set out in Policy T10 in the Unitary Development Plan and Policy RD7 in the Residential Design Guide. However, these are maximum standards and the NPPF states that any local standards should only be imposed where there is clear evidence that measures are required to manage the impact of traffic on the highway network. In this case, the Local Highway Authority has not raised any objections to the amended proposals, subject to conditions. Given the close proximity of the site to regular bus and train services to larger settlements and the level of services and facilities within

Mossley, it is considered that the site is in a sustainable location. On the basis of a combination of these factors, it is considered that the level of parking proposed would not result in a severely harmful impact upon highway safety.

- 12.5 Conditions requiring the laying out of the car parking spaces prior to the occupation of the development, the retention of pedestrian visibility splays and the approval of a Construction Environment Management Plan are considered reasonable and can be attached to the decision notice. The requirement for a highway condition survey to be submitted is considered not to be reasonable or necessary as this can be required by the Local Highway Authority via its powers under the Highways Act.

13. FLOOD RISK/DRAINAGE

- 13.1 The site is in Flood Zone 1 and is therefore considered to be at a lower risk of flooding. The applicant has submitted a Flood Risk Assessment with the planning application, which details that existing site levels are not to be lowered below 135.2m AOD. This measure will ensure that any raised water levels in the adjacent canal (caused by overflow from excess flows in the neighbouring River Tame during times of flood) will not in turn flood on to the application site. On the basis that a condition is attached to any permission granted requiring compliance with this measure, the Environment Agency has no objections to the proposals.

- 13.2 United Utilities have raised no objections to the proposals subject to the imposition of conditions requiring the foul and surface water drainage mechanisms to be separated and details of a sustainable surface water drainage strategy being approved and implemented. The applicant has confirmed on the application form that foul water would be drained from the development via a connection to the existing main sewer. It is considered that the proposed development would not result in a harmful impact in relation to flood risk, subject to the details of the means of surface water drainage being secured by condition.

14. ECOLOGY

- 14.1 GMEU have raised no objections to the proposals. Having assessed the ecological potential of the site, the ecology officer considers that the proposals would not result in a detrimental impact on any features of biodiversity value, subject to a condition requiring the submission and approval of an Environmental Construction Method Statement detailing how pollution of the Canal waters is to be avoided during the construction phase of the development. This condition is considered reasonable given the biodiversity value of the canal and can be attached to the decision notice. The inclusion of additional tree planting would also represent a biodiversity enhancement.

15. OTHER MATTERS

- 15.1 In relation to the impact on trees, the Borough Tree Officer has no objection to the application as the proposals would not result in the loss of any significant trees on the site. Details of a scheme of landscape planting as part of the development shall be secured at the reserved matters stage. A condition requiring protection of the trees adjacent to the eastern boundary of the site during the construction process is attached to the recommendation.

- 15.2 The Borough's Environmental Health Officer has raised no objections to the proposals, subject to the imposition of conditions. Conditions limiting the hours of operation and deliveries during the construction phase of the development and requiring the details of bin storage arrangements to be approved are all considered to be reasonable and necessary. These conditions are in addition to the soundproofing scheme referred to previously in this report.
- 15.3 The Borough Contaminated Land Officer has not raised any objections to the proposals, subject to securing further investigation work into potential sources of ground contamination on the site by condition. A condition requiring this additional work to be undertaken and that any necessary mitigation measures are agreed and implemented prior to the commencement of development is considered to be reasonable given the former industrial use of the site.
- 15.4 In relation to designing out crime, the applicant has submitted a Crime Impact Statement with the application. The feedback from the Police includes the weakness of an inactive frontage to the canal, which has been addressed in the revised proposals. The cul-de-sac arrangement of one way in and one way out of the development is supported. An alleyway would be provided to allow access to the rear of plot 6 but the entrance to would be well surveyed, given the close proximity of neighbouring properties within the development.
- 15.5 It is considered reasonable to condition details of a scheme of external lighting to public areas and arrangements for the management of the communal space associated with the flats can be included within the Section 106 Agreement. The Police do not raise any objections to the proposals, subject to the provision of additional information once the detailed design of the development is known. It is considered reasonable to condition the submission of the details to be included in an application for Secured by design Status and confirmation that this accreditation has been achieved prior to the occupation of the development.
- 15.6 In relation to financial contributions required to mitigate the impact of the development, the applicant will be required to make a contribution to the provision of open space within the local area, in accordance with policy H5 and a contribution to the upgrading of highway infrastructure, in order to achieve the objectives of policy T1 of the adopted UDP. The scale of development is a reserved matter and so the size of each property in terms of number of bedrooms is unknown at this outline stage. The Section 106 will therefore include a formula giving an amount per property, with the range of potential contributions given below.
- 15.7 In relation to local green space, the contribution from this development would be approximately £8,845.79 regardless of the mix, based on the Council's Developer Contributions formula, which shall be secured via a Section 106 Agreement. A scheme for improvements to teen and junior play equipment at Egmont Street Playing Fields is the project that has been identified for this contribution.
- 15.8 In relation to a contribution to the upgrading of the highway network to offset the impact of the development, there would be no charge if 14 x 1 bed units were advanced. However, a contribution of £2,635.97 would be required to offset the

impact of a larger development (the same levy applies to 2, 3 or 4 bed units). This funding would be used to contribute towards improving pedestrian connectivity along Manchester Road in Mossley.

- 15.9 These contributions are considered to meet the CIL regulations in that they are necessary to make the development acceptable in planning terms (given the limited amenity space to be provided on site and the additional traffic to be generated), directly related to the development (as the close proximity ensures that residents are likely to use these facilities) and proportionate in that the sum is based on the size of the development.
- 15.10 The scheme proposes less than 25 dwellings and therefore no affordable housing or education contributions are required, in accordance with the adopted policies of the Unitary Development Plan.

16. CONCLUSION

- 16.1 Despite the established use being commercial, the site is not allocated for employment purposes and is located adjacent to substantial areas of land allocated as Established Employment Areas and a Development Opportunity Area, where new employment uses could locate. Given the proximity of 60 Egmont Street to the north east of the site, it is considered that the site would not be appropriate for intensive commercial uses. These factors are considered to limit the extent of the harm arising from the loss of the employment site. Weighing this limited harm against the benefits of redeveloping a brownfield site in a sustainable location to boost the supply of housing in the Borough, officers conclude that the principle of development is acceptable.
- 16.2 It is considered that the revised layout would have a positive impact on the character of the site and surrounding area and would not result in an unreasonable impact on the residential amenity of neighbouring properties, subject to the scale being considered acceptable at the reserved matters stage. The level of parking provision is considered to be sufficient given the close proximity to regular public transport, services and facilities.
- 16.3 There are no objections to the proposals from the statutory consultees in relation to highway safety, flood risk/drainage, the impact on trees or ecology. Financial contribution to the provision of green space and upgrading of the adopted highway within the surrounding area can be secured via a Section 106 agreement.
- 16.4 The application is therefore considered to accord with the relevant national and local planning policies listed earlier in this report.

RECOMMENDATION

Grant planning permission, subject to the completion of a Section 106 Agreement to secure the following:

Green Space contribution - £8,845.79 to be used for improvements to play equipment at Egmont Street Playing Fields.

Highway works: £2,635.97 contribution towards improving pedestrian connectivity along Manchester Rd in Mossley

A Management and Maintenance plan relating to the upkeep of communal green space and parking areas within the site.

And the following conditions:

1. Application for approval of reserved matters must be made not later than the expiry of three years beginning with the date of this permission and the development must be begun not later than the expiry of two years from the final approval of the reserved matters or, in the case of approval on different dates, the final approval of the last such matter to be approved.
2. Before any development is commenced approval shall first be obtained from the Local Planning Authority with respect to the reserved matters, namely the landscaping, scale and appearance of the development.
3. The development hereby approved shall be carried out in accordance with the following approved plans:
 - 1:1250 Site location plan (drawing no. 3134/15)
 - Proposed site plan (drawing no. 3134/17D) (insofar as it relates to the means of access and layout of the development only)
4. Notwithstanding any description of materials in the application, no above ground construction works shall take place until samples and/or full specification of materials to be used: externally on the buildings; in the construction of all boundary walls, fences and railings; and, in the finishes to all external hard-surfaces have been submitted to, and approved in writing by, the local planning authority. Such details shall include the type, colour and texture of the materials. Development shall be carried out in accordance with the approved details.
5. The car parking spaces to serve the development hereby approved shall be laid out as shown on the approved proposed site plan (drawing no. 3134/17D), prior to the occupation of any of the dwellings and shall be retained free from obstruction for their intended use at all times thereafter.
6. Notwithstanding the details shown on the approved plans, no development shall commence until scaled plans detailing the construction of the access, including vehicular swept paths and visibility splays to be maintained free from obstruction on both sides of the access to serve the development hereby approved have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details and shall be retained as such thereafter.
7. No development shall commence until details of tree protection measures to meet the requirements of BS5837:2012 have been installed around the trees on the eastern boundary of the site. The protection measures shall be installed in accordance with the approved details prior to the commencement of development and shall be retained in situ for the duration of the construction works. The construction works shall be carried out in accordance with the approved details.

8. During demolition/construction no work (including vehicle and plant movements, deliveries, loading and unloading) shall take place outside the hours of 07:30 and 18:00 Mondays to Fridays and 08:00 to 13:00 Saturdays. No work shall take place on Sundays and Bank Holidays.
9. No development shall commence until such time as a Construction Environment Management Plan has been submitted to and approved in writing by the Local Planning Authority. This shall include details of:
 - Wheel wash facilities for construction vehicles;
 - Arrangements for temporary construction access;
 - Contractor and construction worker car parking;
 - Turning facilities during the remediation and construction phases;
 - Details of on-site storage facilities;

The development shall be carried out in accordance with the approved Construction Environmental Management Plan.

10. No part of the development hereby approved shall be occupied until details of the means of storage and collection of refuse generated by the development have been submitted to and approved in writing by the Local Planning Authority. The details shall include scaled plans showing the location of storage and the means of enclosure. The bin storage arrangements for each dwelling shall be implemented in accordance with the approved details prior to the occupation of that dwelling and shall be retained as such thereafter.
11. Prior to the commencement of any development, a surface water drainage scheme, based on the hierarchy of drainage options in the National Planning Practice Guidance with evidence of an assessment of the site conditions shall be submitted to and approved in writing by the Local Planning Authority. The surface water drainage scheme must be in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement national standards. Foul and surface water shall be drained on separate systems and in the event of surface water draining to the public surface water sewer, details of the flow rate and means of control shall be submitted. The scheme shall include details of on-going management and maintenance of the surface water drainage system to be installed. The development shall be completed in accordance with the approved details and retained and maintained as such thereafter.
12. None of the dwellings hereby approved shall be occupied until details indicating that the development shall achieve Secured By Design status have been submitted to and approved in writing by the Local Planning Authority. The crime prevention measures shall be implemented in accordance with the approved details, prior to the first occupation of any of the dwellings and the development shall be retained as such thereafter.
13. No tree felling or vegetation removal shall take place during the optimum period for bird nesting (March to July inclusive) unless otherwise agreed in writing with the Local Planning Authority.

14. No development, other than site clearance and site compound set up, shall commence until such time as the following information has been submitted in writing and written permission at each stage has been granted by the Local Planning Authority.

i) A preliminary risk assessment to determine the potential for the site to be contaminated shall be undertaken and approved by the Local Planning Authority. Prior to any physical site investigation, a methodology shall be approved by the Local Planning Authority. This shall include an assessment to determine the nature and extent of any contamination affecting the site and the potential for off-site migration.

ii) Where necessary a scheme of remediation to remove any unacceptable risk to human health, buildings and the environment (including controlled waters) shall be approved by the Local Planning Authority prior to implementation.

iii) Any additional or unforeseen contamination encountered during development shall be notified to the Local Planning Authority as soon as practicably possible and a remedial scheme to deal with this approved by the Local Planning Authority.

iv) Upon completion of any approved remediation schemes, and prior to occupation, a completion report demonstrating that the scheme has been appropriately implemented and the site is suitable for its intended end use shall be approved in writing by the Local Planning Authority.

The discharge of this planning condition will be given in writing by the Local Planning Authority on completion of the development and once all information specified within this condition and other requested information have been provided to the satisfaction of the Local Planning Authority and occupation/use of the development shall not commence until this time.

15. Any windows above ground level in any of the following elevations of the development hereby approved (as identified on the approved proposed site plan) shall be fitted with obscured glazing (to meet the requirements of Pilkington Level 3 as a minimum) and shall be fixed shut below a height of 1.7 metres above the internal floor level of the rooms that they serve:

- first floor windows in the north eastern elevation of plot 8
- first floor windows in the southern elevation of the apartment building

The development shall be retained as such thereafter.

16. No development shall commence until an Environmental Construction Method Statement detailing how pollution of the Canal adjacent to the western boundary of the site is to be avoided during the construction phase of the development has been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details.

17. No development shall commence until an acoustic assessment has been undertaken on the development site and has been submitted to and approved in writing by the Local Planning Authority. The assessment shall take account of all local noise sources that may affect the development, including noise from deliveries to and from adjacent commercial uses (including the wood recycling plant), activity in the service yards and any plant and machinery associated with those uses. The assessment shall detail the measures considered necessary to mitigate the impact of the identified noise sources and shall include manufacturer's specifications and scaled plans of the measures to be installed. The development shall be carried out in accordance with the approved details and shall be retained as such thereafter.
18. Prior to the occupation of any part of the development hereby approved, visibility splays shall be provided on both sides of the site access where it meets the footway. The visibility splays shall measure 2.4metres along the edge of the site access and 2.4 metres along the footway. It must be clear of anything higher than 600mm above ground level. The visibility splays shall be retained as such thereafter.
19. No development shall commence until scaled plans detailing the existing and proposed ground levels on the site and the finished floor and ridge levels of the dwellings (with reference to a fixed datum point) have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details and shall be retained as such thereafter.
20. Prior to the first occupation of any of the dwellings hereby approved, details of a scheme for external lighting to serve the development shall be submitted to and approved in writing by the Local Planning Authority. The details shall include a scale plan indicating the location of the lighting to be installed, a LUX contour plan indicating the levels of light spillage and scaled elevations of lighting columns/supporting structures. The external lighting scheme shall be implemented in accordance with the approved details prior to the first occupation of any of the dwellings and shall be retained as such thereafter.
21. The development shall be carried out in accordance with the mitigation measures detailed in the Flood Risk Assessment submitted with the planning application and shall be retained as such thereafter.

Reasons for conditions:

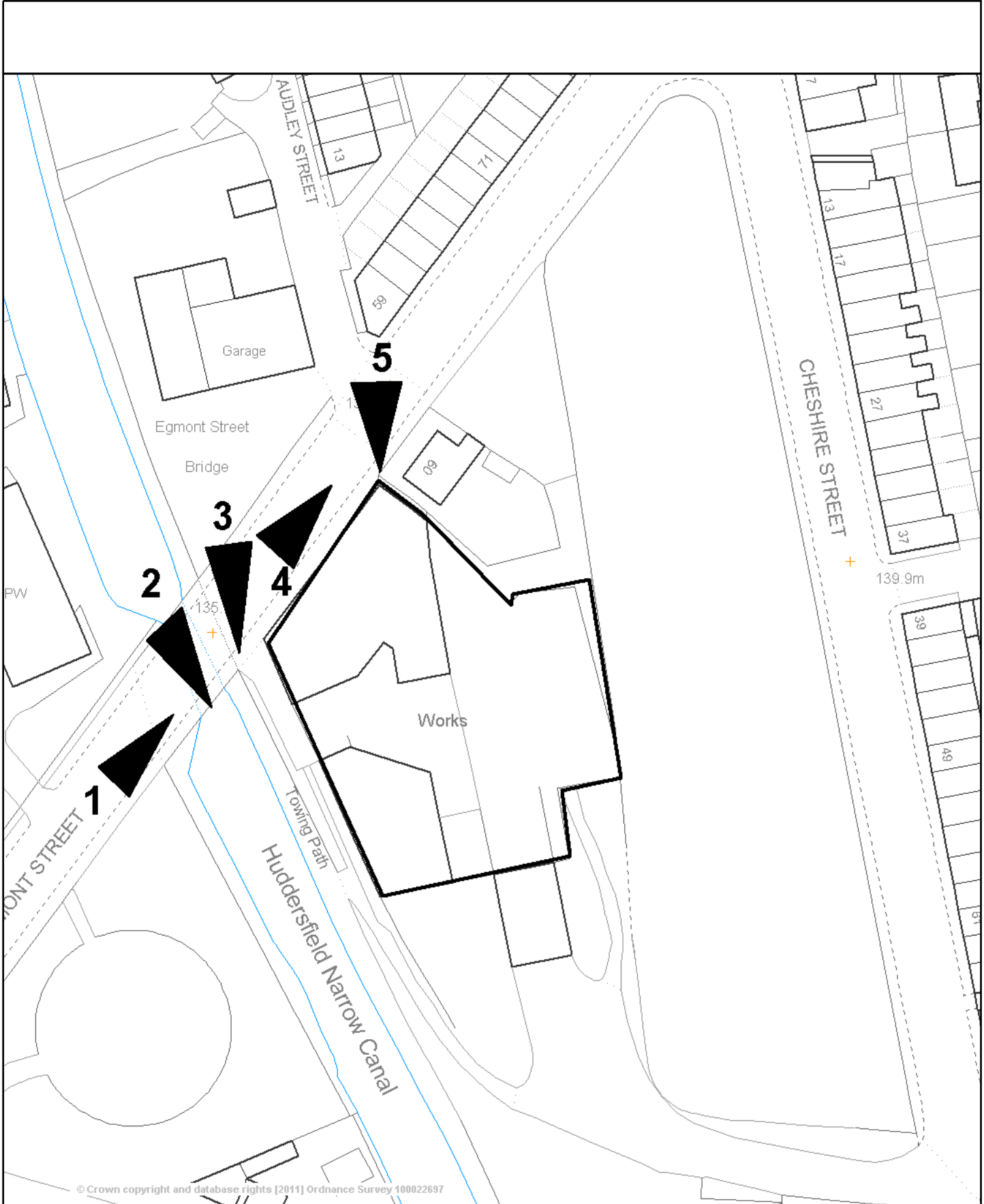
1. Required to be imposed by Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.
2. This planning permission grants outline consent only.
3. For the avoidance of doubt.
4. To ensure that the appearance of the development reflects the character of the surrounding area.

5. To ensure that the development is served by adequate parking provision.
6. In the interests of highway safety.
7. To ensure adequate protection of the trees to be retained on the site as part of the development.
8. To ensure that the residential amenity of the future occupiers of the development is preserved, in accordance with policy H10 of the Tameside UDP and the NPPF ..
9. To ensure that the impact of the construction phase of the development would be contained within the site and would not have a detrimental impact on highway safety or the residential amenity of neighbouring properties.
10. To provide adequate secure bin storage to serve the development and to safeguard the general amenity of the area in accordance with UDP policy 1.12/1.13/H10.
11. To secure a satisfactory system of drainage to serve the development.
12. To ensure that the development is designed to minimise opportunities for crime.
13. In order to prevent any habitat disturbance to nesting birds in accordance with the National Planning Policy Framework.
14. To ensure that the site is suitable for its intended end use and to remove any unacceptable risk to people/buildings/environment from contaminated land as per paragraph 121 of the National Planning Policy Framework.
15. To ensure that the residential amenity of neighbouring properties and the future occupants of the development hereby approved is adequately preserved.
16. To ensure that the proposed development does not result in harm to the biodiversity value of the adjacent Canal.
17. To protect the amenities of future occupants from external noise in accordance with UDP policy H10.
18. In order to maintain highway safety.
19. To ensure that the development does not have an adverse impact on the character of the surrounding area or the residential amenity of neighbouring properties.
20. To ensure that adequate lighting is provided to public areas

21. To ensure that the development does not result in an increased risk of flooding.

Informatives:

- 1) Section 106 Agreement to secure contributions towards off site green space provision and highways improvements.
- 2) Detailing the applicant's obligations in relation to avoiding harm to Protected species during the construction process.



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Scale 1/800 Date 15/5/2018

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Application Number: 17/00949/FUL Egmont Street, Mossley

Photo 1 – view looking northwards towards the site from Egmont Street.



Photo 2 – view looking southwards along the western boundary of the site from the bridge on Egmont Street.



Photo 3 – view looking south westwards from the entrance to the canal towpath adjacent to the north western corner of the site.



Photo 4 – looking northwards along Egmont Street from the north western corner of the site.

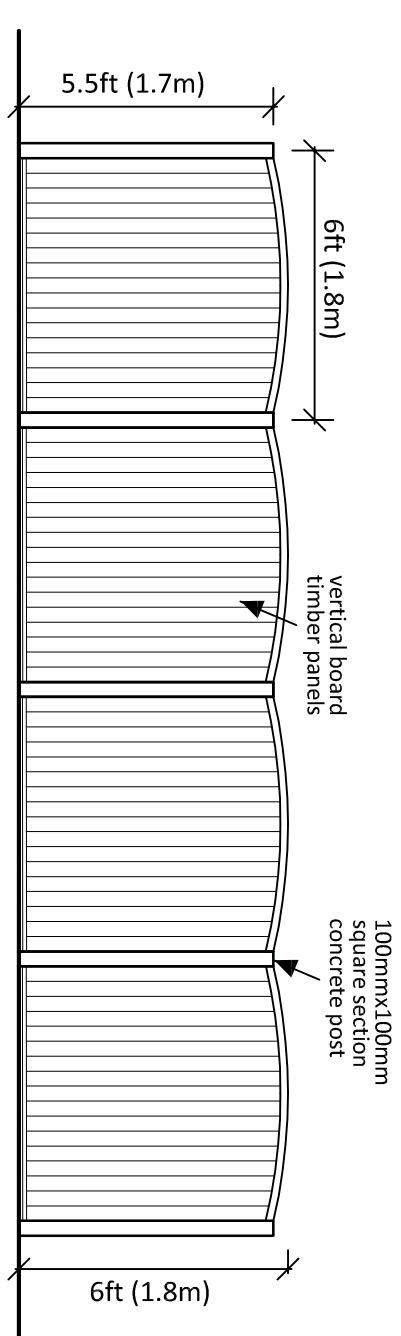
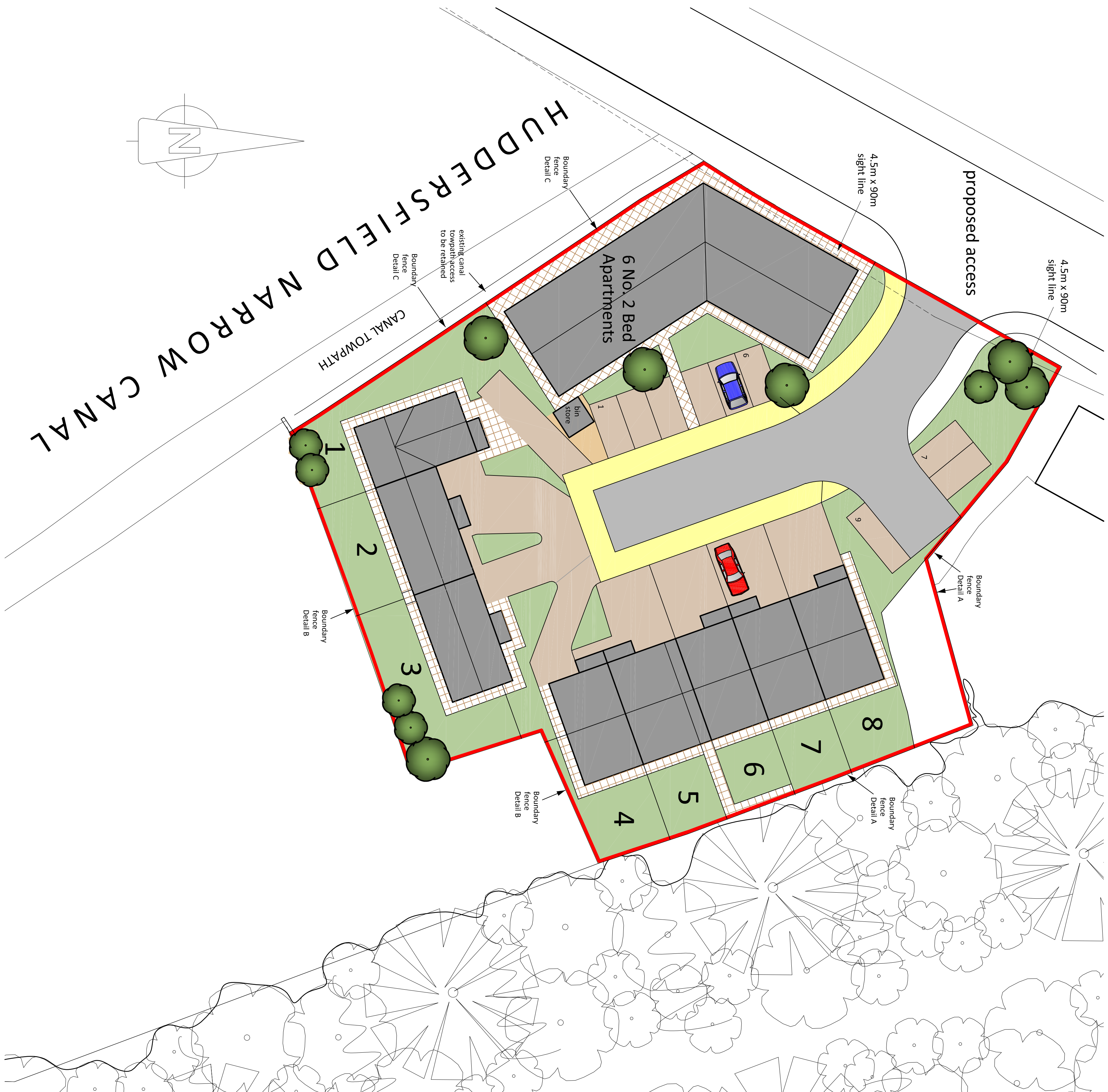


Photo 5 – view of the existing building and neighbouring property at 60 Egmont Street, which is adjacent to the north eastern corner of the application site.

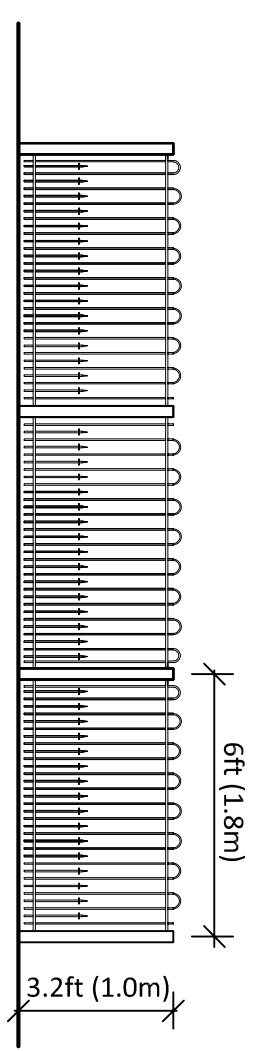


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Proposed Site Layout

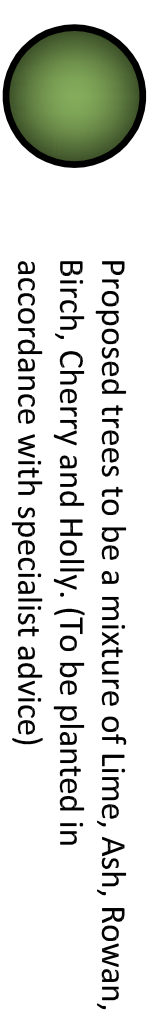


Boundary Fence Detail A & B (Scale 1:50)
NB - Close boarded acoustic type fence used for Detail B.




Boundary Fence Detail C (Scale 1:50)

planting



- | | |
|-----------------------|--------------|
| 1. Tilia x europaea | Common Lime |
| 2. Fraxinus excelsior | Common Ash |
| 3. Sorbus aucuparia | Rowan |
| 4. Betula pubescens | Birch |
| 5. Prunus serotina | Cherry |
| 6. Ilex aquifolium | Common Holly |

 Proposed shrubs / hedgerow's to be a mixture of Holly, Beech and Hawthorns. (To be planted at a density of 3 No. per m² and in accordance with specialist advice)

C - Dwellings repositioned and access alignment amended - 10/04/18
B - Turning head highlighted - 01/21/18
Revision

Client: **MCKAY HOMES LTD**
Project: **Land at Egmont Street, Mossley.**

PROPOSED SITE PLAN

Chartered Surveyors & Estate Agents
48 Wellington Road
Aston-under-Lyne OL6 6DQ

Cordingleys
Scale: 1 : 200 @ A1
Checked by: June 2017
Dwg. No. 3134/17D

Planning application boundary - contents edged red - 0.6 Acres or thereabouts

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Application Number 17/01033/FUL

Proposal Construction of 11 No. dwellings and associated works

Site Land on the rear of and 81-95 Ridge Hill Lane, Stalybridge

Applicant Cheshire Homes Limited

Recommendation Grant planning permission, subject to the prior completion of a Section 106 agreement, and subject to conditions

Reason for report A Speakers Panel decision is required because the application constitutes major development and includes the requirement of a Section 106 legal agreement.

1. APPLICATION DESCRIPTION

- 1.1 The applicant seeks full planning permission for erection of 11 dwellings on the site. The submitted layout proposes the erection of 4 pairs of semi-detached dwellings fronting onto the internal access road to be constructed along the south eastern edge of the site, with 3 detached units facing the end of that access road and backing on to the north eastern boundaries of the site.
- 1.2 The applicant has provided the following documents in support of the planning application:
 - Flood Risk and Drainage Assessment;
 - Ecology Reports;
 - Planning, Design and Access and sustainability Statement;
 - Planning Statement;
 - Topographical survey;
 - Highways swept path analysis; and
 - Preliminary ground investigation.

2. SITE & SURROUNDINGS

- 2.1 The application site is land to the rear of the dwellings at 81-103 Ridge Hill Lane in Stalybridge. The rear boundaries of the properties at 81-95 Ridge Hill Lane abut the south eastern boundary of the site, which then juts in to follow the alignment of Ash Lea, which leads to the properties to the north east of the site. There is an existing unmade access which connects the land to the highway between the dwellings at 79 and 81 Ridge Hill Lane. Land levels rise up from Ridge Hill Lane into the site, ensuring that the central part of the site sits at a higher level than the properties to the south of the site.

3. PLANNING HISTORY

- 3.1 11/00644/OUT – Extension to the time to implement planning permission of outline planning permission 08/00649/OUT – approved.
- 3.2 08/00649/OUT – Outline residential development comprising of 10 houses - approve
- 3.3 06/01278/REM – Reserved matters application following the granting of outline planning permission ref. 06/00109/OUT – approved.
- 3.4 06/00109/OUT – Outline planning permission for the erection of 5 dwelling houses with associated details of siting, design, external appearance and access – approved.

- 3.5 03/01612/FUL – full planning permission for the erection of 2 detached dwellings – refused on highway safety grounds (that site was limited to the eastern part of the application site proposed in the applications referred to above and this current application and would have been accessed via Foxglove Lane/Ash Lea).

4. RELEVANT PLANNING POLICIES

- 4.1 National Planning Policy Framework (NPPF)

- 4.2 Planning Practice Guidance (PPG)

4.3 Tameside Unitary Development Plan (UDP) Allocation

Unallocated, within the settlement of Stalybridge.

4.4 Part 1 Policies

- 1.3: Creating a Cleaner and Greener Environment.
- 1.4: Providing More Choice and Quality Homes.
- 1.5: Following the Principles of Sustainable Development
- 1.10 Protecting and Enhancing the Natural Environment
- 1.12: Ensuring an Accessible, Safe and Healthy Environment

4.5 Part 2 Policies

- H2: Unallocated Sites.
- H4: Type, size and affordability of dwellings
- H5: Open Space Provision
- H7: Mixed Use and Density (Density being relevant to this proposal)
- H10: Detailed Design of Housing Developments
- OL4: Protected Green Space
- OL10: Landscape Quality and Character
- T1: Highway Improvement and Traffic Management.
- T10: Parking
- C1: Townscape and Urban Form
- N3: Nature Conservation Factors
- N4: Trees and Woodland.
- N5: Trees Within Development Sites.
- N7: Protected Species
- MW11: Contaminated Land
- MW12 Control of Pollution
- U3: Water Services for Developments
- U4 Flood Prevention
- U5 Energy Efficiency

4.6 Other Policies

Greater Manchester Spatial Framework - Publication Draft October 2016;
Residential Design Supplementary Planning Document; and,
Trees and Landscaping on Development Sites SPD adopted in March 2007.

4.7 National Planning Policy Framework (NPPF)

- Section 1 Delivering sustainable development
- Section 6 Delivering a wide choice of high quality homes
- Section 7 Requiring good design

4.8 Planning Practice Guidance (PPG)

- 4.9 This is intended to complement the NPPF and to provide a single resource for planning guidance, whilst rationalising and streamlining the material. Almost all previous planning Circulars and advice notes have been cancelled. Specific reference will be made to the PPG or other national advice in the Analysis section of the report, where appropriate.

5. PUBLICITY CARRIED OUT

- 5.1 Neighbour notification letters were issued in accordance with the requirements of the Town and Country Planning (Development Management Procedure) (England) Order 2015 and the Council's adopted Statement of Community Involvement. This is in addition to a site notice and press notice.

6. RESPONSES FROM CONSULTEES

- 6.1 Borough Tree Officer: Raises no objections to the proposals. There is a mature Poplar tree in the rear garden of 95 Ridge Hill Lane which should be protected during the construction phase of the development.
- 6.2 United Utilities: No objection to the proposed development subject to conditions relating to the details of foul and surface water drainage (including management of maintenance of sustainable drainage systems to be installed) being attached to any approval.
- 6.3 Greater Manchester Ecological Unit (GMEU): No objections to the proposals subject to the imposition of conditions limiting the timing of tree/vegetation removal and a scheme to deal with invasive species on the site.
- 6.4 Borough Contaminated Land Officer: Recommends that a standard contaminated land condition is attached to any planning approval granted for development at the site, requiring the submission and approval of an assessment into potential sources of contamination and a remediation strategy.
- 6.5 Borough Environment Health Officer: Raises no objections to the proposed development subject to the imposition of conditions limiting the hours of works and deliveries during the construction process and requiring the bin storage arrangements indicated on the submitted plans to be implemented prior to occupation.
- 6.6 Local Highway Authority: Raises no objections to the proposals subject to the imposition of conditions requiring the laying out (and retention free from obstruction thereafter) of the car parking spaces prior to the first occupation of the development, the retention of pedestrian visibility splays on either side of the proposed access arrangements, the submission of a survey of the condition of the highway and the submission of a Construction Environment Management Plan prior to the commencement of development.
- 6.7 Greater Manchester Police (Design Out Crime Officer): No objections to the proposals subject to a condition requiring compliance with the recommendations listed in section 3.3 of the Crime Impact Statement submitted with the application.

7. SUMMARY OF THIRD PARTY RESPONSES RECEIVED

- 7.1 6 letters of objection have been received from neighbouring residents, raising the following concerns (summarised):

- The proposals would result in the loss of the ability of the existing residents at 81-95 Ridge Hill Lane to access the rear of their properties as a result of the proposed access arrangements to serve the development. The Council has confirmed previously that the residents of the properties fronting Ridge Hill Lane have a right to access the rear of those properties, a right that should be maintained.
- Planning permission has previously been granted for 10 dwellings, a larger development should not be permitted.
- The proposed dwellings should be limited to 2 storeys in height maximum in order to prevent unreasonable overlooking and preserve the residential amenity of neighbouring properties.
- The proposals may result in a detrimental impact on the condition of the boundary treatment to the property at 6 Springs Lane to the west of the site.
- The noise and disturbance and traffic congestion during the construction period would have an adverse impact on the residential amenity of neighbouring properties. Heavy goods vehicles and equipment accessing the site may affect the structural stability of adjacent properties and block the access to existing neighbouring properties.
- The proposed landscaping scheme would result in a loss of light to neighbouring properties and further restrict the ability of the occupants of the properties fronting Ridge Hill Lane to access the rear of the those dwellings.
- The traffic generated by the proposed development would result in additional congestion at the junctions on Ridge Hill Lane, St George's Street, Springs Lane and Darnton Road.
- The proposed development will result in regular car movements in close proximity to the rear boundaries of neighbouring residential properties, which will be detrimental to the amenity of the occupants of those dwellings.
- Development of the land would potentially make the existing sewage drainage problems worse, there is a history of blockages in the area.
- The access road to the planned development will meet Ridge Hill Lane less than 50 metres from its junction with St George's Street. This is a very busy junction with the majority of the traffic going to the Ridge Hill and Arlies Estates via this junction. The area adjacent to the access point is surrounded with parked vehicles. The proposal will therefore result in a highway safety hazard.
- Concerns regarding the impact of the proposed external lighting scheme on the amenity of neighbouring properties.
- Concerns regarding the security of the site during the construction process, given the open nature of the access.

8. ANALYSIS

8.1 The issue to be assessed in the determination of this planning application are:

- 1) The principle of development;
- 2) The impact of the design and scale of the development on the character of the site and the surrounding area;
- 3) The impact upon the residential amenity of neighbouring properties;
- 4) The impact on highway safety;
- 5) The impact on flood risk;
- 6) The impact on ecology; and,
- 7) Other matters

9. PRINCIPLE OF DEVELOPMENT

- 9.1 The site is approximately 0.37 hectares in area and the proposal equates to approximately 30 dwellings per hectare. Whilst this is at the lower end of the range indicated by policy H7 as representing an efficient use of land, the surrounding area is characterised by relatively low density development, with long rear gardens to the majority of plots. Within this context and given the proximity of neighbouring properties to the north east, south and west, the proposed density of development is considered to be acceptable.
- 9.2 The scheme would result in the loss of an area of undeveloped open space. Policy OL4 of the UDP seeks to retain areas of protected green space, including not only designated spaces (this site is not designated in this regard) but also 'areas of land in similar use but which are too small to be shown as Protected Green Spaces on the proposals map'.
- 9.3 Criterion (d) of the policy states that an exception to the policy requirement to retain green space can be made where the retention of a site or facilities for sport or recreational use is not necessary and the site has no special significance to the interests of sport and recreation. Tameside has recently produced a Playing Pitch Strategy and Action Plan report which does not identify the application site as being necessary to deliver the Council's aspirations to develop leisure space in the long term (next 6 years+).
- 9.4 There are a number of protected areas of open space within 10 minutes walking distance of the proposed development sites, which is the recommended walking distance threshold for Tameside, including the extensive facilities at Stamford Park off Darnton Road. On that basis, it is considered that the caveat at criterion (d) applies and that the scheme does not contravene policy OL4 as a result.
- 9.5 Paragraph 77 of the NPPF states that Local Green Space designation will not be appropriate for most green areas or open space and that the designation should only be used where the following criteria apply:
- Where the green space is in reasonably close proximity to the community it serves;
 - Where the green space is demonstrably special to a local community and holds a particular local significance, for example because of its beauty, historic significance, recreational value, tranquillity or richness of its wildlife; and
 - Where the green area is local in character and does not apply to an extensive tract of land.
-
- 9.6 Whilst the land would comply with criterion 1 and 3, it is considered that the land does not hold the value required by criterion 2. The site is not designated as a site of ecological or historic significance (either nationally or locally.) The site could be considered to be of amenity value to the properties on Ash Lea and Foxglove Lane. However, its wider significance and amenity value is limited by the fact that it is not prominent from public views. Due to the combination of these factors, it is considered that the land does not hold the value required by criterion 2 to warrant protection, in line with the guidance contained within paragraph 77 of the NPPF.
- 9.7 The site is considered to be in a sustainable location, with regular buses to Ashton and Hyde running along St. George's Street and Springs Lane, within a short walking distance of the application site. Stalybridge train station is within 0.5 miles and the facilities in the town centre are within 1 mile of the site. Overall therefore, there is good access to services and facilities and public transport and the principle of residential development in this location is therefore considered to be acceptable, subject to all other material considerations being satisfied.

10. CHARACTER OF THE SURROUNDING AREA

- 10.1 The scheme proposes a layout which would respect the linear form of development that characterises the pattern of development in the surrounding area. It is considered that the proposals would not result in an adverse impact on the character of the site or the surrounding area. Residential development exists to the north east of the site on Ash Lea and to the west on Springs Lane. Within that context, it is considered that the development to the rear of the frontage properties on Ridge Hill Lane would not result in an encroachment into open space to an extent that would be detrimental to the character of the surrounding area.
- 10.2 It is acknowledged that the properties to the west of the site are single storey in height and the property to the north east is 1.5 storeys in height. However the majority of the properties on Ridge Hill Lane and immediately adjacent to the site entrance are 2 storey semi-detached dwellings. Whilst the proposed dwellings would be effectively 2.5 storeys in height, the units would be set back a relatively significant distance from the Ridge Hill Lane frontage and the layout ensues that views from the access into the site and also from the junction between Ash Lea/Foxglove Lane and Ridge Hill Lane would remain relatively open, with gaps between the dwellings being aligned with these vistas. Given these factors, it is considered that the additional height would not be a dominant feature on views of the development on the streetscene.
- 10.3 The scheme would include the provision of landscape planting to soften the impact of the widening of the access at the entrance to the development and the inclusion of additional planting on the southern edge of the development would help to soften the impact of the scheme and assimilate the development into the surrounding area. The siting of soft landscaping within the development would also help to reduce the prominence of parking to the front of plots 2-6 and 9-11, none of which would be particularly visible from public views of the site.
- 10.4 Following the above assessment, it is considered that the proposals would preserve the character of the site and the surrounding area.

11. RESIDENTIAL AMENITY

- 11.1 The gable elevation of plot 1 at the south western end of the development would not contain any openings and would be in excess of 15 metres from the rear elevation of the bungalows to the south west of the site. Whilst the ridge height of the proposed dwellings would be elevated above a standard 2 storey property to accommodate rooms in the roof, the separation distance to be retained would exceed the requirements of the residential Design Guide for this relationship. On that basis it is considered that the proposals would not result in unreasonable overlooking in to or overshadowing of those neighbouring properties.
- 11.2 The front elevations of plots 1-8 would be set 12 metres back from the southern boundary of the site and would be in excess of 30 metres from the rear elevation of the properties fronting on to Ridge Hill Lane. The applicant has submitted a proposed section plan which indicates that the land is to be regraded, reducing the extent of the rise in levels from the existing neighbouring properties to the central part of the application site. On the basis of the proposed level changes, the separation distances would exceed minimum separation distances required by the Residential Design Guide in terms of that relationship.
- 11.3 The first floor rear elevations of the properties at plots 9-11 would be approximately 21 metres from the corresponding elevation of the 1.5 storey dwelling to the north east of the site. At ground floor, this would reduce to approximately 18 metre due to the projecting single storey extension to the rear of the proposed units, which would be set approximately

13 metres off the common boundary. Given the presence of an existing timber fence on that boundary, which would prevent overlooking at ground floor level, the separation distances to be retained are considered sufficient to prevent unreasonable overlooking or overshadowing into that neighbouring property.

- 11.4 Given the blank nature of the gable elevations of the proposed units and the orientation of the units, the proposed layout would preserve the residential amenity of the future occupants of the development.
- 11.5 A noise assessment considering the impact of traffic generated by the development on the amenity of the neighbouring properties and whether mitigation (such as acoustic fencing on the common boundary) is required can be secured by condition.

12. HIGHWAY SAFETY

- 12.1 The application site would incorporate some of the land currently within the curtilage of the property at 79 Ridge Hill Lane, in order to be able to provide an access of adoptable width. The proposed layout demonstrates that vehicular visibility splays of 2.4 metres by 43 metres can be achieved in either direction from the junction, which is considered to be acceptable in this location.
- 12.2 Objectors have referred to the impact of the additional traffic on congestion at junctions between Ridge Hill Lane and Darnton Road. The scheme would generate an anticipated level of 66 trips across a 24 hour period. Whilst clearly the level of trips would be higher in the peak periods, the number of trips during those times would be approximately 8 in the AM peak and 7 in the PM peak, on the basis of standard TRICS modelling. Given that the site is located in a residential area and that the width of the access is considered to be sufficient to allow cars to pass each other at the entrance to the development, it is considered that the additional car movements would not result in an increase in congestion that could be considered to be severely detrimental to highway safety. On the basis of the guidance in paragraph 32 of the NPPF, planning permission should not be refused on this basis.
- 12.3 The scheme makes provision for 2 car parking spaces per dwelling. Whilst each of the dwellings would have 4 bedrooms, this meets the requirements of the Residential Design Guide and given that the site is in a sustainable location, close to public transport services, it is considered that the level of parking provision is acceptable.
- 12.4 The Local Highway Authority has not raised any objections to the proposals, subject to a number of conditions. Conditions requiring the laying out of the car parking spaces prior to the occupation of the development, the retention of pedestrian visibility splays and the approval of a Construction Environment Management Plan are considered reasonable and can be attached to the decision notice. The requirement for a highway condition survey to be submitted is considered not to be reasonable or necessary as this can be required by the Local Highway Authority via its powers under the Highways Act.

13. FLOOD RISK/DRAINAGE

- 13.1 The site is in Flood Zone 1 and is therefore considered to be at a lower risk of flooding. United Utilities has raised no objections to the proposals subject to the imposition of conditions requiring the foul and surface water drainage mechanisms to be separated and details of a sustainable surface water drainage strategy being approved and implemented. The applicant has confirmed on the application form that foul water would be drained from the development via a connection to the existing main sewer. It is considered that the

proposed development would not result in a harmful impact in relation to flood risk, subject to the details of the means of surface water drainage being secured by condition.

14. ECOLOGY

- 14.1 GMEU has raised no objections to the proposals. Having assessed the ecological potential of the site, the ecology officer considers that the proposals would not result in a detrimental impact on any features of biodiversity value, subject to conditions limiting the timing of tree/vegetation removal and a scheme for control and removal of invasive species. These conditions are considered reasonable and can be attached to the decision notice. The inclusion of additional tree planting would also represent a biodiversity enhancement.

15. OTHER MATTERS

- 15.1 In relation to the impact on trees, the Borough Tree Officer has no objection to the application as the proposals would not result in the loss of any significant trees on the site. The scheme proposes new landscape planting on the either side of the access road and along the southern boundary of the site. The trees to be planted include native species and the specimens would be planted at intervals that would result in a comprehensive scheme, enhancing the appearance of the development.
- 15.2 Concerns have been expressed by local residents that these trees would result in a loss of light to the neighbouring dwellings to the detriment to the amenity of those properties. Given the length of the gardens of the properties that front onto Ridge Hill Lane to the south of the site, it is considered that the impact in terms of overshadowing would be limited in relation to the habitable room windows and the majority of the amenity space associated with those dwellings. However, to ensure that these trees and all areas within the development that fall outside of the curtilage of the dwellings are appropriately maintained, details of the management arrangements to be put in place can be secured as part of the Section 106 Agreement.
- 15.3 A number of the neighbouring residents have raised objection to the loss of access to rear of the properties that front on to Ridge Hill Lane. The existing access to the rear of those properties from Ridge Hill Lane is an unmade track, which is limited in width and does not form part of the adopted highway. Whilst it is noted that some of the existing properties do not have parking areas within their curtilage, a number of them do. Parking is not restricted on this part of Ridge Hill Lane. In addition to these factors, it must be considered that private rights of way across land are not a material planning consideration, as such matters fall to be determined under civil law. The applicant has indicated on the application form that they own the land that is the subject of the application and this has not been challenged during the consultation process. Planning permission should not therefore be withheld therefore, even if there is a later challenge regarding the implications of the development in terms of private rights of access.
- 15.4 The Borough's Environmental Health Officer has raised no objections to the proposals, subject to the imposition of conditions limiting the hours of operation and deliveries during the construction phase of the development and requiring the details of bin storage arrangements to be approved. Whilst the former is considered necessary, there are sufficient details of the bin storage arrangements on the proposed site plan to indicate that the latter can be covered by a compliance condition.
- 15.5 The Borough Contaminated Land Officer has not raised any objections to the proposals, subject to securing the further investigation work by condition. A condition requiring this additional work to be undertaken and that any necessary mitigation measures are agreed and implemented prior to the commencement of development is considered to be

reasonable given the extent of excavation works required to lay foundations due to the changes in land levels on the site.

- 15.6 Greater Manchester Police has confirmed that they have no objections to the proposals. All public areas would be well surveyed and it is not necessary to provide alleyways to access the rear of plots. Compliance with the Crime Impact Statement submitted with the planning application can be secured by condition.
- 15.7 In relation to financial contributions required to mitigate the impact of the development, the applicant will be required to make a contribution to the provision of open space within the local area, in accordance with policy H5 of the adopted UDP. The contribution from this development is £5,426.22, based on the Council's Developer Contributions formula, which shall be secured via a Section 106 Agreement. A scheme for improvements to play equipment in Stamford Park is the project that has been identified for this contribution.
- 15.8 The Local Highway Authority has indicated that the £8,617.69 contribution required to offset the impact of the development in highway terms would be allocated towards cycleway improvements between Stalybridge and Ashton, as identified within Tameside Cycling Strategy Options Report (2015).
- 15.9 These contributions are considered to meet the CIL regulations in that they are necessary to make the development acceptable in planning terms (given the limited amenity space to be provided on site and the additional traffic to be generated), directly related to the development (as the close proximity ensures that residents are likely to use these facilities) and proportionate in that the sum is based on the size of the development.
- 15.10 The scheme proposes less than 25 dwellings and therefore no affordable housing or education contributions are required, in accordance with the adopted policies of the Unitary Development Plan.

16. CONCLUSION

- 16.1 The principle of development on the site is considered to be acceptable given the predominantly residential character of the surrounding area and the relatively close proximity of the services and facilities in Stalybridge, including schools and public transport to larger settlements in the Borough.
- 16.2 It is considered that the scheme would respect the character of the site and surrounding area and would not be of a scale or design that would result in an unreasonable impact on the residential amenity of neighbouring properties. The level of parking provision is considered to be acceptable.
- 16.3 There are no objections to the proposals from the statutory consultees in relation to highway safety, flood risk/drainage, the impact on trees or ecology. Financial contributions to the provision of green space and upgrading of the adopted highway within the surrounding area can be secured via a Section 106 agreement.
- 16.4 The application is therefore considered to accord with the relevant national and local planning policies listed earlier in this report.

RECOMMENDATION

Grant planning permission, subject to the completion of a Section 106 Agreement to secure the following contribution:

Green Space - £5,426.22 to be used for improvements to play equipment in Stamford Park

Highway upgrade - £8,617.69 towards cycleway improvements between Stalybridge and Ashton, as identified within Tameside Cycling Strategy Options Report (2015).

Management of the public realm within the development

and the following conditions:

1. The development must be begun not later than the expiration of three years beginning with the date of this permission.
2. The development hereby approved shall be carried out in accordance with the following approved plans/details:
 - 1:1250 site location plan (Drwg No. 05/17/076 02)
 - 1:200 Proposed site plan (Drwg No. 05/17/076 01)
 - Detached dwellings plans and elevations (Drwg No. 05/17/076 03)
 - Semi-detached dwellings plans and elevations (Drwg No. 05/17/076 04)
 - Proposed highway widening plan (SCP/17491/F01)
 - Swept path analysis plan (SCP/17491/ATR01)
3. Notwithstanding any description of materials in the application, no above ground construction works shall take place until samples and/or full specification of materials to be used: externally on the buildings; in the construction of all boundary walls, fences and railings; and, in the finishes to all external hard-surfaces have been submitted to, and approved in writing by, the local planning authority. Such details shall include the type, colour and texture of the materials. Development shall be carried out in accordance with the approved details.
4. Notwithstanding the details shown on the approved plans, no development shall commence until scaled plans detailing the existing and proposed ground levels on the site and the finished floor and ridge levels of the dwellings (with reference to a fixed datum point) have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details and shall be retained as such thereafter.
5. The car parking spaces to serve the development hereby approved shall be laid out as shown on the approved proposed site plan (Drwg No. 05/17/076 01) prior to the occupation of any of the dwellings and shall be retained free from obstruction for their intended use at all times thereafter.
6. Prior to the first occupation of any of the dwellings hereby approved, details (including scaled plans and details of the construction material and colour finish) of the boundary treatments to be installed across the development shall be submitted to and approved in writing by the Local Planning Authority. The boundary treatments for each plot shall be installed in accordance with the approved details prior to the first occupation of that dwelling
7. The soft landscaping scheme shall be installed in accordance with the details as shown on the approved proposed site plan (Drwg No. 05/17/076 01). The approved scheme of landscaping shall be implemented before the first occupation of any part of the development or in accordance with a programme agreed previously with the local planning authority. Any newly planted trees or plants forming part of the approved scheme which, within a period of 5 years from the completion of the planting, are removed, damaged,

destroyed or die shall be replaced in the next appropriate planting season with others of similar size and species.

8. Notwithstanding the details shown on the approved plans, no development shall commence until scaled plans detailing the construction of the access, including the provision of visibility splays of 2.4 metres x 43 metres (on land within the ownership of the applicant or the adopted highway) on both sides of the access to serve the development hereby approved have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details and shall be retained as such thereafter.
9. No development shall commence until details of tree protection measures to meet the requirements of BS5837:2012 have been installed around the trees to be retained within and adjacent to the boundaries of the site and details of the method of construction to be employed within the Root Protection Area of the trees to be retained have been submitted to and approved in writing by the Local Planning Authority. The protection measures shall be installed in accordance with the approved details prior to the commencement of development and shall be retained in situ for the duration of the construction works. The construction works shall be carried out in accordance with the approved details.
10. During demolition/construction no work (including vehicle and plant movements, deliveries, loading and unloading) shall take place outside the hours of 07:30 and 18:00 Mondays to Fridays and 08:00 to 13:00 Saturdays. No work shall take place on Sundays and Bank and Public Holidays.
11. No development shall commence until such time as a Construction Environment Management Plan has been submitted to and approved in writing by the Local Planning Authority. This shall include details of:
 - Wheel wash facilities for construction vehicles;
 - Arrangements for temporary construction access;
 - Contractor and construction worker car parking;
 - Turning facilities during the remediation and construction phases;
 - Details of on-site storage facilities;

The development shall be carried out in accordance with the approved Construction Environmental Management Plan.

12. The refuse storage arrangements to serve each of the dwellings hereby approved shall be implemented in the location and to the design specified on the approved proposed site plan (Drwg No. 05/17/076 01) prior to the occupation of that dwelling and shall be retained as such thereafter.
13. Prior to the commencement of any development, a surface water drainage scheme, based on the hierarchy of drainage options in the National Planning Practice Guidance with evidence of an assessment of the site conditions shall be submitted to and approved in writing by the Local Planning Authority. The surface water drainage scheme must be in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement national standards. Foul and surface water shall be drained on separate systems and in the event of surface water draining to the public surface water sewer, details of the flow rate and means of control shall be submitted. The scheme shall include details of on-going management and maintenance of the surface water drainage system to be installed. The development shall be completed in accordance with the approved details and retained and maintained as such thereafter.

14. The development hereby approved shall be carried out in accordance with the measures detailed in section 3.3 of the Crime Impact Assessment submitted with the planning application and shall be retained as such thereafter.
15. No tree felling or vegetation removal shall take place during the optimum period for bird nesting (March to July inclusive) unless otherwise agreed in writing with the Local Planning Authority.
16. No development, other than site clearance and site compound set up, shall commence until such time as the following information has been submitted in writing and written permission at each stage has been granted by the Local Planning Authority.
 - i) A preliminary risk assessment to determine the potential for the site to be contaminated shall be undertaken and approved by the Local Planning Authority. Prior to any physical site investigation, a methodology shall be approved by the Local Planning Authority. This shall include an assessment to determine the nature and extent of any contamination affecting the site and the potential for off-site migration.
 - ii) Where necessary a scheme of remediation to remove any unacceptable risk to human health, buildings and the environment (including controlled waters) shall be approved by the Local Planning Authority prior to implementation.
 - iii) Any additional or unforeseen contamination encountered during development shall be notified to the Local Planning Authority as soon as practicably possible and a remedial scheme to deal with this approved by the Local Planning Authority.
 - iv) Upon completion of any approved remediation schemes, and prior to occupation, a completion report demonstrating that the scheme has been appropriately implemented and the site is suitable for its intended end use shall be approved in writing by the Local Planning Authority.

The discharge of this planning condition will be given in writing by the Local Planning Authority on completion of the development and once all information specified within this condition and other requested information have been provided to the satisfaction of the Local Planning Authority and occupation/use of the development shall not commence until this time.
17. No development shall commence unless and until a Method Statement is submitted to and approved in writing by the Local Planning Authority detailing how Japanese Knotweed and any other invasive species on the site will be removed from the site. The development shall thereafter proceed in strict accordance with the approved Method Statement.
18. Prior to the occupation of any part of the development hereby approved, visibility splays shall be provided on both sides of the site access where it meets the footway. The visibility splays shall measure 2.4metres along the edge of the site access and 2.4 metres along the footway. It must be clear of anything higher than 600mm above ground level. The visibility splays shall be retained as such thereafter.
19. No development shall commence until a noise impact assessment considering the potential impact of noise generated by the development on the residential amenity of the neighbouring properties has been submitted to and approved in writing by the Local Planning Authority. The assessment shall indicate the anticipated noise levels in relation to background noise and shall indicate necessary mitigation measures to ensure that existing background noise levels are not exceeded. The development shall be carried out in accordance with the approved details and shall be retained as such thereafter.

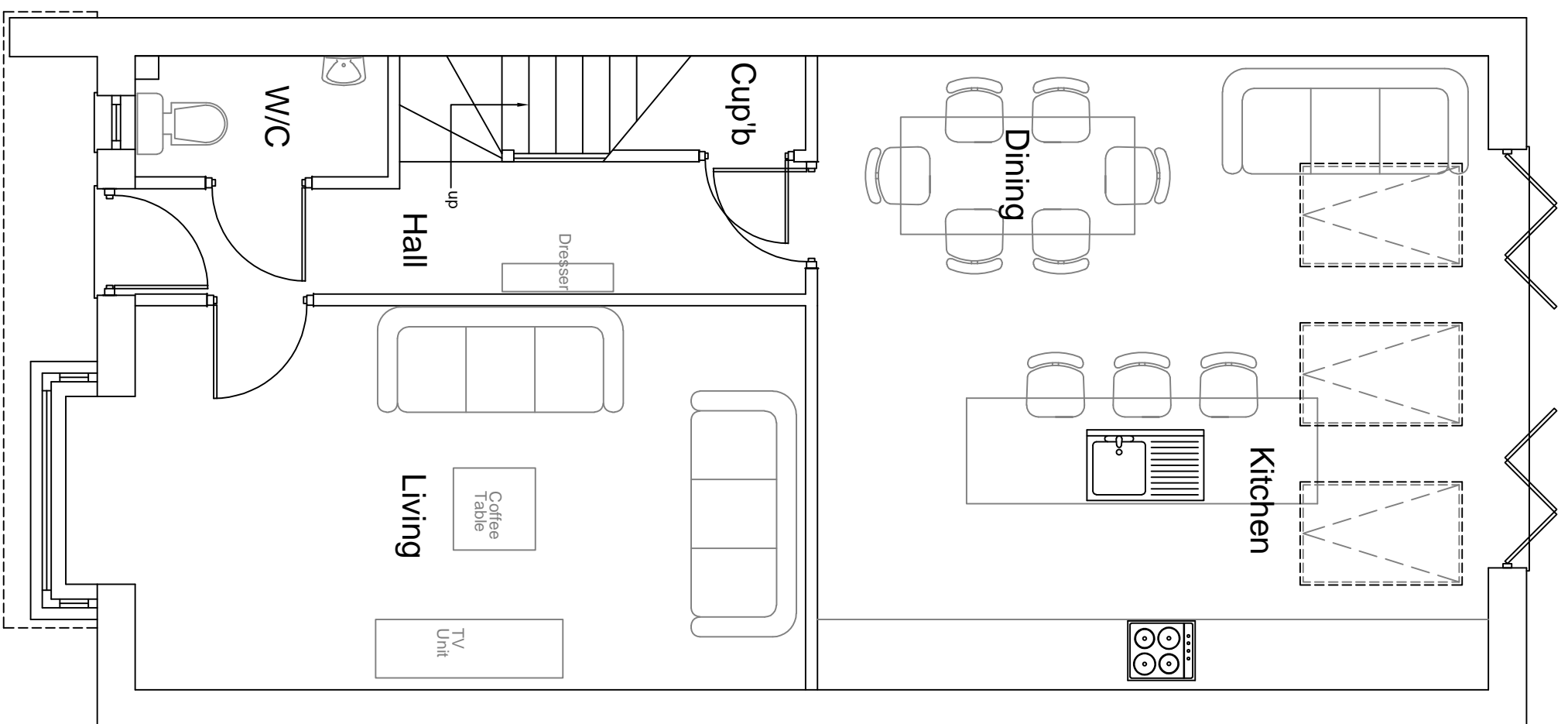
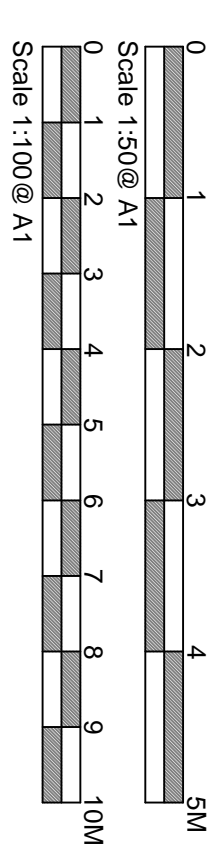
Reasons for conditions:

1. Required to be imposed by Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.
2. For the avoidance of doubt.
3. To ensure that the appearance of the development reflects the character of the surrounding area.
4. To ensure that the development does not have an adverse impact on the character of the surrounding area or the residential amenity of neighbouring properties.
5. To ensure that the development is served by adequate parking provision.
6. To protect the amenities of occupants of nearby properties/dwelling houses in accordance with UDP policies 1.12 and E6.
7. To ensure that sufficient hard and soft landscaping are implemented and maintained to ensure that the overall development respects the character of the surrounding area.
8. In the interests of highway safety.
9. To ensure adequate protection of the trees to be retained on the site as part of the development.
10. To ensure that the residential amenity of the future occupiers of the development is preserved, in accordance with policy H10 of the Tameside UDP and the NPPF.
11. To ensure that the impact of the construction phase of the development would be contained within the site and would not have a detrimental impact on highway safety or the residential amenity of neighbouring properties.
12. To provide adequate secure bin storage to serve the development and to safeguard the general amenity of the area in accordance with UDP policy 1.12/1.13/H10.
13. To secure a satisfactory system of drainage and to prevent pollution of the water environment in accordance with the National Planning Policy Framework.
14. To ensure that the development is designed to minimise opportunities for crime.
15. In order to prevent any habitat disturbance to nesting birds in accordance with the National Planning Policy Framework.

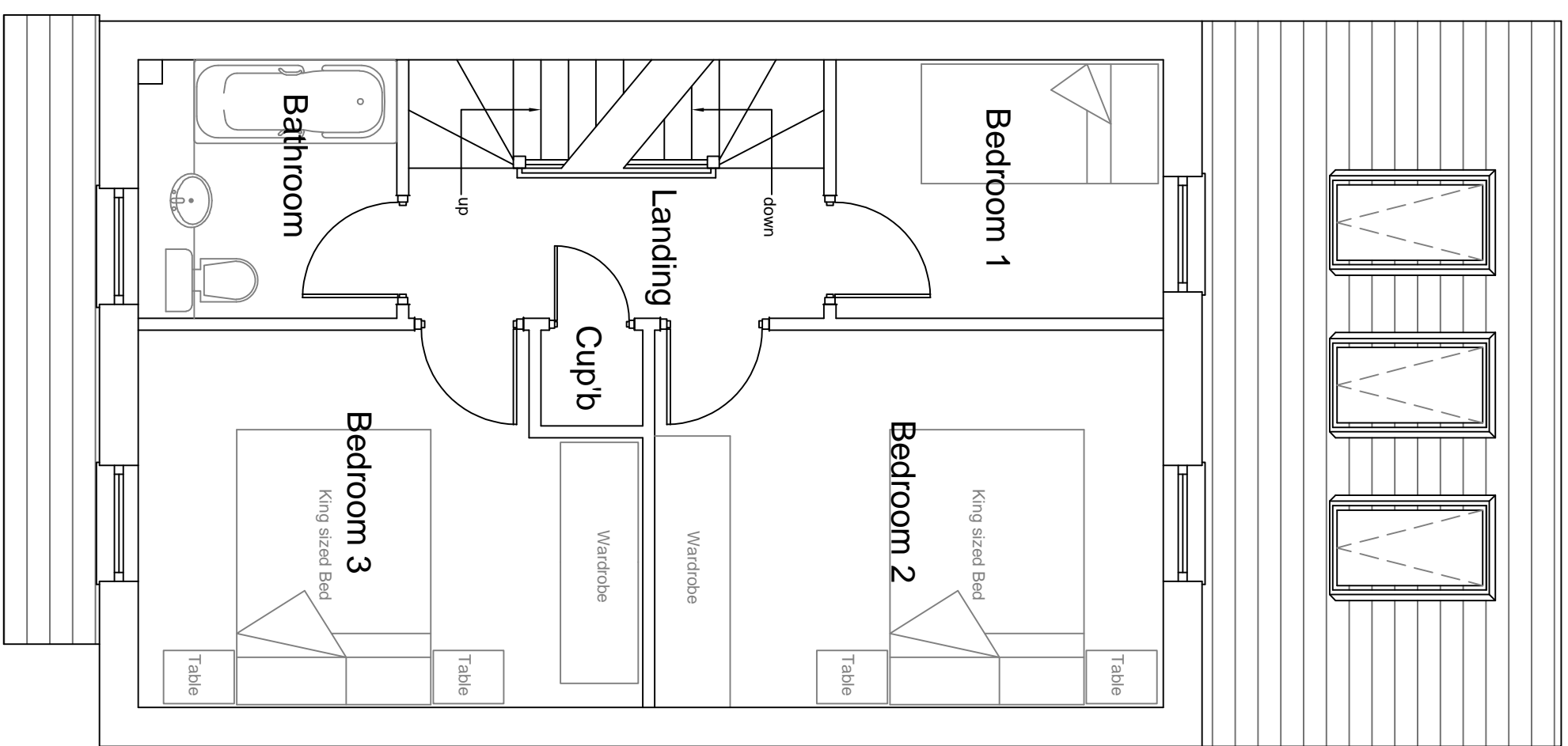
16. To ensure that the site is suitable for its intended end use and to remove any unacceptable risk to people/buildings/environment from contaminated land as per paragraph 121 of the National Planning Policy Framework.
17. To ensure that invasive species are removed from the site appropriately.
18. To ensure that the development maintains highway safety.
19. To ensure that the development preserved the residential amenity of neighbouring properties.

Informatives:

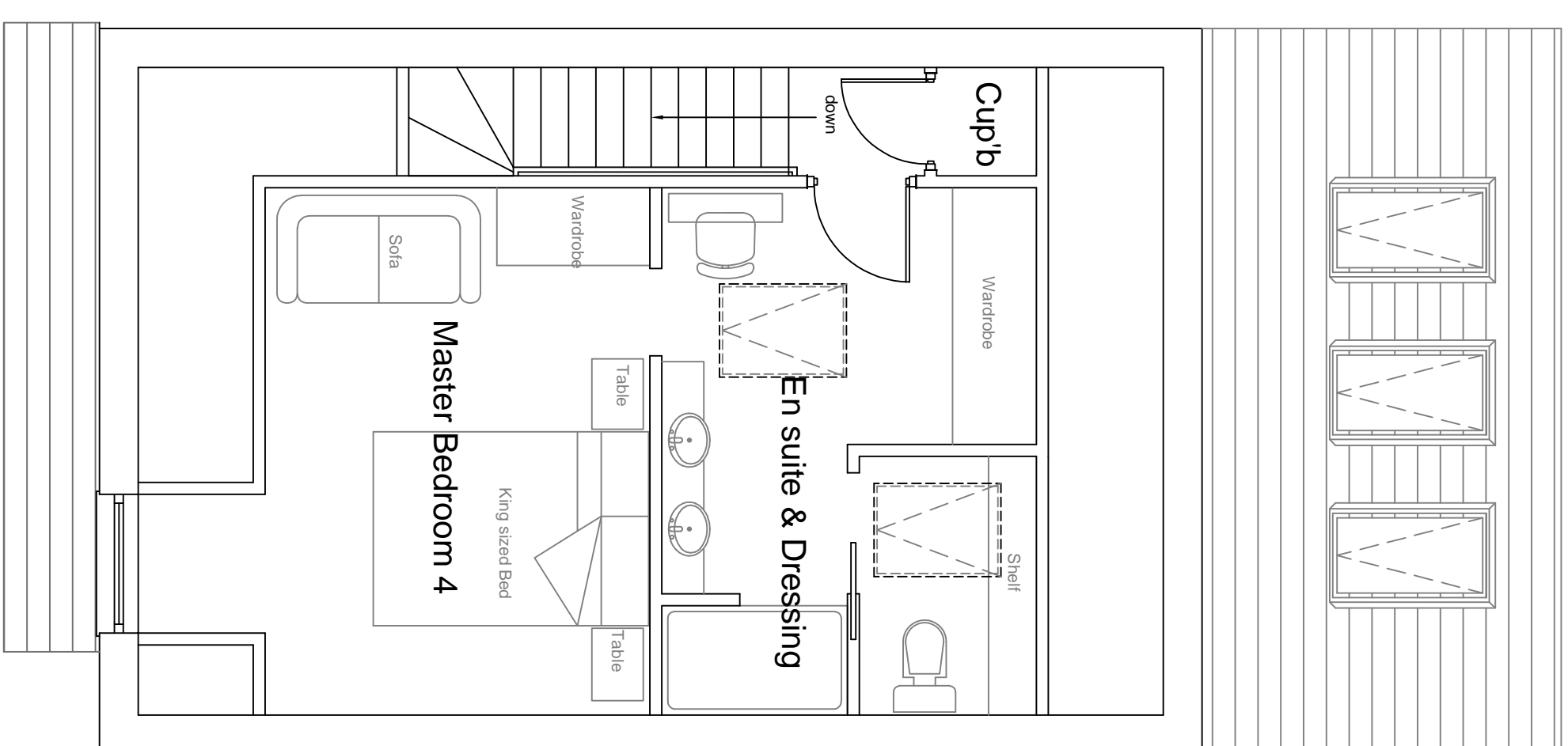
- 1) Section 106 Agreement to secure contribution towards off site green space provision and highway improvements.
- 2) Detailing the applicant's obligations in relation to avoiding harm to Protected species during the construction process.



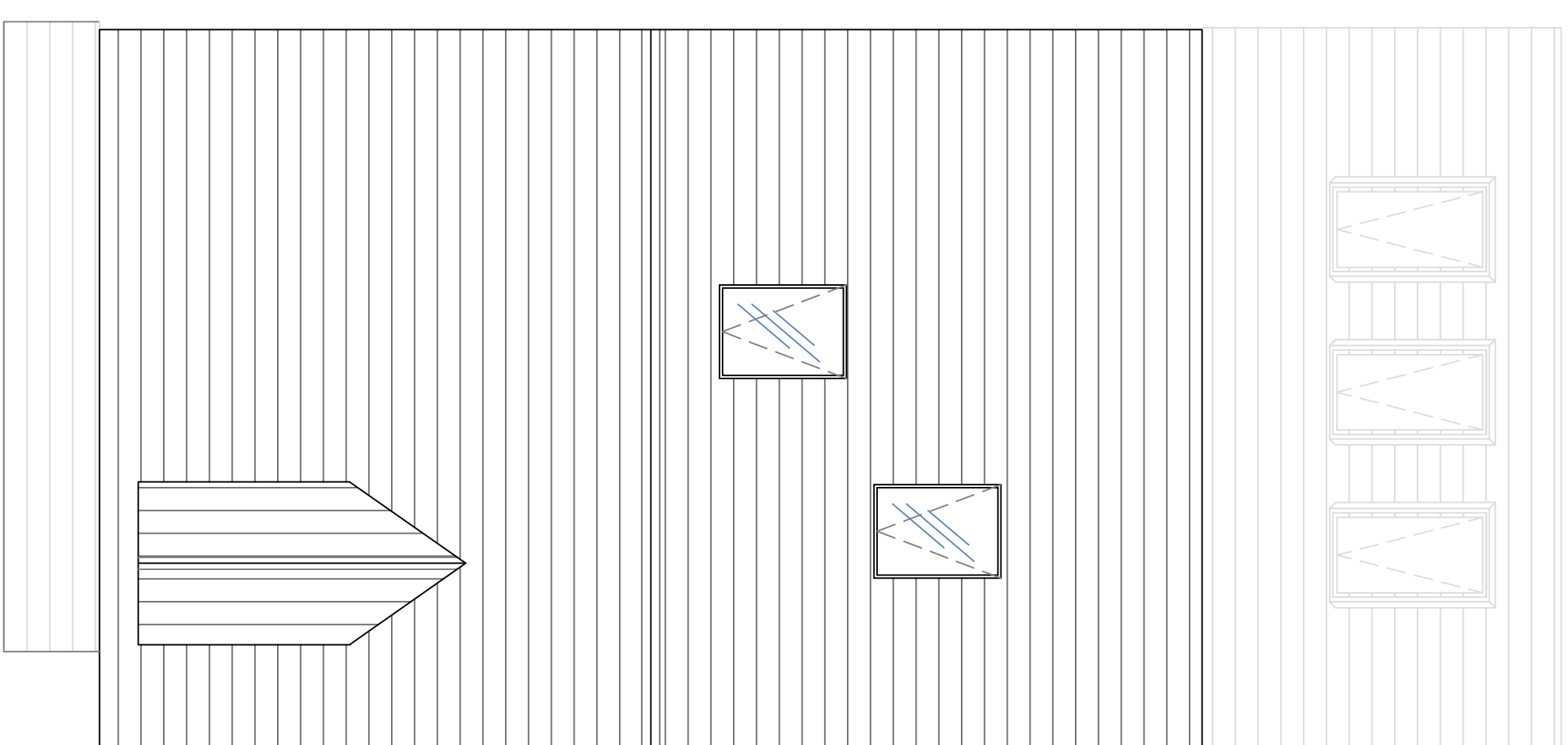
Proposed Ground Floor Plan



Proposed First Floor Plan



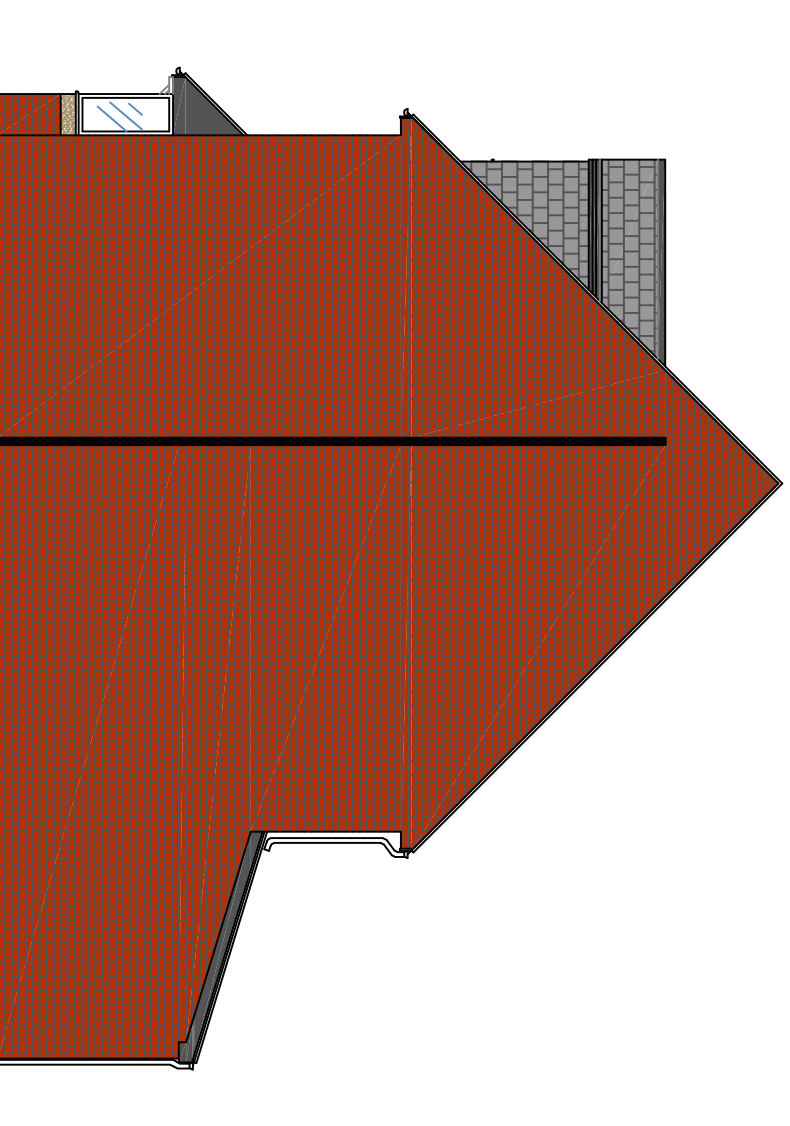
Proposed Second Floor Plan



Proposed Roof Plan



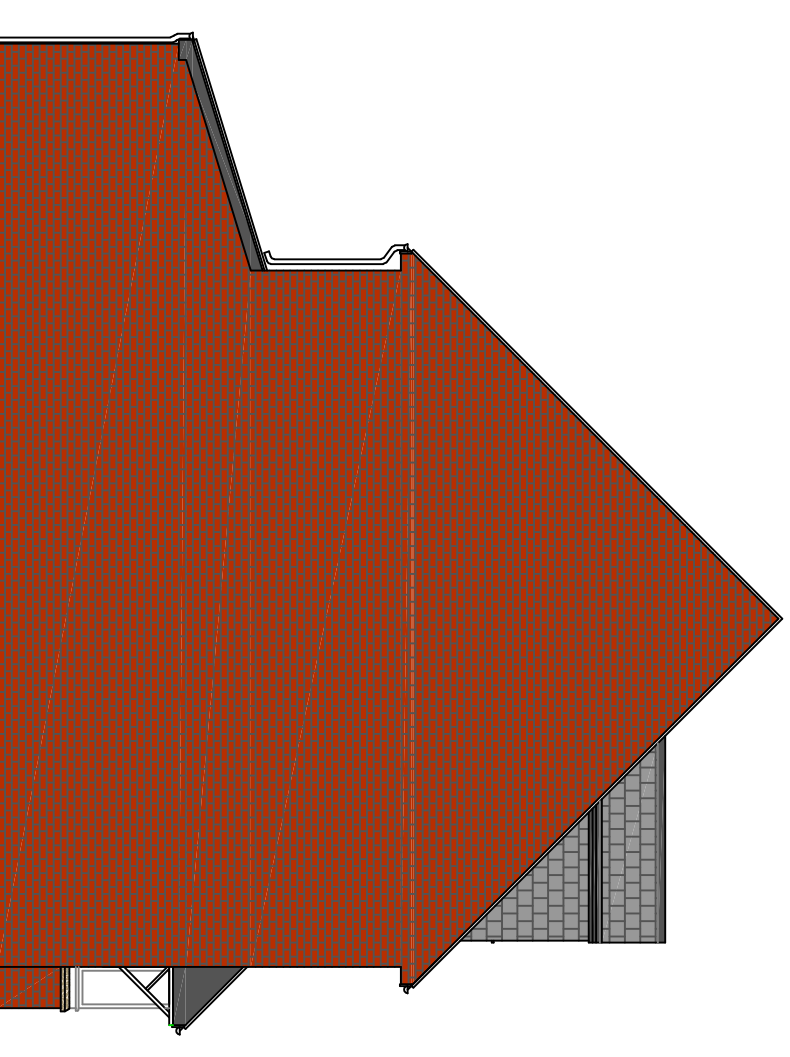
Proposed Front Elevation



Proposed Side Elevation



Proposed Rear Elevation



Proposed Side Elevation

HT2 D
4 Bedroom Detached
Plots: 9 10 11
Sq FT: 1460

Planning Drawing

Project Title :- Ridge New Dwellings to Land Adjacent Ridge Hill Lane Houses Plots 9, 10 & 11 Plans & Elevations		Client :- Castle Properties	
Dwg No :- 05/17/076	Date :- Oct 17	Rev :-	Drawn :- SCM
Scale :- 1:100 & 1:50 @ A1	Checked :-	Site Address :- Land Adjacent Ridge Hill Lane Slaykidge Tameside	

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Scale 1/1000 Date 15/5/2018

Centre = 395992 E 399144 N

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Application Number: 17/01033/FUL Ridge Hill Lane Stalybridge

Photo 1 – looking into the site from proposed access onto Ridge Hill Lane



Photo 2 – rear of properties fronting Ridge Hill Lane from location of proposed internal access road



Photo 3 – from central part of the site looking towards the top of the access road into the site.



Photo 4 – view of the rear of properties fronting on to Ridge Hill Lane



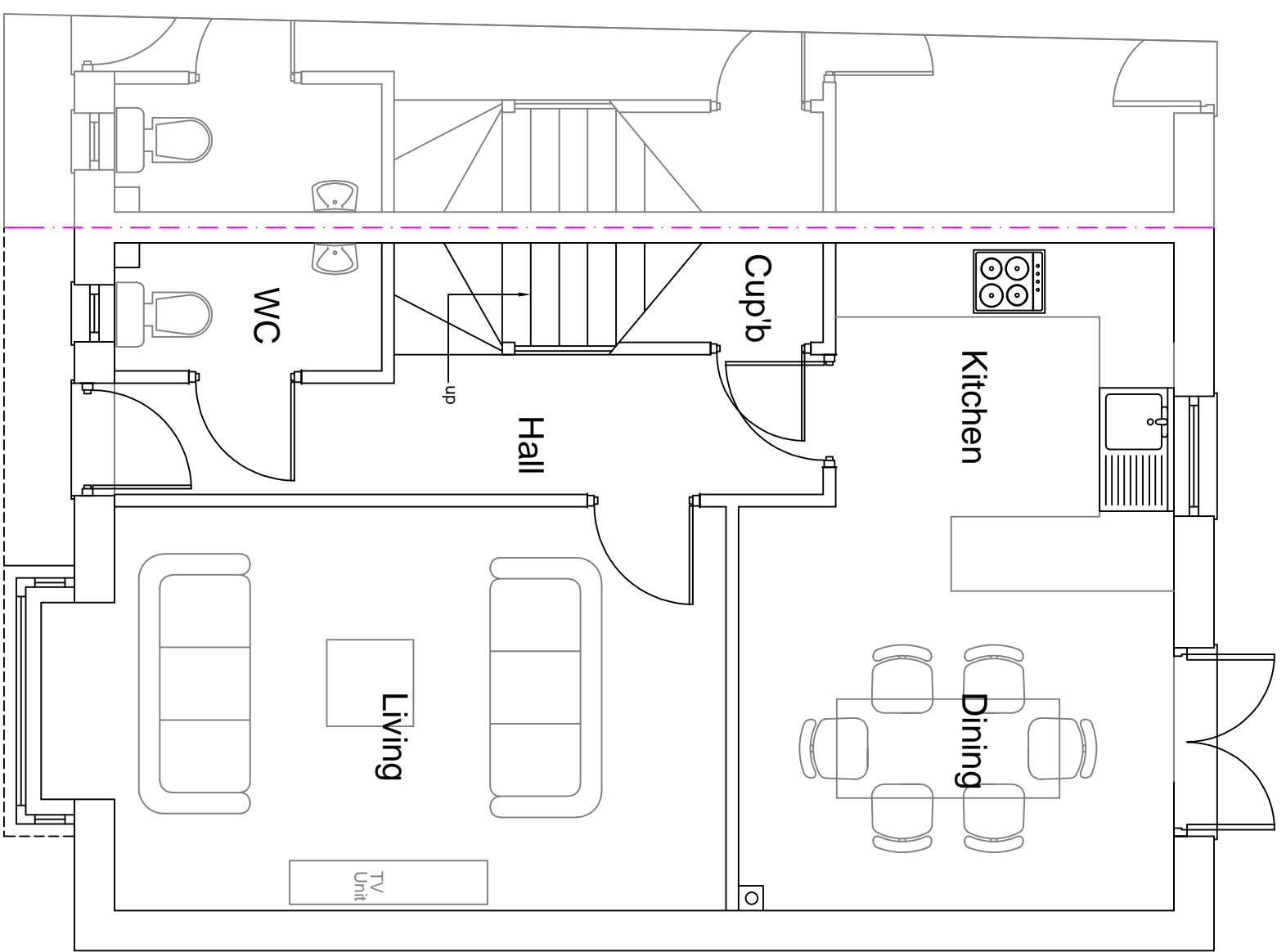
Photo 5 – looking towards north eastern edge from the centre of the site



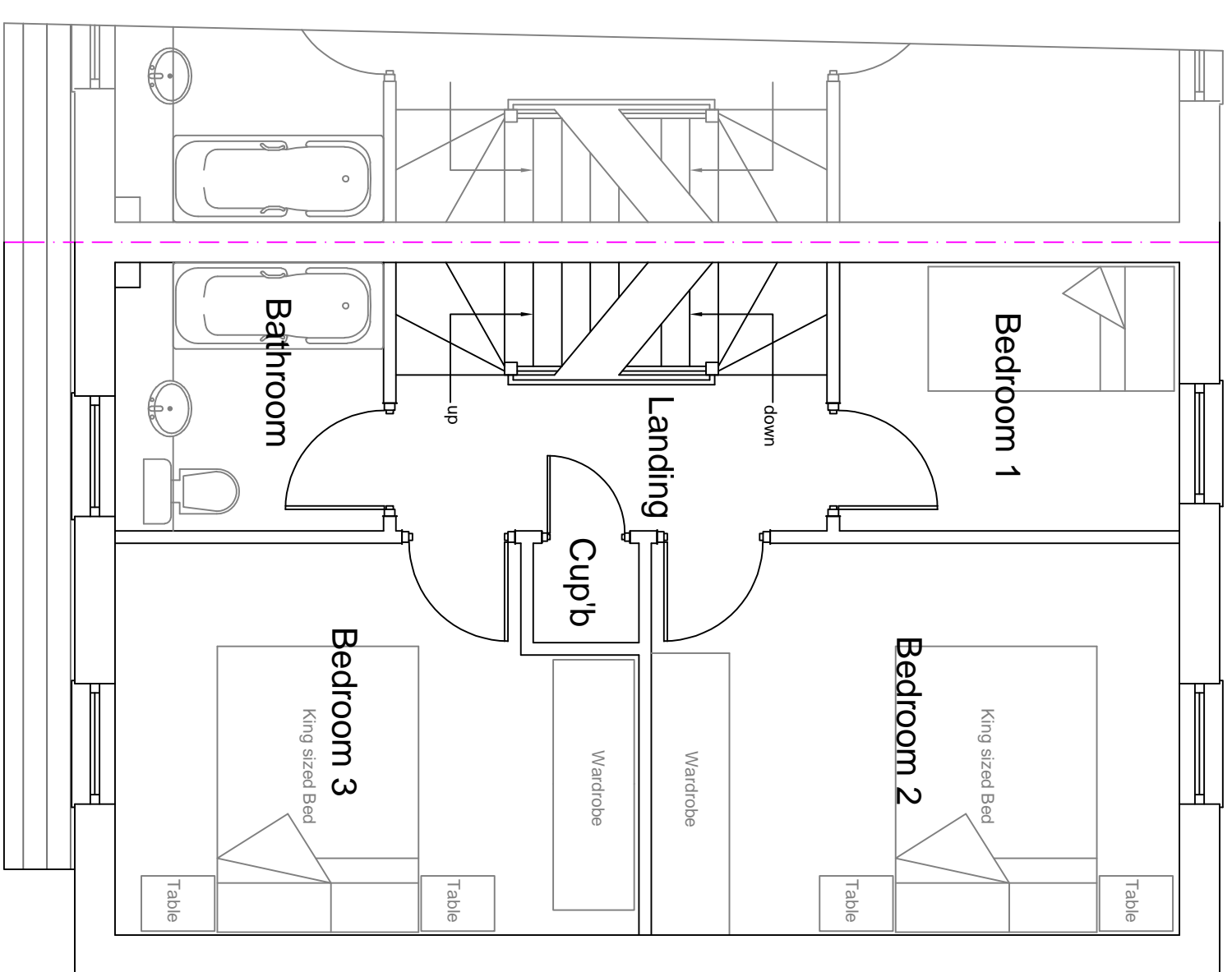
Photo 6 – neighbouring property adjacent to the north eastern boundary of the site.



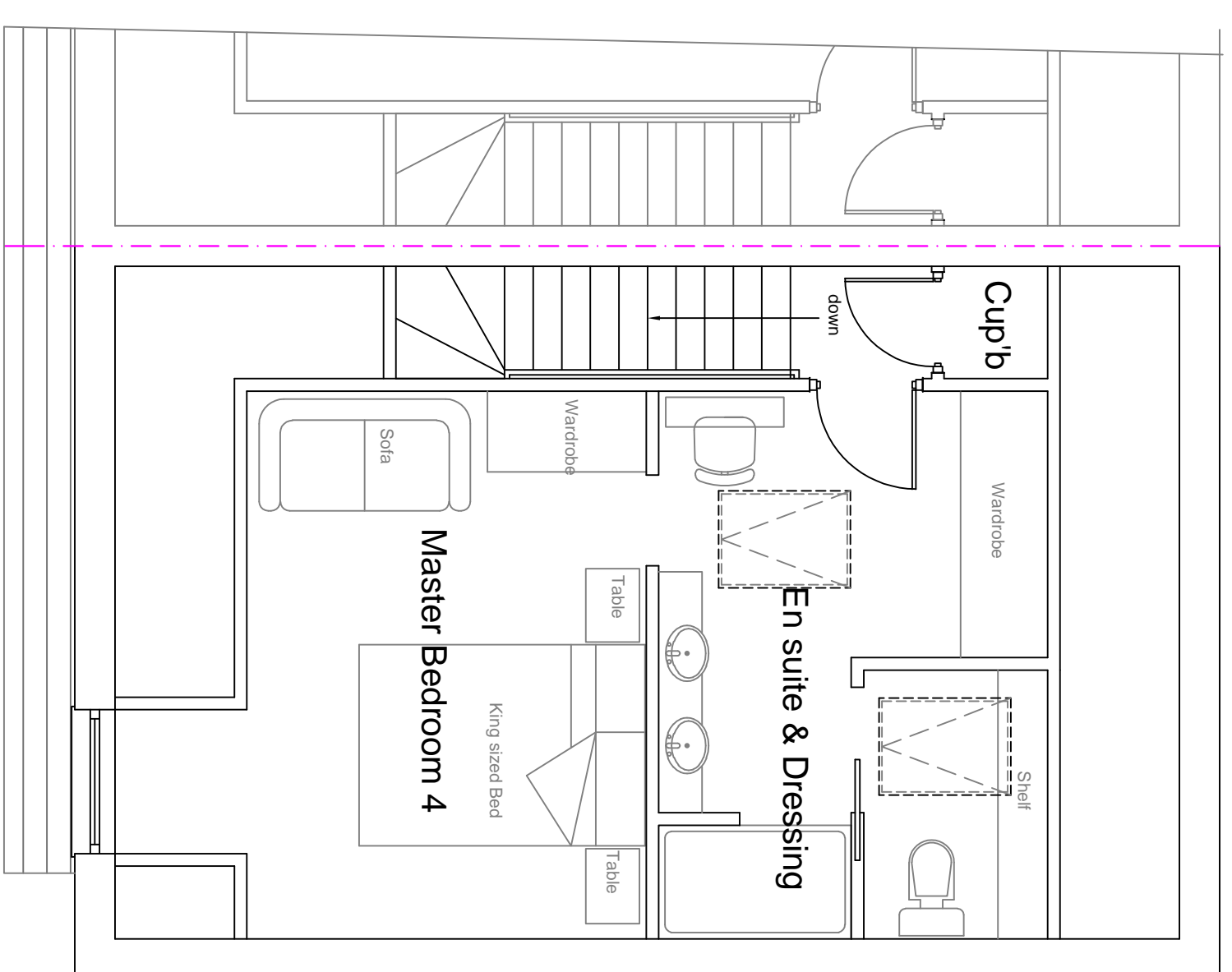
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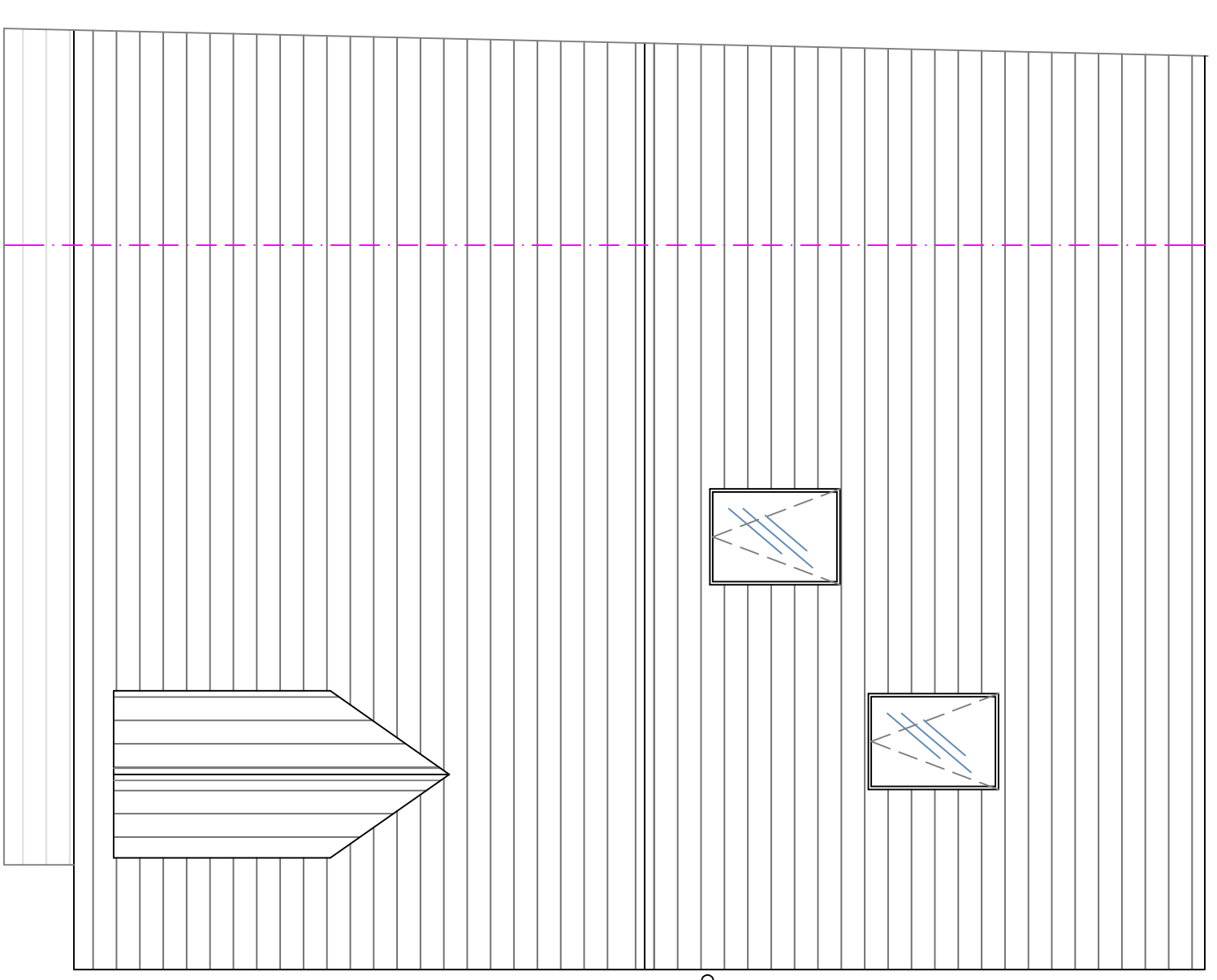
Proposed Ground Floor Plan



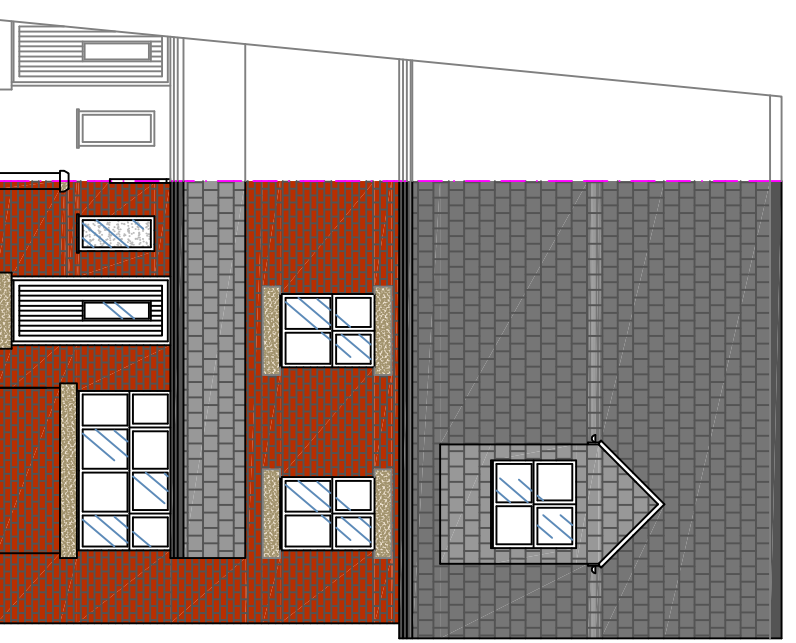
Proposed First Floor Plan



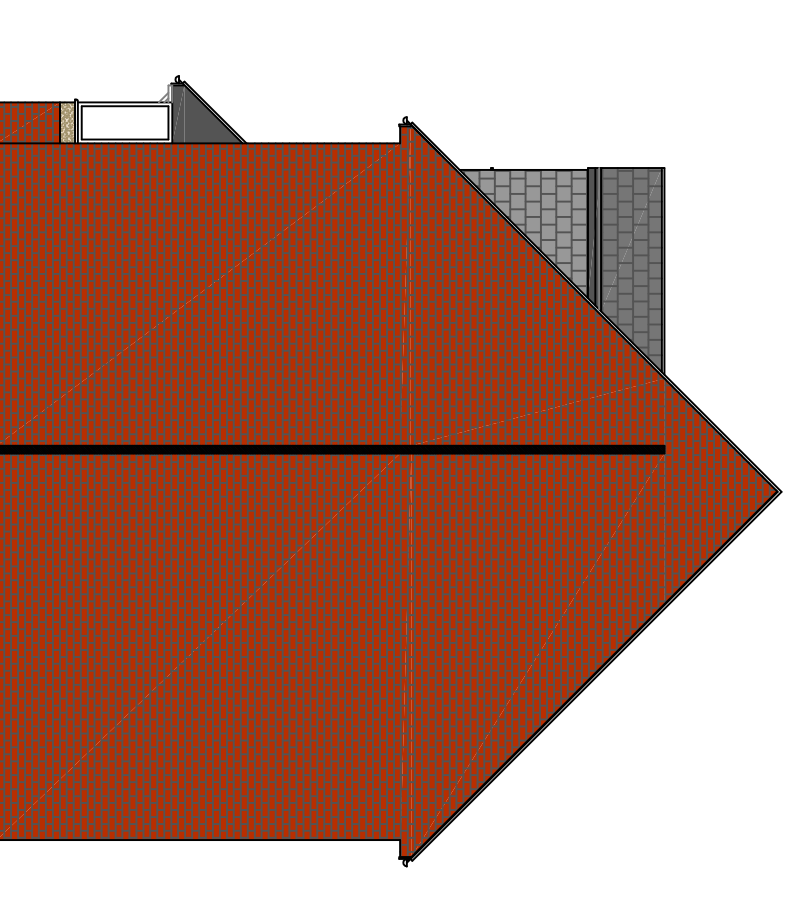
Proposed Second Floor Plan



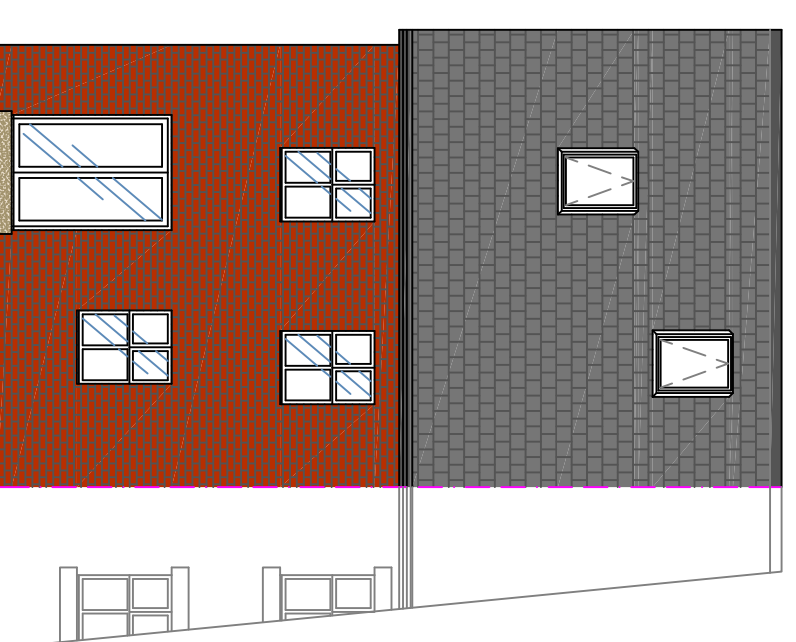
Proposed Roof Plan



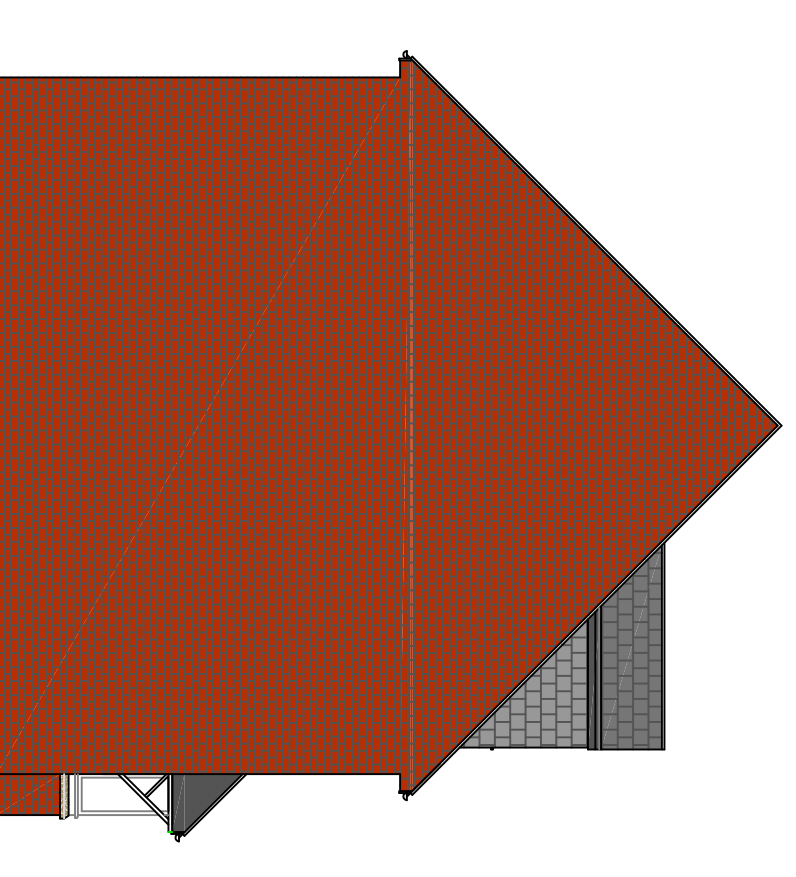
Proposed Front Elevation



Proposed Side Elevation



Proposed Rear Elevation



Proposed Side Elevation

HT2
 4 Bedroom Semi Detached
 Plots: 2 4 6 & 8
 Sq FT: 1300

Planning Drawing

Project Title :-		Ridge New Dwellings to Land Adjacent Ridge Hill Lane House No 2 Plots & Elevations.	
Client :-		Castle Properties	
Dwg No :-	04	Date :-	Oct 17
Rev :-	Preliminary	Drawn :-	SCM
Scale :-	1:100 & 1:50 @ A1	Checked :-	
Site Address :-		Land Adjacent Ridge Hill Lane Slaybridge Tamerside	

The Old Post Room
 34 Creek Valley Road
 Sleaford
 Notts
 NG13 7DA
 Tel: 01457 871202
 www.william-mccall.com | enquiries@william-mccall.com

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Timber Bin Store (Typ)

Dimensions
 Overall Width: 2400 mm
 Overall Depth: 1200 mm
 Overall Height: 2100 mm
 Internal Product Dimensions: 1100 x 650 x 650 (mm)
 Material: FSC Certified Pine

Refuse Storage

Dimensions
 Overall Width: 2400 mm
 Overall Depth: 1200 mm
 Overall Height: 2100 mm
 Internal Product Dimensions: 1100 x 650 x 650 (mm)
 Material: FSC Certified Pine

Landscaping Schedule			
Trees			
Key	Botanical Name	Common Name	Girth
BP	Betula Pendula	Birch	10-12cm
CF	Carpinus Betulus Fastigiata	Hornbeam	10-12cm
FE	Fraxinus Excelsior	Ash	10-12cm
SA	Sorbus Aucuparia	Rowan	10-12cm
Hedges			
Key	Botanical Name	Common Name	Girth
	Betula Pubescens	Birch	5%
	Crataegus Monogyna	Hawthorn	80%
	Ilex Aquifolium	Holly	10%
	Rosa Canina	Rose	5%
Wildflower Grassland Mix			
Key	Botanical Name	Common Name	Girth
	Papaver Rhoeads	Corn Poppy	10.0%
	Centaura Cyanus	Cornflower	16.0%
	Ranunculus acris	Meadow Buttercup	7.0%
	Malva Moschata	Musk Mallow	1.0%
	Leucanthemum Vulgare	Ox Eye Daisy	12.0%
	Plantago Diocia	Red Campion	12.0%
	Plantago lanceolata	Ribwort Plantain	12.0%
	Sanguisorba Minor	Salted Burnet	12.0%
	Prunella vulgaris	Self heal	10.0%
	Silene Alba	White Campion	3.0%
	Achillea Millefolium	Yarrow	5.0%



Wildflower Grassland Mix



Carpinus betulus fastigiata (Hornbeam)

Project Title :- New Dwellings to Land Adjacent Ridge Hill Lane Proposed Site Plan

Client :- Castle Properties

Dwg No :- 05/17/076

Date :- Nov '17

Scale :- 1:200 & 1:1250 @ A1

Site Address :- Land Adjacent Ridge Hill Lane, Selsbridge, Tameside

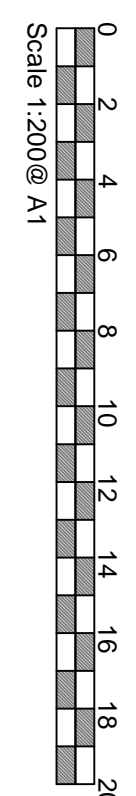
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 OL3 7DA
 Tel: 01457 871202
 www.william-mccall.com | enquiries@william-mccall.com

WM
 ARCHITECTURAL SERVICES

LABC
 Landscape Architecture & Building Consultancy

A1

Planning Drawing



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Application Number 17/01034/FUL

Proposal Full planning application for the erection of 116 no. dwellings with associated works including car parking, access road and landscaping

Site Vacant land at Edge Lane Droylsden

Applicant Droylsden Site Investments

Recommendation Approve, subject to conditions and the prior completion of a Section 106 Agreement

Reason for report A Speakers Panel decision is required because the application is a major.

REPORT

1. APPLICATION DESCRIPTION

1.1 The applicant seeks full planning permission for redevelopment of the land for 116 dwellings with associated works including car parking, access road and landscaping. The proposed housing mix would be as follows:

22 x 4 bed dwellinghouses

24 x 3 bed dwellinghouses

4 x 2 bed dwellinghouses

66 x 2 bed apartments (across 2 blocks)

1.2 The applicant has provided the following documents in support of the planning application:

- Air Quality Assessment
- Noise Assessment
- Arboricultural Impact Assessment
- Preliminary Ecological Appraisal
- Planning Statement
- Design, Access and Crime Impact Statement
- Transport Assessment and Travel Plan
- Phase I Site Investigation

1.3 In accordance with the Environmental Impact Assessment Regulations (as amended) 2017 Regulations, as the scheme proposes a development of less than 150 dwellings on a site below 5 hectares in area, the proposals are considered not to be Schedule 2 development. Therefore a Screening Opinion against the criteria in Schedule 3 of the Regulations is not required.

2. SITE & SURROUNDINGS

2.1 The application site is located on the junction of Edge Lane and Fairfield Road, to the south west of Droylsden Town centre and west of the Fairfield Conservation Area. Ashton Canal (a designated Site of Biological Importance) runs parallel with the eastern boundary of the site. A vacant former public house and single storey commercial building are situated adjacent to the south eastern corner of the site. Palisade fencing runs along the majority of the southern and western boundaries of the site, with 2 points of vehicular access taken from Edge Lane (the northern most of which is blocked by overgrown vegetation.)

2.2 There are extensive areas of hardstanding in the southern portion of the site, a clear reference to its previous industrial use. In terms of active adjacent uses, there is a car repair garage immediately beyond the north western boundary, Edge Lane Business Park is located to the west and a plant hire commercial use is located to the east. Residential

development, including development of part 3 storeys in height is located to the south of the site.

3. PLANNING HISTORY

- 3.1 05/00608/OUT – Outline planning application for mixed use development comprising 4no. apartment blocks and 1no. office block following demolition of existing industrial premises - refused. Two reasons were given for refusing the application, the first being that the proposal will result in the loss of an employment site and the second being the impact of the neighbouring commercial uses on the residential amenity of the future occupants of the development.
- 3.2 02/00060/FUL– erection of 5 Industrial units for B1 Use (business) B2 Use (General Industrial) and B8 (Storage and Distribution) – approved

4. RELEVANT PLANNING POLICIES

4.1 Tameside Unitary Development Plan (UDP) Allocation

Allocated under policy E3 as an Established Employment Area

4.2 Part 1 Policies

- 1.3: Creating a Cleaner and Greener Environment.
- 1.4: Providing More Choice and Quality Homes.
- 1.5: Following the Principles of Sustainable Development
- 1.6: Securing Urban Regeneration
- 1.10: Protecting and Enhancing the Natural Environment
- 1.12: Ensuring an Accessible, Safe and Healthy Environment

4.3 Part 2 Policies

- C1: Townscape and Urban Form
- E3: Development Established Employment Areas
- H2: Unallocated Sites (for housing)
- H4: Type, size and affordability of dwellings
- H5: Open Space Provision
- H6: Education and Community Facilities
- H7: Mixed Use and Density.
- H10: Detailed Design of Housing Developments
- MW11: Contaminated Land
- MW12: Control of Pollution
- MW14 Air Quality
- N1b: National Nature Conservation Sites
- N2: Locally Designated Nature Conservation Sites
- N3: Nature Conservation Factors
- N4 Trees and Woodland
- N5: Trees Within Development Sites
- N6: Protection and Enhancement of Waterside Areas
- N7: Protected Species
- OL7: Potential of Water Areas
- OL10: Landscape Quality and Character
- T1: Highway Improvement and Traffic Management.
- T10: Parking
- T11: Travel Plans.
- U3: Water Services for Developments

U4 Flood Prevention
U5 Energy Efficiency

4.4 **Other Policies**

Greater Manchester Spatial Framework - Publication Draft October 2016
Residential Design Supplementary Planning Document
Trees and Landscaping on Development Sites SPD adopted in March 2007
Employment Land SPD adopted January 2009

4.5 **National Planning Policy Framework (NPPF)**

Section 1 Delivering sustainable development
Section 6 Delivering a wide choice of high quality homes
Section 7 Requiring good design
Section 8 Promoting healthy communities

4.6 **Planning Practice Guidance (PPG)**

4.7 This is intended to complement the NPPF and to provide a single resource for planning guidance, whilst rationalising and streamlining the material. Almost all previous planning Circulars and advice notes have been cancelled. Specific reference will be made to the PPG or other national advice in the Analysis section of the report, where appropriate.

5. **PUBLICITY CARRIED OUT**

5.1 Neighbour notification letters were issued in accordance with the requirements of the Town and Country Planning (Development Management Procedure) (England) Order 2015.

6. **RESPONSES FROM CONSULTEES**

6.1 Highways England – no objections to the proposals.

6.2 Borough Environmental Health Officer (EHO) – some concerns relating to the noise generated by the waste transfer site to the north of the site on the future occupants of the proposed development. The Noise Impact Assessment submitted with the planning application details mitigation measures which are considered to be acceptable but further mitigation is required in relation to the impact of odour and dust migration. Standard conditions recommended relating to the hours of work during the construction phase of the development and details of refuse storage to serve the development.

6.3 Greater Manchester Ecology unit (GMEU) – no objections to the proposals following the submission of additional information relating to the potential impact on the biodiversity value of Ashton Canal running parallel with the north eastern edge of the site. A number of conditions are recommended to ensure adequate mitigation of the impacts of the development.

6.4 Transport for Greater Manchester (TfGM) – some concerns regarding the level of trip generation and the resulting impact of congestion arising from the development, although it is acknowledged that the junction between the access into the development and Edge Lane would not result in capacity issues on that section of the highway.

6.5 Local Highway Authority – no objections to the proposals following consideration of the Transport Assessment submitted with the application. A number of conditions are

recommended and a financial contribution towards highway upgrade works is to be secured through the Section 106 Agreement.

- 6.6. Greater Manchester Archaeological Advisory Service (GMAAS) – no objections to the proposals subject to a condition requiring a written scheme of investigation to be undertaken prior to the commencement of development, given that the historic records indicate that the site is of local significance in terms of industrial heritage.
- 6.7. United Utilities have raised no objections to the proposals subject to the imposition of conditions.
- 6.8. Greater Manchester Police (Design Out Crime Officer) – no objections received
- 6.9. Environment Agency – no objections subject to the imposition of a condition requiring the undertaking of an intrusive investigation into potential sources of contamination on the site and the approval of any necessary remediation prior to the commencement of development.
- 6.10. Borough Contaminated Land Officer: recommends that a standard contaminated land condition is attached to any planning approval granted for development at the site, requiring the submission and approval of an assessment into potential sources of contamination and a remediation strategy.
- 6.11. Borough Tree Officer – no objections to the proposals given the submission of an appropriate landscaping scheme.
- 6.12. Natural England – no comments to make on the application
- 6.13. Coal Authority - no objections to the proposals given that the site is not located in a high risk area with regard to coal mining legacy and no conditions are considered necessary

7. SUMMARY OF THIRD PARTY RESPONSES RECEIVED

- 7.1. No representations have been received in relation to the application.

8. ANALYSIS

- 8.1. The issue to be assessed in the determination of this planning application are:

- 1) The principle of development
- 2) The impact of the proposed layout, design and scale of the development on the character of the site and the surrounding area)
- 3) The impact upon the residential amenity of neighbouring properties
- 4) The impact on highway safety
- 5) The impact on the ecological significance of the site
- 6) The impact on flood risk and environmental health
- 7) Other matters

9. PRINCIPLE OF DEVELOPMENT

- 9.1. The application site forms part of a wider site designated as an Established Employment Area in the UDP. The proposals must therefore be considered against the criteria of policy E3.

- 9.2 The policy states that the conversion of such sites to residential or mixed use development will not be permitted unless it is considered that the need for housing and the regeneration benefits of such development outweigh the need to retain the site for employment purposes. The policy states that, in making this assessment, the following factors should be considered:
- (a) The quality and type of employment sites and premises available in the area
 - (b) Evidence of demand for employment sites and premises in the area
 - (c) The suitability of the site for further employment use in terms of size, physical characteristics, access, traffic impact, and sensitivity of surrounding land uses
 - (d) The opportunity which may be presented for new forms of employment as part of a mixed use scheme
- 9.3 The redevelopment of this site would not result in the loss of the entire Established Employment Area, with land to the east and west of the site to be retained within this designation. In considering the availability of other sites within Droylsden for employment uses, Little Moss Business Park is designated as an Established Employment Area, as is the former Robertsons Jam site. In addition, a Development Opportunity Area has been allocated to the south of Droylsden Town Centre and under the provisions of policy E2 is earmarked as suitable for uses including office and light industrial uses, which meet the definition of 'employment' uses.
- 9.4 There is no evidence to suggest that the site would not be suitable for employment purposes in environmental terms but it is the case that the site has been actively marketed for an extensive period. The applicant has provided details of the marketing exercise undertaken by Greenham Commercial Chartered Surveyors. The information confirms that marketing began in 2016, including an advertisement board on the site and electronic marketing on a number of national websites and circulation to Manchester based land agents.
- 9.5 Interest was generated but this was predominantly for redevelopment of the site for residential purposes. There was interest from 3 parties looking at commercial development of the site but initial enquiries did not result in offers, despite being followed up by the agent. The price sought could not be considered a deterrent, as the advert clearly stated that 'offers are invited' and so the lack of interest, within an 18 month period, would tend to indicate that factors other than the cost of the land are relevant to the lack of interest in returning the vacant site to employment use. Therefore, whilst it may be environmentally suitable, the evidence presented by the application indicates that the site is not a desirable location for continued use for employment purposes.
- 9.6 The Employment Land Review of 2013 indicated that the supply of industrial use properties was greater than demand for such premises in the Borough. The surplus of employment land has been reduced through the inclusion of some allocated employment land within the Strategic Housing Land Availability Assessment (SHLAA) (i.e. there is an assumption that some of these sites would be redeveloped for residential use).
- 9.7 The Housing and Employment Land Availability Report 2016 forms the most up to date position in relation to housing and employment supply in the Borough. The assessment explains that the overall housing requirement for Tameside between 2015 and 2035 is approximately 680 units per annum, 13,600 over the whole period.
- 9.8 One of the purposes of the review was to identify opportunities to boost the supply of housing on allocated employment sites as the anticipated supply position in 2015 was approximately 568 dwellings per annum, substantially below the target quoted above. The report identified 5 relatively large Established Employment Sites, including the land of which this application site forms a part, which are considered suitable to be considered for residential development.

- 9.9 These sites were assessed as being ‘...vacant with no commercial interest for employment (having been shown) for a considerable time.’ The report concluded that even if these allocated employment sites were re-developed for housing, there would still be a 45.55 hectare supply of employment land in the Borough and the deficit in housing supply would substantially reduce. The report does acknowledge that even with the uplift in numbers through developments of site such as this for residential use, the projections still fall short of a five year supply of housing land. Given that the other large scale sites included in the report (including the former Robertson’s Jam site in Droylsden) have not yet had planning applications for residential development approved, these factors weigh in favour of the proposals in principle, subject to the scheme meeting the definition of sustainable development.
- 9.10 Taking the above factors as a whole, it is considered that, whilst the surplus in employment land has reduced since 2013, this proposal would not result in the loss of the entire designated Established Employment Area within which the site is situated. Requiring the redevelopment of this site to include commercial uses in a mixed use scheme would likely reduce the number of residential units on the site. In addition, the low level of interest generated during the marketing of the site suggests that demand for this type of development in this location is not significant. It is therefore considered that the benefit of boosting the supply of housing in the Borough would outweigh the retention of part of the site for employment purposes in a mixed use development, addressing the requirements of criteria (d) of policy E3.
- 9.11 there are alternative areas within droylsden designated for employment use. whilst this list does include the former robertson’s jam site, which has also been included in the shlaa following the 2016 review, there is no live planning permission or application for the redevelopment of that site at the time of determining this application. the current designation of that site is therefore relevant to the determination of this application.
- 9.12 in addition to these factors, the weight to be attributed to the retention of the employment site must be considered in light of the inclusion of the site within the shlaa as contributing to the future housing needs of the borough in 2016, after the udp policy was adopted.
- 9.13 whilst it is acknowledged that an application was refused in 2005 partly on the basis that the employment site would be lost through redevelopment for alternative uses, as would be the case here, officers consider that there has been a material change in circumstances since that time. the inclusion of the site in the shlaa and the requirement of the nppf (introduced in 2012) to boost the supply of housing (including on brownfield sites in sustainable location such as this) are the principle considerations which lead officers to conclude that the principle of the loss of employment land is now considered to be acceptable.
- 9.14 the site is considered to be in a sustainable location, within 0.6 miles of edge lane tram stop, with regular services to manchester and ashton and regular bus routes also within walking distance of the site. the site is approximately 1 kilometre from droylsden district centre and 400 metres from openshaw district centre, which together provide a range of services and facilities.
- 9.15 in relation to density, the scheme proposes 116 dwellings on a site of approximately 1.3 hectares. this equates to a density of approximately 89 dwellings per hectare. given that the site is considered to be situated in a sustainable location, this density is considered to accord with policy h7 of the udp. the proposal constitutes the efficient use of land through the redevelopment of a brownfield site, both of which are factors encouraged by the nppf and weigh heavily in favour of the proposals.
- 9.16 in concluding the issue of the principle of development, following the above assessment officers consider that the weight to attributed to the loss of the employment site is limited

and the weight to be attributed to the need to boost the supply of housing in a sustainable location is substantial. on that basis, the principle of development is considered to be acceptable, subject to all other material considerations being satisfied.

10. CHARACTER OF THE SITE AND SURROUNDING AREA

- 10.1 Section 7 of the NPPF requires development to achieve good standards of design and states at paragraph 56 that 'Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people.' National policy also emphasises the importance of new development effectively responding to the character of the surrounding area.
- 10.2 The scheme proposes a block of apartments to provide a strong frontage to the Fairfield Road/Edge Lane junction. The majority of the block would be four storey, rising to five on the corner. The scheme also proposes a four storey block of apartments facing the canal on the north eastern edge of the development. The use of buildings of this height in these locations are considered to be positive elements of the scheme in design terms, providing a sense of scale and presence to prominent boundaries of the site, emphasising the link to the regeneration of the wider area.
- 10.3 The proposed houses would be 3 storeys in height across the scheme which would result in a logical scale to the overall development from public views. On the approach to the site from the west along Edge Lane, the 5 storey apartments would form the backdrop to the 3 storey buildings, clearly defining the edge of the site and resulting in a legible development. Similarly from the canal towpath adjacent to the north eastern boundary of the site and the public open space on the opposite side of the canal, the frontage apartments would provide strong definition to the edge of the development, with the 3 storey buildings set behind this providing a step down to the two storey dwellings along Edge Lane, further in the background from these views.
- 10.4 In terms of overall layout, the scheme proposes a relatively dense arrangement of properties in relatively short, linear rows. This is considered to respect the regular, 'grid' formation of buildings that is characteristic of residential development within this urban part of Droylsden. The proposal provides a consistent active frontage along the western boundary onto Edge Lane and the height of the canal side apartments would frame the view from the access road into the development, albeit at an oblique angle. Active frontage is provided to all of the access roads within the development, reinforcing a strong urban character and the appropriateness of the density of the development.
- 10.5 The use of integrated parking within the dwellings would reduce the extent of frontage parking, providing a positive design solution to a potentially negative side effect of a higher density layout. The communal parking courts associated with the two apartment blocks within the development would be overlooked due to the height of the blocks. Soft landscaping along the access road would help to soften the visual impact of the canal side apartment parking area from the entrance to the site. The most prominent long range view of the parking area associated with the apartments on the Edge Lane/Fairfield Road corner would be screened by the properties on the western edge of the development. This area would be visible from the southern boundary but this is a localised view, seen within the context of the surrounding development.
- 10.6 In terms of elevational treatment, the apartment blocks would be regular in form, with a strong vertical emphasis, reinforced through the fenestration and brick detailing. There would be a consistency in the massing and appearance of the two blocks, providing a legibility to the design of the overall scheme. A cladding would be applied to the exterior of the five storey section of the apartments on the Fairfield Road/Edge Lane corner to provide emphasis to the taller element of the building, which would help to break up the appearance

of the long elevation of the building, visible from Fairfield Road. Vertical emphasis and regularity of form would also be emphasised by the treatment of the elevations of the dwellinghouses.

- 10.7 Following the above assessment, it is considered that the proposals would be of an appropriate scale and massing, using buildings of scale to address key views of the site and resulting in a layout and form that respects the urban context of the site and its surroundings.

11. RESIDENTIAL AMENITY

- 11.1 In terms of the impact on neighbouring uses, the only residential properties in close proximity to the site are located on the opposite side of the road, to the south east of the site. Whilst the 4/5 storey element of the apartments fronting Fairfield Road would face southwards, the relationship with those neighbouring properties would be oblique, with the properties east of the site. Given this situation and the fact that Fairfield Road bisects the intervening distance, it is considered that the proposals would not result in unreasonable overlooking into or overshadowing of those properties. No other neighbouring residential dwellings would be adversely affected, with substantial separation distances retained to the terrace of properties on Edge Lane to the north west and the properties to the north of the canal.
- 11.2 In relation to the impact on the residential amenity of the future occupants of the development, 21 metres would be retained between the corresponding elevations of properties in 'back to back' relationships and 14 metres were blank elevations of a property face an elevation with habitable room windows in a neighbouring property. The separation distances to be retained between the two apartment blocks and the closest dwellinghouses would take account of the height of those buildings. The angle of the apartment building fronting the canal would also retain an oblique angle to the closest dwellinghouses, further reducing any potential impact. On that basis, the proposals would achieve the residential amenity of the future occupants of the development in terms of overlooking and overshadowing.
- 11.3 In relation to the impact of the neighbouring uses, the Environmental Health Officer has raised some concerns regarding the noise associated with the waste transfer site, although has not formally objected to the proposals. A noise impact assessment has been submitted with the application. The report concludes that a 3 metre high acoustic fence would need to be positioned along the northern boundary of the site in order to mitigate the impact of that neighbouring use on the occupants of the properties on the northern edge of the development. In addition, higher specification glazing and mechanical ventilation systems would need to be installed within the dwellings to mitigate the impact of noise sources, including traffic on the adjacent highways. The report details the minimum requirements in terms of the specification of these installations but does not give details of specific products. The exact specification of the mitigation measures to be installed can be secured by condition.
- 11.4 The EHO does not suggest that this would not be sufficient mitigation in relation to noise, but considers that this would not address odour or dust migration. The applicant has agreed to the imposition of a condition requiring a tree belt to be planted along the northern (outward facing) boundary of the acoustic fence. The Borough Tree Officer considers that there is sufficient space to plant a type of Hornbeam tree at regular intervals along the boundary. The condition would require trees to be semi-mature on planting, in order to reduce the prominence of the acoustic fence from public views and reduce the impact of odour and dust migration on the amenity of those properties.

- 11.5 In relation to impact on air quality, the site falls outside of an Air Quality Management Area. The applicant has submitted an Air Quality Assessment which identifies a number of dust related risks associated with activity during the construction phase of the development. Air quality issues relating to emissions from cars once the development is occupied are considered to be negligible. A number of mitigation measures to manage the impact of dust during the construction phase of the development are listed in Table 19 of the report. Subject to a condition securing compliance with these measures, there are no objections to the proposals with regards air quality.
- 11.6 On the basis of the above assessment, the proposals are considered to preserve the residential amenity of neighbouring properties and, subject to the imposition of conditions, would preserve the amenity of the future occupants of the development.

12. HIGHWAY SAFETY

- 12.1 The scheme proposes to take access from Edge Lane on the western boundary of the site, via a newly constructed access in approximately the same location as the northern most of the two existing access points on that boundary. The predicted trip generation for the overall development is 418 in a 12 hour period, with 44 trips in the AM peak and 55 in the PM peak.
- 12.2 The Transport Assessment assesses the potential impact of the development on the capacity of the surrounding road network, focussing on the junctions between the proposed access into the development and Edge Lane and between Edge Lane and Fairfield Road beyond the south western corner of the site.
- 12.3 In relation to the former, the modelling indicates that there would be significant spare capacity at that junction, even taking into account the worst case (85th percentile) scenario. This relates to both the impact of traffic turning into Edge Lane from the development and the queuing impact of traffic waiting to turn right into the development from Edge Lane during peak periods.
- 12.4 In relation to the latter junction, the report concludes that this junction would operate 'generally within capacity' during the AM peak, although queuing would increase to 4/5 vehicles towards the end of the 5 year period modelled. In the PM peak, the junction is considered to be operating close to capacity with the addition of the traffic generated by this scheme. The report concludes that the extent of the resulting queuing would not be 'severe.'
- 12.5 TfGM has highlighted the fact that the survey indicates that this junction is projected to be operating over capacity. However, the Local Highway Authority concur with the conclusions of the Transport Assessment that the impact would not be severe. On the basis of the guidance in paragraph 32 of the NPPF, planning permission should not be refused on this basis. Highways England has also confirmed that it has no objections to the proposals.
- 12.6 A number of conditions are recommended by the Local Highway Authority. Specific details of the road works and traffic management measures to be put in place to secure safe access to the site would be required as part of a Section 38 or Section 278 Agreement under the Highways Act and it is considered not to be necessary to impose this condition on the planning permission therefore. For the same reason, a condition survey of the highway is considered not to be necessary in planning terms.
- 12.7 However, details of measures to be included within a construction environment management plan to manage the impact of traffic and materials storage during the construction phase of the development is considered to be necessary. In addition, a requirement for the parking to be laid out as shown on the approved plans prior to the first

occupation of any part of the development is considered to be reasonable. It is considered reasonable to restrict the permitted development rights for means of enclosure to the front of the plots facing Edge Lane to ensure adequate visibility splays are maintained, including pedestrian visibility splays.

- 12.8 In relation to parking provision, each of the dwellings houses would be served by 2 parking spaces (one space being provided in integrated garages.) Of these units, 26 would have 4 bedrooms. In order to meet the maximum standard within the Residential Design Guide SPD, 3 spaces should be provided for units of that size in this location, with 2 being sufficient for smaller properties. In relation to the 66 apartments proposed across the scheme, each would be 2 bed and allocated 1 space. The proposal would fall short of the maximum standards set out in the SPD.
- 12.9 However, the harm arising from this deficit is considered to be limited by two key considerations. These are maximum standards, the application of which is no longer consistent with national policy and this site is considered to be located close to regular public transport links to Manchester and Ashton via both tram and bus. In addition to these factors, bike storage areas are indicated on the proposed plans adjacent to each apartment block and the integrated garages within the dwellinghouses would provide space for cycle storage on those plots. For these reasons, it is considered that the proposals would not result in a severe impact on highway safety with regard to the level of parking provision.
- 12.10 The applicant has submitted a Travel Plan in support of the application which includes measures to promote sustainable modes of transport, including cycling and car share initiatives. Specific measures include the provision of residents of the development with a 'welcome pack' detailing public transport options available as alternatives to the private car for trips from the site. Compliance with the measures in the Travel Plan can be secured by condition.
- 12.11 On the basis of the above assessment, it is considered that the proposals would not result in a severe impact upon highway safety, subject to the imposition of conditions

13. ECOLOGY

- 13.1 Ashton Canal, which runs parallel with the north eastern boundary of the site, is designated as a Site of Biological Importance. GMEU reviewed the Preliminary Ecological Appraisal submitted with the planning application. Whilst the report recommended that further survey work be undertaken regarding the potential impact of development on the otter population, GMEU consider this not to be necessary given the lack of evidence of otter activity in the locality. The loss of a Black Poplar tree, which is a Greater Manchester Biodiversity Species should be mitigated by the landscaping scheme associated with the proposed development and this requirement can be secured by condition.
- 13.2 Following the submission of additional information relating to the potential impact of overshadowing from the development on the biodiversity value of the canal, GMEU are satisfied that the impact of the proposals would not result in harm that would warrant refusal of the application. A condition is recommended to secure a management plan detailing measures to be put in place during the construction phase of the development to mitigate any potential adverse impact on the canal. This is considered to be reasonable and can be attached to the decision notice.
- 13.3 The Ashton Canal is highly likely to be used as a forging corridor by bats. Therefore the lighting scheme of the site adjacent to the canal would need to be carefully designed to prevent any additional light pollution reaching the canal. Details or external lighting to be provided within the development can be secured by condition. Conditions can also be attached to the decision notice limiting the timing of tree and vegetation removal from the

- site, a management plan for the control and removal of Japanese Knotweed from the site and the provision of biodiversity enhancements to mitigate the impact of the development.
- 13.4 Following the above assessment it is considered that any potential adverse impact on protected species and the biodiversity value of the site can be adequately mitigated through measures that can reasonably be secured by condition.

14. FLOOD RISK

- 14.1 The applicant has submitted a Flood Risk Assessment and Drainage Strategy. The site is in Flood Zone 1 and is therefore considered to be at a lower risk of flooding. The proposed Drainage Strategy indicates a number of measures that could be incorporated to provide a sustainable means of draining surface water from the site, without reliance on the mains network. These include sedum roofs on the apartment buildings, detention basins and swales and a hydro-brake system to slow the water run off rate before discharge into the canal. These details can only be finalised once the full extent and exact nature of ground contamination on the site is known.
- 14.2 Given the above, a condition requiring the submission of a sustainable surface water drainage strategy to serve the development, including details of management and maintenance is considered reasonable and necessary.
- 14.3 United Utilities have raised no objections to the proposals subject to the imposition of conditions. The requirement for surface water and foul water to be drained via different mechanisms is considered to be reasonable. The EA have no objections to the proposals in this regard.
- 14.4 It is considered that the proposed development would not result in a harmful impact in relation to flood risk.

15. ENVIRONMENTAL HEALTH

- 15.1 In addition to the comments regarding the impact of adjacent uses discussed previously in this report, the EHO has recommended conditions limiting the hours of works during the construction phase of the development and details of the refuse storage arrangements be attached to the decision notice. Given that residential properties are located on the approaches to the site along Edge Lane and Fairfield Road, the suggested limitation on construction hours is considered to be reasonable.
- 15.2 Whilst areas for communal bin storage associated with the apartments are annotated on the proposed plans and the curtilage of each of the dwellinghouses would be sufficient to provide screened refuse storage, the details of the exact size and means of enclosure have not been provided. On that basis, the imposition of this condition is considered reasonable.
- 15.3 The applicant has submitted a Phase I ground investigation report which highlights the likely sources of ground contamination on the site given the historic industrial use of the land. On the basis of the information submitted, the Borough Contaminated Land Officer has not raised any objections to the proposals, subject to securing further investigation work by condition. A condition requiring this additional work to be undertaken and that any necessary mitigation measures are agreed and implemented prior to the commencement of development is considered to be reasonable given the brownfield nature of the site.
- 15.4 The Coal Authority has raised no objections to the proposals given that the site is not located in a high risk area with regard to coal mining legacy and no conditions are

considered necessary. An informative detailing standing advice to the applicant can be attached to the decision notice.

16. OTHER MATTERS

- 16.1 In relation to impact on trees and landscaping, the proposals would result in the loss of 8 Category B trees, which are considered to be of moderate amenity value. The scheme proposes planting along the main route into the development, improving the quality of the public realm. The species to be planted include Alder, Silver Birch and Rowan. Soft landscaping would also be incorporated into the communal parking areas, with Beech hedges used to divide the front gardens of the dwellings. Full details of the number of specimens to be planted, the species mix of the trees, hedges and shrubs and their height on planting have been provided.
- 16.2 The Tree Officer considers that the proposed landscaping scheme would adequately mitigate the loss of existing trees and has no objections to the proposals. Given the advice from GMEU that the Black Poplar trees to be lost should be replaced with the same species and the need to secure the additional planting to screen the acoustic fence along the northern boundary of the site (both matters discussed previously in this report), it is considered necessary to condition the submission of full details of the replacement landscaping scheme. Details of on-going maintenance can also be secured by condition.
- 16.3 In relation to crime impact, the Design and Access Statement addresses how the development has been designed to reduce the risk of crime. The layout would present active frontages to the Edge Lane and Fairfield Road boundaries as well as the internal roads within the scheme. The apartments adjacent to the canal side would overlook the canal towpath. The layout therefore provides for natural surveillance of entry points into the site and the public realm within the development. Security features would be provided for each of the dwellings and external lighting is proposed within parking areas and the public realm. On the basis that these measures are complied with (to be secured by condition), it is considered that the proposals would make sufficient provision for designing out crime.
- 16.4 In relation to the potential impact of the development of features of archaeological importance, GMAAS has indicated that there is evidence of a cotton factory on the site in the 1790's, with extensive development on the site during the 19th Century, with a mixture of industrial and domestic structures present. Uses on the site since that time are considered unlikely to be significant in archaeological terms. On the basis of past research, GMAAS are content that the completion of a written scheme of investigation into the archaeological significance of the site and agreement of any necessary mitigation can be secured via a pre-commencement condition. Such a condition can be attached to the decision notice.
- 16.5 In relation to financial contributions required to mitigate the impact of the development, the applicant will be required to make a contribution to the provision of open space within the local area, in accordance with policy H5 of the adopted UDP. The contribution from this development is £230,332.28, based on the Council's Developer Contributions formula, which shall be secured via a Section 106 Agreement. A scheme for improvements to the play facilities within public open space in the Sunny Bank Park/Fairfield area of Droylsden is the project that has been identified for this contribution.
- 16.6 A contribution of £75,445.41 towards the upgrading of the Edge Lane/Fairfield Road junction can be secured through the Section 106 Agreement. This is considered to offset the impact of the development on the capacity of the highway network.
- 16.7 In relation to other infrastructure, where a proposal exceeds 25 dwellings. Policy H6 requires financial contributions towards education and other community facilities where

current facilities do not have the capacity to meet the additional population of a proposed development. In this case, a sum of £98,793.18 is to be allocated to a project to extend Aldwyn Primary School, which is approximately 1.5 miles from the site. The total cost of the project is £2.5 million, with the objective being to increase the capacity of the school from an intake of 45 pupils per year to 60 pupils per annum. There has been one pooled contribution to this project, attached to the redevelopment of the former Mono Pumps site, but there remains a significant shortfall, which this scheme shall contribute towards

- 16.8 These contributions are considered to meet the CIL regulations in that they are necessary to make the development acceptable in planning terms (given the limited amenity space to be provided on site, the additional traffic to be generated and the additional demand for school places), directly related to the development (as the close proximity ensures that residents are likely to use these facilities) and proportionate in that the sum is based on the size of the development.

17. CONCLUSION

- 17.1 The principle of development is considered to be acceptable. Officers are of the view that the inclusion of the site within the SHLAA and the requirement to boost the supply of housing in the Borough (particularly through the redevelopment of brownfield sites), in accordance with the guidance within the NPPF outweigh the requirements of UDP policy E3 with regard to the protection of Established Employment Areas. In reaching this conclusion, officers have had regard to the 18 month period of advertising the site, with no offers made for a continuation of the employment use of the land, despite the 'offers invited' nature of the advertisement and the fact that the NPPF postdates the UDP.
- 17.2 The proposed layout is considered to be acceptable, providing presence and vertical emphasis to the prominent edges of the site. The scale of the buildings and the elevational treatment of the scheme are considered to be appropriate and would result in a significant improvement to the appearance of the area and contribute to the wider regeneration of this part of Droylsden.
- 17.3 It is acknowledged that the level of trip generation would result in some congestion at junctions adjacent to the site. However, it is considered that the extent of the harm arising from this element of the scheme would not meet the 'severe' level required by the NPPF for planning permission to be refused on highway safety grounds. The Local Planning Authority has not objected to the proposals. The harm arising from the additional trips is considered to be mitigated through the significant contribution to the upgrade of the Edge Lane/Fairfield Road junction to be secured through the Section 106 Agreement. The level of parking provision is considered to be adequate given the close proximity of the site to public transport connections and services and facilities.
- 17.4 It is acknowledged that a number of the surrounding uses are commercial and that the waste transfer site to the north in particular could result have a detrimental impact on the residential amenity of the future occupants of the development. However, mitigation measures can be secured through the provision of acoustic fencing along the northern boundary, with landscape planting along the northern edge of that fencing. In addition, higher specification glazing and mechanical ventilation can be installed within the units to reduce the impact of noise generated by neighbouring uses and the adjacent highways. On that basis, the EHO has not objected to the proposals.
- 17.5 In weighing up all of the material planning considerations, officers consider that the proposals accord with the relevant national and local planning policies quoted above.

RECOMMENDATION

Grant planning permission, subject to the completion of a Section 106 Agreement to secure the following contribution:

Green Space - £230,332.28 towards improvements to the play facilities within public open space in the Sunny Bank Park/Fairfield area of Droylsden

Highway works: £75,445.41 to secure improvements to the junction of Fairfield Road and Edge Lane.

Education provision: £ 98,793.18 towards the cost of an extension to Aldwyn Primary School

A Management and Maintenance plan relating to the upkeep of communal green space and parking areas within the site.

and the following conditions:

1. The development must be begun not later than the expiration of three years beginning with the date of this permission.
2. The development hereby approved shall be carried out in accordance with the following approved plans/details:

1:1250 Site location plan (Drawing no. 001)

Proposed site plan (Drawing no. P-100 Rev. 01)

Proposed general arrangement plan (Drawing no. UG_11612_LAN_GA_DRW_01 Rev. 1)

Proposed Fairfield Apartment block elevations plan (Drawing no. P-210 Rev. 00)

Proposed Fairfield Apartment block ground and 1st floor (Drawing no. P-120 Rev. 00)

Proposed Fairfield Apartment block 2nd/3rd/4th floor plans (Drawing no. P-121 Rev. 00)

Proposed Canalside Apartment block elevations plan (Drawing no. P-211 Rev. 00)

Proposed Canalside Apartment block ground and 1st floor (Drawing no. P-122 Rev. 00)

Proposed Canalside Apartment block 2nd and 3rd floor plans (Drawing no. P-123 Rev. 00)

Proposed house types A and B plans and elevations (Drawing no. P-110 Rev.00)

Proposed house type C plans and elevations (Drawing no. P-111 Rev.00)

Proposed hard landscaping plan (Drawing no. UG_11612_LAN_HL_DRW_02 Rev. 1)

Proposed hard and soft landscaping details produce by Urban Green

Proposed Landscape Strategy (Rev. A dated November 2017) produce by Urban Green

Proposed soft landscaping plan (Drawing no. UG_11612_LAN_SL_DRW_03 Rev. 1)

Plan entitled Canal Linkage produced by Urban Green

3. No development, other than site clearance and site compound set up, shall commence until such time as the following information has been submitted in writing and written permission at each stage has been granted by the Local Planning Authority.
 - i) A preliminary risk assessment to determine the potential for the site to be contaminated shall be undertaken and approved by the Local Planning Authority. Prior to any physical site investigation, a methodology shall be approved by the Local Planning Authority. This shall include an assessment to determine the nature and extent of any contamination affecting the site and the potential for off-site migration.
 - ii) Where necessary a scheme of remediation to remove any unacceptable risk to human health, buildings and the environment (including controlled waters) shall be approved by the Local Planning Authority prior to implementation.

iii) Any additional or unforeseen contamination encountered during development shall be notified to the Local Planning Authority as soon as practicably possible and a remedial scheme to deal with this approved by the Local Planning Authority.

iv) Upon completion of any approved remediation schemes, and prior to occupation, a completion report demonstrating that the scheme has been appropriately implemented and the site is suitable for its intended end use shall be approved in writing by the Local Planning Authority.

The discharge of this planning condition will be given in writing by the Local Planning Authority on completion of the development and once all information specified within this condition and other requested information have been provided to the satisfaction of the Local Planning Authority and occupation/use of the development shall not commence until this time, unless otherwise agreed by the Local Planning Authority.

4. Notwithstanding any description of materials in the application, no above ground construction works shall take place until samples and/or full specification of materials to be used: externally on the buildings; in the construction of all boundary walls, fences and railings; and, in the finishes to all external hard-surfaces have been submitted to, and approved in writing by, the local planning authority. Such details shall include the type, colour and texture of the materials. Development shall be carried out in accordance with the approved details.
5. The car parking spaces to serve the development hereby approved shall be laid out as shown on the approved proposed site plan (Drawing no. P-100 Rev. 01), prior to the first occupation of any of the dwellings hereby approved and shall be retained free from obstruction for their intended use thereafter.
6. Prior to the first occupation of the dwellings hereby approved, details of the boundary treatments to be fully installed as part of the development shall be submitted to and approved in writing by the Local Planning Authority. The details shall include scaled plans of the treatments and details of the construction material and the finish to be applied. The boundary treatments for each dwelling shall be installed in accordance with the approved details prior to the first occupation of that dwelling.
7. No development shall commence until such time as a Construction Environment Management Plan has been submitted to and approved in writing by the Local Planning Authority. This shall include details of:
 - Wheel wash facilities for construction vehicles;
 - Arrangements for temporary construction access;
 - Contractor and construction worker car parking;
 - Turning facilities during the remediation and construction phases;
 - Details of on-site storage facilities;

The development shall be carried out in accordance with the approved Construction Environmental Management Plan.

8. Notwithstanding the details shown on the approved plans, no part of the development hereby approved shall be occupied until details of the means of storage and collection of refuse generated by the development have been submitted to and approved in writing by the Local Planning Authority. The details shall include scaled plans showing the location of storage and the means of enclosure. The bin storage arrangements for each dwelling shall

be implemented in accordance with the approved details prior to the occupation of that dwelling and shall be retained as such thereafter.

9. Notwithstanding the details submitted with the planning application, no above ground development shall commence until full details of a scheme of hard and soft landscaping to be incorporated into the development hereby approved have been submitted to and approved in writing by the Local Planning Authority. The scheme shall include the following specific measures:
 - A plan showing the location of all trees/hedges/shrubs to be planted, details of the species mix (to include Black Poplar trees), the number of specimens to be planted, spacing between them and their height on planting
 - Details of the location, number and species mix of trees to be planted on the northern edge of the acoustic fence to be installed on the northern boundary of the site and their height on planting (the height indicating that the trees are semi-mature on planting)
 - A plan showing the location and construction material of all hard surfacing.

The landscaping scheme shall be implemented in accordance with the approved details prior to the first occupation of any of the dwellings hereby approved.

10. The approved scheme of landscaping scheme shall be implemented before the first occupation of any part of the development or in accordance with a programme agreed previously with the local planning authority. Any newly planted trees or plants forming part of the approved scheme which, within a period of 5 years from the completion of the planting, are removed, damaged, destroyed or die shall be replaced in the next appropriate planting season with others of similar size and species.
11. Prior to the commencement of any development, a surface water drainage scheme, based on the hierarchy of drainage options in the National Planning Practice Guidance with evidence of an assessment of the site conditions shall be submitted to and approved in writing by the Local Planning Authority. The surface water drainage scheme must be in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement national standards. Foul and surface water shall be drained on separate systems and in the event of surface water draining to the public surface water sewer, details of the flow rate and means of control shall be submitted. The scheme shall include details of on-going management and maintenance of the surface water drainage system to be installed. The development shall be completed in accordance with the approved details and retained and maintained as such thereafter.
12. Prior to the first occupation of any of the dwellings hereby approved, details of a scheme for external lighting to serve the development shall be submitted to and approved in writing by the Local Planning Authority. The details shall include a scale plan indicating the location of the lighting to be installed, a LUX contour plan indicating the levels of light spillage and scaled elevations of lighting columns/supporting structures. The external lighting scheme shall be implemented in accordance with the approved details prior to the first occupation of any of the dwellings and shall be retained as such thereafter.
13. No development shall commence until the following details have been submitted to and approved in writing by the Local Planning Authority:
 - scaled plans showing the exact location and elevations of the acoustic fencing to be installed along the northern boundary of the site and a manufacturers specification of the fencing; and

- scaled plans showing the location of windows to be treated with high specification glazing and mechanical ventilation and manufacturers specifications of each of the noise mitigation measures to be installed.

The noise mitigation measures shall be implemented in accordance with the approved details, prior to the first occupation of any of the dwellings and shall be retained as such thereafter.

14. No development shall take place until the applicant or their agents or successors in title has secured the implementation of a programme of archaeological works. The works are to be undertaken in accordance with Written Scheme of Investigation (WSI) submitted to and approved in writing by Tameside Planning Authority.

The WSI shall cover the following:

1. An archaeological desk based assessment.
 2. A phased programme and methodology of site investigation and recording to include:
 - archaeological evaluation through trial trenching (subject to a new WSI).
 - dependent on the results from the evaluation trenches, a targeted more detailed area excavation and recording (subject to a new WSI)
 3. A programme for post investigation assessment to include:
 - production of a final report on the significance of the below-ground archaeological interest.
 4. Deposition of the final report with the Greater Manchester Historic Environment Record.
 5. Dissemination of the results of the archaeological investigations commensurate with their significance.
 6. Provision for archive deposition of the report and records of the site investigation.
 7. Nomination of a competent person or persons/organisation to undertake the works set out within the approved WSI.
15. Notwithstanding the provisions of Class A, Part 2 of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking or re-enacting that Order with or without modification), no boundary treatments or means of enclosure shall be carried out without the prior written consent of the Local Planning Authority.
16. No development shall commence unless and until a Method Statement is submitted to and approved in writing by the Local Planning Authority detailing how Japanese Knotweed and any other invasive species on the site will be removed from the site. The development shall thereafter proceed in strict accordance with the approved Method Statement.
17. No development shall commence until an Environmental Construction Method Statement detailing how pollution of the Canal adjacent to the western boundary of the site is to be avoided during the construction phase of the development has been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details.

18. The development shall be carried out in accordance with the measures detailed in the Travel Plan produced by Axis submitted with the planning application.
19. The development hereby approved shall be implemented and maintained in accordance with the crime reduction measures listed in Section 06 (Crime Impact Statement) of the Design, Access and Crime Impact Statement produced by nc architecture, submitted with the planning application.
20. The proposal shall be carried out in accordance with the mitigation measures detailed in Table 19 of the Air Quality Assessment submitted with the planning application and shall be retained as such thereafter.
21. No tree felling or vegetation removal shall take place during the optimum period for bird nesting (March to July inclusive) unless otherwise agreed in writing with the Local Planning Authority.
22. Prior to the occupation of any part of the development hereby approved, visibility splays shall be provided on both sides of the site access where it meets the footway. The visibility splays shall measure 2.4 metres along the edge of the site access and 2.4 metres along the footway. It must be clear of anything higher than 600mm above ground level. The visibility splays shall be retained as such thereafter.
23. No development above ground level shall commence until details of Biodiversity enhancement measures to be installed as part of the development hereby approved has been submitted to and approved in writing by the Local Planning Authority. The details shall include a specification of the installations and scaled plans showing their location within the development. The approved enhancement measures shall be installed in accordance with the approved details, prior to the first occupation of any of the dwellings and shall be retained as such thereafter.
24. No development above ground level shall commence until details of the sedum roofs to be installed on the roofs of the apartments blocks of the development hereby approved have been submitted to and approved in writing by the Local Planning Authority. The details shall include the following:
 - Scaled plans (including sections) showing the extent of the areas to be covered by the sedum roofs
 - The species mix to be incorporated into the sedum roof
 - Details of a scheme for on-going management and maintenance of the sedum roofs.

The sedum roofs shall be installed in accordance with the approved details prior to the first occupation of any of the apartments and shall be retained in accordance with the approved management arrangements.
25. No development above ground level shall commence until details of renewable energy generation measures to be incorporated into the development hereby approved have been submitted to and approved in writing by the Local Planning Authority. The details shall include the specifications of the measures to be installed and information demonstrating the extent of the energy requirements of the development that will be provided by renewable sources. The renewable energy generation measures shall be installed in accordance with

the approved details prior to the first occupation of any part of the development and shall be retained as such thereafter.

26. During demolition/construction no work (including vehicle and plant movements, deliveries, loading and unloading) shall take place outside the hours of 07:30 and 18:00 Mondays to Fridays and 08:00 to 13:00 Saturdays. No work shall take place on Sundays and Bank Holidays.

Reasons for conditions:

1. Required to be imposed by Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.
2. For the avoidance of doubt.
3. To ensure that the site is suitable for its intended end use and to remove any unacceptable risk to people/buildings/environment from contaminated land as per paragraph 121 of the National Planning Policy Framework.
4. To ensure that the appearance of the development reflects the character of the surrounding area.
5. To ensure that the development is served by adequate parking provision.
6. To ensure that the development would preserve the residential amenity of the occupants of neighbouring properties.
7. To ensure that the impact of the construction phase of the development would be contained within the site and would not have a detrimental impact on highway safety or the residential amenity of neighbouring properties.
8. To safeguard the general amenity of the area in accordance with UDP policy 1.12/1.13/H10.
9. To ensure appropriate landscaping of the development.
10. To ensure that the soft landscaping scheme would be adequately maintained.
11. To secure a satisfactory system of drainage and to prevent pollution of the water environment in accordance with the National Planning Policy Framework.
12. To ensure that adequate lighting is provided to public areas and that any lighting scheme would not have an adverse impact on the biodiversity value of the Canal.
13. To protect the amenities of future occupants from external noise in accordance with UDP policy H10.
14. To record and advance understanding of heritage assets impacted on by the development and to make information about the heritage interest publicly accessible, in accordance with NPPF Section 12, Paragraph 141.

15. To ensure that the required visibility splays from the vehicular access to the site are maintained free from obstruction, in the interests of highway safety.
16. To ensure that invasive species are removed from the site appropriately.
17. To ensure that the proposed development does not result in harm to the biodiversity value of the adjacent Canal.
18. To ensure that the proportion of trips generated by the development made via sustainable modes of transport is maximised.
19. To ensure that the development is designed to minimise opportunities for crime.
20. To protect the amenities of occupants of the development from any adverse impacts in relation to air quality.
21. In order to prevent any habitat disturbance to nesting birds in accordance with the National Planning Policy Framework.
22. To ensure that the development maintains highway safety.
23. To ensure biodiversity enhancements are secured to mitigate the environmental impacts of the scheme.
24. To ensure that the sedum roofs result in a biodiversity enhancement and incorporate appropriate species.
25. To ensure that the reliance on non-renewable sources of energy is reduced in order for the development to meet the environmental element of the definition of sustainable development as set out in the NPPF.
26. To ensure that the residential amenity of the future occupiers of the development is preserved, in accordance with policy H10 of the Tameside UDP and the NPPF.

Informatives

Section 106 Agreement to secure contributions towards off site green space and education provision and highways improvements.

Protected species obligations

Coal Authority low risk area

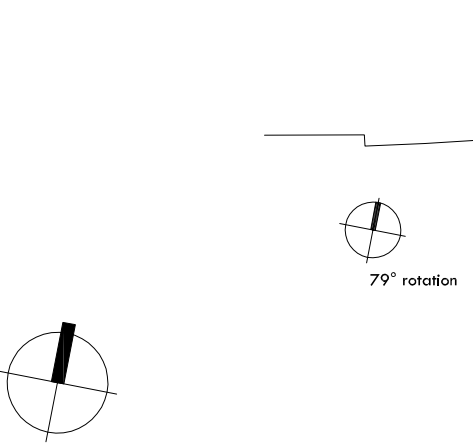
126 to 144

DRAWING
Proposed Site Plan
SCALE @ A1 1:250
SCALE @ A3 1:500
PROJECT NO. 8415
DWG NO. P-100
REV. NO. 01
1ST ISSUE
10/17
DRAWN BY TK
CHECKED BY NSC

PLANNING DRAWINGS

REV	DESCRIPTION	INTL	CHK	DATE
01	updated site edged red to match lg plan	TP	TK	11/17

Total units - 116no
Houses - 50no
 - All houses have integral garages @ 141sqft
 - NB: Integral garage area not included in the house areas below
Apartment Block - 28no (4 storeys with part undercroft parking)
 Type 1: 22no 2-beds 650sqft
 Type 2: 6no 2-beds 665sqft
Fairfield Block - 38no (4 storeys with part undercroft parking)
 Type 1: 34no 2-beds 650sqft
 Type 3: 4no 2-beds 725sqft



NOTES
 All works are to be undertaken in accordance with the Building Regulations, and the latest British Standards. All proprietary materials and products are to be used in strict accordance with the manufacturer's recommendations. Figures shown on this drawing are for information only and do not constitute a contract. The drawing remains the copyright of the Architect and may not be copied in whole or in part without the written consent of the Architect.
 The Architect: The Board, 1st Floor Suite 50, Aon Centre, Adelaide Road, Manchester, M2 2WQ
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 E: info@mcarchitect.com
 www.mcarchitect.com
RIBA Chartered Practice



This area to be designed in liaison with the Landscape Architects to create an inviting link to Canal Amenity

This area to be designed in liaison with the Landscape Architects to create an inviting link to Canal Amenity

Landscape area to provide screening to houses from overlooking by apartments

100% parking for Fairfield Apartments - 38no allocated parking spaces

100% parking for Canaldale Apartments - 28no allocated parking spaces

Garage
70

Ashton Canal

Street A

Street B

Street C

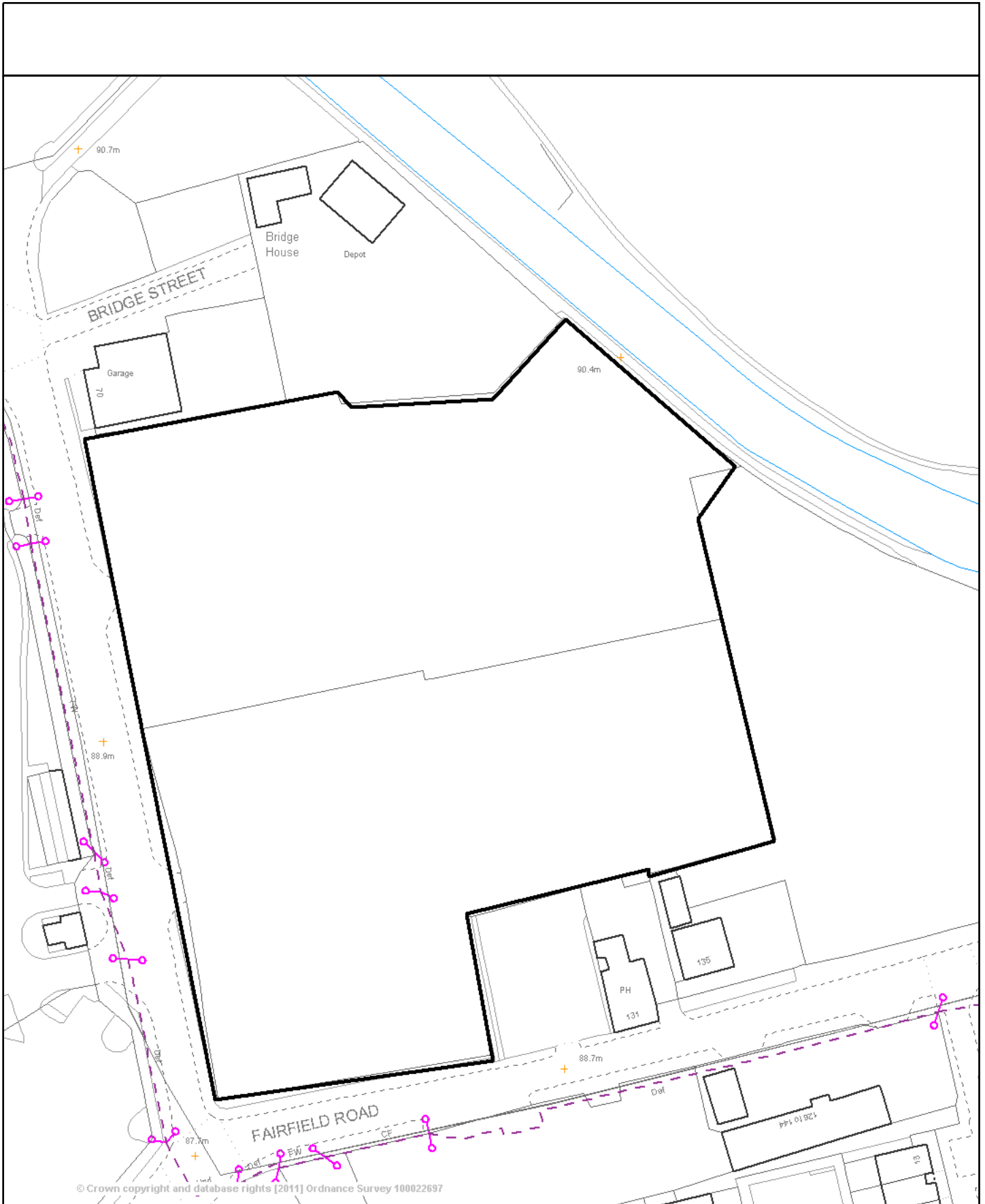
Edge Lane

Fairfield road

PH 131

135

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Scale 1/1000 Date 15/5/2018

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Application Number: 17/01034/FUL Edge Lane Droylsden

Photo 1 – looking east along Fairfield Road on southern boundary of the site towards the junction with Edge Lane.



Photo 2 – looking west on the opposite side of Fairfield Road – the site boundary runs to the rear of the buildings fronting onto Fairfield Road.



Photo 3 – view looking into the site towards the south eastern corner from the existing access on Edge Lane.



Photo 4 – looking northwards along Edge Lane opposite the existing entrance to the site.



Photo 5 – looking southwards along Edge Lane opposite the existing entrance to the site.



Photo 6 – view of terrace of residential properties on Edge Lane to the north west of the site.



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Application Number 18/00035/REM

Proposal Approval of reserved matters (landscaping) for a development of 10 dwellings. The matters of appearance, layout, scale and access were all approved under outline planning permission ref. 17/00368/OUT.

Site Land at junction of St Marys Road and Talbot Road, Hyde

Applicant Ashton Alban (Central) Ltd

Recommendation Grant planning permission subject to conditions

Reason for report A Speakers Panel decision is required because the application constitutes major development.

1. APPLICATION DESCRIPTION

1.1 The applicant seeks approval of reserved matters (landscaping) for a development of 10 dwellings. The matters of appearance, layout, scale and access were all approved under outline planning permission ref. 17/00368/OUT.

1.2 Condition 4 of the outline planning permission stated 'The landscaping scheme to be submitted as part of the reserved matters application shall include details of the number, species and location of trees to be planted, their size on planting and details of the means of protection. The scheme shall include planting of mature specimens in the north eastern corner of the site and a management plan for that area.'

1.3 The applicant has provided the following details in support of the application:

- Arboricultural Impact Assessment
- Plan indicating the location and species of trees to be planted and proposed boundary treatments to be installed
- A management plan relating to the open space and tree planting in the north eastern corner of the site.

2. SITE AND SURROUNDINGS

2.1 The application site is an area of open space, bound by St. Mary's Road to the north and west and Talbot Road to the south. A terrace of properties (90-100 St. Mary's Road) face the site to the north east, with the side elevations of the properties on Talbot Road adjacent to the south eastern corner of the site. The boundary with Talbot Road is demarcated by a stone wall, fencing and hedgerow planting run along the eastern boundary. The northern and western boundaries to the site are open. Ground levels on the site are relatively flat.

3. PLANNING HISTORY

3.1 17/00368/OUT - Outline application for the construction of 10 dwellings - approved

4. RELEVANT PLANNING POLICY

4.1 National Planning Policy Framework (NPPF)

Section 1 Delivering sustainable development
Section 6 Delivering a wide choice of high quality homes
Section 7 Requiring good design
Section 8 Promoting healthy communities

4.2 Planning Practice Guidance (PPG)

This is intended to complement the NPPF and to provide a single resource for planning guidance, whilst rationalising and streamlining the material. Almost all previous planning Circulars and advice notes have been cancelled. Specific reference will be made to the PPG or other national advice in the Analysis section of the report, where appropriate.

4.3 Tameside Unitary Development Plan (UDP)

Unallocated
Within Settlement Boundary

4.4 Part 1 Policies

1.3: Creating a Cleaner and Greener Environment.
1.4: Providing More Choice and Quality Homes.
1.5: Following the Principles of Sustainable Development
1.12: Ensuring an Accessible, Safe and Healthy Environment

4.5 Part 2 Policies

OL10: Landscape Quality and Character
C1: Townscape and Urban Form
N4: Trees and Woodland.
N5: Trees Within Development Sites.

4.6 Other Policies

Greater Manchester Spatial Framework – Publication Draft October 2016
Residential Design Supplementary Planning Document
Trees and Landscaping on Development Sites SPD adopted in March 2007.

5. PUBLICITY CARRIED OUT

5.1 Neighbour notification letters and a press notice were issued in accordance with the requirements of the Town and Country Planning (Development Management Procedure) (England) Order 2015 and the Council's adopted Statement of Community Involvement. A notice was also displayed on site.

6. RESPONSES FROM CONSULTEES

6.1 Borough Tree Officer – no objections to the proposals.

7. SUMMARY OF THIRD PARTY RESPONSES RECEIVED

7.1 No third party representations have been received.

8. ANALYSIS

8.1 The sole issue to be assessed in the determination of this reserved matters application is the appropriateness of the proposed landscaping scheme (in relation to boundary treatments as well as hard and soft landscaping.) All other material planning considerations, including the principle of development, impact on the amenity of neighbouring residents, highway safety etc, were considered at the outline planning application stage. Appropriate conditions dealing with those matters were also attached to the outline planning permission. As such, none of those matters are to be revisited at the reserved matter stage.

9. APPROPRIATENESS OF THE PROPOSED LANDSCAPING SCHEME

9.1 The proposed landscaping plan indicates that 3 Small Leaved Lime trees would be planted in the open space in the north eastern corner of the site and 1 Scots Pine tree to be planted adjacent to the turning head at the end of the access road into the development. The applicant has confirmed that the Small Leaved Lime trees would be between 3 and 4 metres (meeting the definition of 'extra heavy standards' of the tree nursery stock as per BS8545:2014) which the Tree Officer considers satisfactory, meeting the requirements of the condition on the outline permission that these trees are 'mature' on planting.

9.2 The Tree Officer considers that the species proposed are appropriate and that the Arboricultural Assessment submitted with the planning application provides adequate detail in terms of how the existing Ash tree on the southern boundary of the site would be protected during the construction phase of the development.

9.3 The applicant has provided details of the measures to put in place to ensure the effective management of the open space in the north eastern corner of the site. The measures include regular inspections and the plan details the maintenance works to be carried out. The plan states that should any of the trees be damaged, become diseased or die, they will be replaced by trees of the same species in the next planting season. Compliance with the management plan and requirements regarding the on-going maintenance of the soft landscaping can be secured by condition.

9.4 The proposed plans indicate the location of boundary fences and confirm that the existing low rise wall on the southern boundary of the site would be retained. The plans have been amended to soften the southern boundary treatment, with ivy green screen fencing replacing the originally proposed close boarded timber fencing. This would reduce the prominence of this boundary treatment on the Talbot Road streetscene.

9.5 A 0.6 metre high brick wall would extend along the majority of the length of the side boundary of the plots in the south western corner of the site, which is a prominent corner at the entry to St. Mary's Road. Limiting the extent of close boarded fencing to the side of the rear garden areas of the plots facing Talbot Road is considered to be a significant improvement on the original submission and consistent with the recommendations in the Crime Impact Statement submitted in support of the application.

9.6 Following these amendments, it is considered that the prominence of hard boundary treatments would be reduced to an acceptable degree and would preserve the character of the surrounding area. Tarmac surfacing would be limited to the highway, with block paving surfacing the driveways and around the perimeter of the dwellings. Condition 6 of the outline planning permission requires the submission and approval of the construction materials of the boundary treatments and hard surfacing to be installed.

10. CONCLUSION

- 10.1 Following the above assessment, the proposed hard and soft landscaping and boundary treatments to be installed as part of the development are considered to be acceptable.

RECOMMENDATION

Grant planning permission, subject to the following conditions:

1. The development hereby approved shall be carried out in accordance with the following approved plans/details:

Site Location Plan

Proposed site plan (drawing no. RG206/RM01 Rev. C.)

2. The hard and soft landscaping (including boundary treatments) to be implemented as part of development hereby approved shall be installed in accordance with the details indicated on the approved proposed site plan (drawing no. RG206/RM01 Rev. C.) The Small Leaved Lime trees to be planted in the north eastern corner of the site shall be of a height meeting the dimensions of 'extra heavy standards' as per BS8545:2014 on first planting. The hard landscaping and boundary treatments shall be installed in accordance with the details submitted to discharge condition 6 (materials) of outline planning permission 17/00368/OUT and shall be retained as such thereafter.
3. The approved soft and hard landscaping scheme shall be implemented before the first occupation of any part of the development or in accordance with a programme agreed previously with the local planning authority. Any newly planted trees or plants forming part of the approved scheme which, within a period of 5 years from the completion of the planting, are removed, damaged, destroyed or die shall be replaced in the next appropriate planting season with others of similar size and species.
4. The soft landscaping scheme hereby approved shall be maintained in accordance with the measures detailed in the approved management plan submitted with the planning application (detailed on drawing no. RG206/RM01 Rev. C) and shall be retained as such thereafter.

Reasons for conditions:

1. For the avoidance of doubt.
2. To ensure appropriate hard and soft landscaping of the development
3. To ensure that the approved landscaping scheme is appropriately maintained.
4. To ensure that the approved landscaping scheme is appropriately maintained.

Informatives;

This reserved matters permission is linked to outline planning permission 17/00368/OUT

- NOTES:**
1. do not scale from this drawing - use figured dimensions only
 2. read in conjunction with all other consultants/specialists drawings and report any discrepancies before work commences
 3. all setting out dimensions to be checked on site by contractor before work commences
 4. these drawings are for General Arrangement only, and under no circumstances will the arrangement be liable for errors that may occur during and after construction.
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- LEGEND**
- Grass
 - Paving
 - Paving
 - Tarmac road and pavement
 - 2m high timber fencing
 - 1m high timber fencing
 - Existing stone wall



LANDSCAPING PLAN - 1:200

SITE BOUNDARY TREATMENT PLAN - 1:200

Rev. _____

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client: Ashton Albon (Central) Ltd
 project: Land at St Mary's Road, Newton, Hyde, SK14 4LW
 drawing: Proposed Landscaping and Boundary Treatment Plans
 scale: 1:200 @ A1 date: 13.01.18 drawn: RL
 drawing no: RG206 / RM01 revision: -

do not scale from this drawing

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Scale 1/789 Date 15/5/2018

Centre = 395377 E 396273 N

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Application Number: 18/00035/REM St. Mary's Rd/Talbot Rd Hyde

Photo 1



Photo 2



Photo 3



Photo 4



Photo 5



Photo 6



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Application Number	17/01058/FUL
Proposal	Works to the Grade II listed Engine House including; removal of the east elevation and roof, reduction in wall height of the north, south and west elevations to maximum 3.4m; and internal area to be made good and landscaped to form a garden area. (See also associated Listed Building consent 17/01059/LBC)
Site Location	Former Site of 10 - 12 Castle Street, Stalybridge Tameside
Applicant	Mossclare Housing and Rowlinson Construction
Recommendation	Grant Listed Building Consent Grant subject to conditions
Reason for report	A Speakers Panel decision is required because the application is part of the wider major redevelopment scheme for the Castle Street Site

1. APPLICATION DESCRIPTION

- 1.1 This is a full application for planning permission for the works to the Grade II listed Engine House for works to remove the east elevation and roof, reduction in wall height of the north, south and west elevations to maximum 3.4m; and internal area to be made good and landscaped to form a communal garden area to form part of the garden areas for the surrounding residential schemes.
- 1.2 The site is partially within the boundary of the former Castle Street Mills, which were listed in entirety as a single grade 2 listed building entry on the National Heritage List for England in December 1990. Most of the mill complex was demolished in the early 2000s except for a substantial 4-storey stone mill building, outside of the current proposals site, which was repaired and converted to residential use in 2009. The engine house which is the subject of this application is now in a very dilapidated condition and is located on the west boundary of the site.
- 1.3 The garden amenity space located within the secure development will occupy the footprint of the Engine House and will be accessible from all three apartment blocks.
- 1.4 To retain the historic character of the building the stone capping at roof eaves level will be removed to facilitate the lowering of the walls, then reused to cap the wall garden enclosure. The remaining walls will be lowered to below the arched openings at the west elevation.
- 1.5 The application has been supported by the following assessments and documentation -
Heritage Impact Assessment
Remediation & Enabling Works Strategy (Ref: 11-493-r3)
Archaeological Assessment (Ref: 2016/59 V1)

2. SITE & SURROUNDINGS

- 2.1 The application site itself is located within Stalybridge Town Centre and lies within the Stalybridge Conservation area. It comprises of 0.5 ha of previously developed land fronting Castle Street to the south and the River Tame to the north and contains mostly cleared land and rubble. There are existing commercial properties across the river to the north and east, a large restaurant to the south and residential development to the west. The character of the surrounding area is largely determined by its position on the fringe of Stalybridge Town

Centre but comprising mainly residential uses with a mix of retail, restaurants and car parking nearby.

- 2.2 The site was previously used as part of the Castle Street Mills, which have now largely been cleared with the exception of the Grade II Listed engine house the subject of this application and is the only current built feature on the site.
- 2.3 In terms of topography, the land slopes gently down to a retaining wall on the northern boundary with the River Tame running below. The site boundaries consist of 2m high palisade fencing along the south, east and northern edge of the site with wire mesh fencing, brick and wooden board fencing along the boundary with Pattern House to the west.
- 2.4 The sites is located within 200m of Stalybridge Bus Station offering numerous services to the whole of Tameside and Manchester and 300m of Stalybridge Train Station providing regular services to Leeds, Liverpool and the rest of Greater Manchester to the west. The site is within 300m of the nearest primary school, 200m of the nearest doctors' surgery and, given its town centre location, in close proximity to a wide range of shops, services and facilities.

3. PLANNING HISTORY

- 3.1 The site has a long planning history but the most relevant to this application is the most recent permission 17/00019/FUL granted in March 2017 for the Hybrid planning application seeking; Full permission for ground works and remediation of entire site; Full permission for works to masonry wall and terracing of land adjacent to river bank; Full permission for the erection of a three storey block comprising 38 dwellings with associated car parking and landscaping on parcel A of the site and Outline permission with all matters reserved except from access for residential development up to 24 dwellings on Parcel B.

Other permissions on the site and surrounding include:

06/00117/OUT A mixed use development across a larger site comprising of up to 275 dwellings, 3,000 square metres commercial floorspace (Use Classes A1, A2, A3, B1, D1 and D2) and a hotel up to 2,500 square metres and up to a maximum of 175 parking spaces October 2006

03/01099/R3D Realignment of Castle Street (involving exhumation of former Caroline Street Methodist Church Burial Ground), closure of redundant section of Castle Street and provision of car parking area for proposed Longlands Mill development September 2003

04/00245/LBC Seek Listed Building Consent for demolition of blocks E, H2 and H3 and the east part of H Approved May 2004

04/01864/LBC Seek Listed Building Consent for partial demolition of buildings April 2005

05/00545/R3D Provision of temporary car park and new boundary fencing on two side to match existing Approved June 2005

06/00105/LBC Listed Building Consent for a mixed use development comprising of internal and external alterations, part demolition and conversion of the grade II listed Longlands Mill buildings to create 44 residential dwellings and new buildings to provide 60 dwellings and 1750m² gross commercial floor space to be used as either use classes A1,A2,A3,A4,B1,D1 and D2 and 267 car parking spaces, highways alterations to Castle Street and other associated works. Approved October 2006

06/00104/FUL Mixed use development comprising of alterations and conversion of the grade II listed Longlands Mill Buildings to create 44 residential dwellings (32 x one bed and 12 x two bed units) and new buildings to provide 60 dwellings (29 x one bed 31 x two bed units) and 1750m² gross commercial floor space to be used as either use classes A1, A2, A3, A4, B1, D1 and D2 and 267 car parking spaces, highways alterations to Castle Street and other associated works Approved October 2006

08/00388/FUL 2no. additional apartments within development approved under application 06/00104/FUL and rearrangement of service space Approved July 2007

11/00712/LBC Brick up rear openings in ground floor rear wall Approved October 2011

4. RELEVANT PLANNING POLICIES

Tameside Unitary Development Plan (UDP) Allocation Stalybridge Town Centre
Stalybridge Town Centre Conservation Area

Tameside UDP

Part 1 Policies

1.3: Creating a Cleaner and Greener Environment.

1.12: Ensuring an Accessible, Safe and Healthy Environment

Part 2 Policies

E2 (8) Development Opportunity Areas Castle Street/ Longlands Mill

S1 Town Centre Improvement

C1: Townscape and Urban Form

C2 Conservation Areas

C4 Development in or Adjoining Conservation Areas

C6 Setting of Listed Buildings

C10 Development Affecting Archaeological Sites

N5: Trees Within Development Sites.

N6 Protection and Enhancement of Waterside Areas

N7: Protected Species

OL7 Potential of Water Areas

OL9 Derelict Land Reclamation

OL10 Landscape Quality and Character

OL15 Openness and Appearance of River Valleys

MW11: Contaminated Land.

National Planning Policy Framework (NPPF)

Section 2 Ensuring the vitality of town centres

Section 7 Requiring good design

Section 8 Promoting healthy communities

Section 12: Conserving and enhancing the historic environment

Planning Practice Guidance (PPG)

This is intended to complement the NPPF and to provide a single resource for planning guidance, whilst rationalising and streamlining the material. Almost all previous planning Circulars and advice notes have been cancelled. Specific reference will be made to the PPG or other national advice in the Analysis section of the report, where appropriate.

5. PUBLICITY CARRIED OUT

- 5.1 As part of the planning application process 184 notification letters were sent out to neighbouring properties on 24th January 2018 a notice was also posted at the site and displayed in a local newspaper on the 1st February 2018.

6. RESPONSES FROM CONSULTEES

Greater Manchester Ecology Unit

The habitat creation plan which also shows the biodiversity enhancements (bird and bat boxes) (TEP drawing no D6325.002B dated 28/03/2019) together with the lighting plan (Kingfisher Lighting drawing no D30180/JM/A dated 29/03/2017) submitted with the application appear to be adequate for the development. The plans should therefore be implemented in full.

Greater Manchester Archaeological Advisory Service (GMAAS)

Concerned that the favoured proposal is to reduce the engine walls to a low level and remove the features that make it distinctive as an engine house. The resultant 'garden wall' will have no relevance as an industrial heritage structure and will surely lead to loss of listed status as it will have lost all its significance. We encourage the Council to do all in its powers to secure the long term future of the 1840s engine house through a scheme of shell repair to support the development proposals and ensure the amenity of future residents for whom the building currently presents an eyesore

Recommend conditions requiring a programme of below-ground investigation and recording is undertaken to complement the previous survey of the extant historic fabric and that the industrial heritage of the Castle Street Mills is commemorated through interpretation panels, heritage displays and a published document.

Historic England

The application should be considered in line with national policy and legislation.

7. SUMMARY OF THIRD PARTY RESPONSES RECEIVED

In response to the original notifications 2 objections have been received from households both from Pattern House. The grounds given for objecting are:

Objection to significantly changing the Grade 2 listed Engine House

8. ASSESSMENT

The principal issues in determining this application are:

- Principle of Development
- Layout and Design
- Heritage assets conservation and listed buildings
- Amenity
- Ecology
- Archaeology

9. PRINCIPLE OF DEVELOPMENT

- 9.1 The Planning (Listed Buildings and Conservation Areas) Act 1990, states that the primary duty of the Local Planning Authority in relation to listed buildings is to have special regard to the desirability of preserving the building, its setting and any features of special architectural or historic interest which it possesses.
- 9.2 Policy C5 in the Local Plan requires that alterations to listed buildings should not adversely affect the essential character of the building, should be in keeping with its architectural style and features, and should harmonise with its surroundings. Furthermore, it requires that such development is of a high quality design, and that the external appearance, design and materials will be expected to match as near as possible those of the existing building. Policy C7 permits the restoration of Listed Buildings where the development will secure the long term future of the heritage asset.
- 9.3 The National Planning Policy Framework (NPPF) is relevant as a material planning consideration and should be afforded significant weight given it post-dates the Local Plan. Paragraph 128 requires the applicant to describe the significance of the heritage asset including any contribution made by their setting. Significance is defined within the NPPF as being the value of a heritage asset to this and future generations because of its heritage interest and can include archaeological, architectural, artistic or historic features.
- 9.4 The proposed development would bring about the final part of the redevelopment of a site long-identified as a 'Development Opportunity Area' under policy E2 (8) of the UDP and would also finally secure the beneficial re-use of all of a large, derelict and contaminated site within Stalybridge Town Centre and which has detracted from the Stalybridge Conservation Area for many years.
- 9.5 The proposals would also make a valuable contribution to the reuse of the site and allow for a derelict building to become part of an active site in a well-designed scheme and part of the wider redevelopment of the whole site.
- 9.6 The application has been accompanied by a heritage statement which details the assessment and contribution of the engine house and the proposal would accord with the aims and policy objectives of the adopted UDP and having particular regard to the guidance contained within the NPPF and Practice Guidance and should be supported. There are however a number of detailed matters which also need to be considered and which are now discussed in more detail below.

10. LAYOUT AND DESIGN

- 10.1 In terms of layout and design of the scheme, the partial demolition and repurposing of the engine house as a communal amenity garden will secure the long term future of the structure without causing any further damage to its context. Whilst the proposed works include the loss of the western facing wall and lowering of the remaining walls these works will enable the retention of the remaining structure, creating a heritage asset of interest.
- 10.2 The new use as an amenity garden space will ensure residents can enjoy a retained industrial and historical asset whilst maintaining the character and appearance of the area. The engine house will be surrounded by residential development on all sides and is located between the new blocks on the Phase 2 development. Both the blocks have their entrances facing the car park and the engine house block
- 10.3 In terms of scale and mass, it is also considered that the development is acceptable and would fit within the surrounding proposed residential block whilst resulting in a building which is sympathetic and compatible with the character of the locality but with interesting

design features that would complement the character and appearance of the area in accordance with UDP and SPD policies and is acceptable.

- 10.4 In overall terms the development is acceptable taking into account the site characteristics and relationships with neighbouring buildings and would comply with the design and conservation based policies in the UDP, SPD and would accord with the guidance contained within the NPPF and PPG.

11. HERITAGE ASSETS: CONSERVATION AREA AND LISTED BUILDING

- 11.1 The principles within the NPPF and UDP policies permits the restoration of Listed Buildings where the development will secure the long term future of the heritage asset. The proposal for the engine house respects this position and through a number of works will retain the historic character and features of the building.
- 11.2 The Engine House has been cut off from its original historical setting by The Pattern House, removing it from its context and leading to it becoming isolated within the site. As such, its historical use is no longer evident, and the building structure and architectural features have become dilapidated. At present the building is in very poor repair rendering it isolated and forgotten. The machinery which was originally housed within the building has been removed, and other architectural elements of interest have fallen into poor condition.
- 11.3 In terms of external appearance, it is considered that the design of the proposed scheme is acceptable and would result in a positive feature in the conservation area which is sympathetic to the special historic character of the conservation area and provide a positive reuse for the remaining historic building. This approach is consistent with paragraph 131 of the NPPF which require account to be taken of the desire of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation and have regard to a positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality. This is further detailed within the assessment of the Listed Building application for the same scheme (17/01059/LBC)

12. RESIDENTIAL AMENITY

- 12.1 The position of the engine house has been considered when the residential blocks have been located on the site. With the proposed reduction in height there will be a less than significant effect on the amenities of future occupiers of residential properties.
- 12.2 The site layout also shows that sufficient spaces exist around the blocks for residential amenity for the proposed dwellings and this is in line with the council's requirements in the SPD.
- 12.3 The proposed reduction in height will reduce the impact of the structure on the outlook and visual amenity for the existing residents of Pattern House and as such is considered to be a positive development in amenity terms.

13. ECOLOGY

- 13.1 The application is accompanied by an Ecological assessment which states there is one dilapidated building (the engine house) which has negligible bat roost potential. This has been considered by GMEU who support the opportunities for improving the natural habitat in this area shown on the proposed plans. They agree that the proposed

biodiversity enhancements (bird and bat boxes) together with the proposed lighting plan are adequate for the development and raise no objection to the scheme on ecological grounds.

- 13.2 The proposals would not therefore have any adverse effect upon protected species and are thus in accordance with policy N7: Protected Species and the guidance contained within the NPPF and PPG.

14. ARCHAEOLOGY

- 14.1 The application is accompanied by an Heritage Impact assessment and Structural Inspection report. An archaeological survey has previously been undertaken of the engine house as part of a wider study of Longlands Mill in 2003, this along with an additional archaeological survey and an archaeological desk based assessment undertaken in December 2016 for the adjacent former Foundry development site identified that there will be below-ground archaeological interest within the engine house once the current floor levels are removed. This will relate to the foundations of the 1840s steam engine.
- 14.2 The Greater Manchester Archaeological Advisory Service (GMAAS) are concerned that the reducing the engine walls to a low level and removing the features that make it distinctive as an engine house and will lose the relevance as an industrial heritage structure and state that it is a shame that the engine house has been previously isolated from the remaining mill building
- 14.3 GMAAS recommend that a programme of below-ground investigation and recording is undertaken to complement the previous survey of the extant historic fabric. And that the industrial heritage of the Castle Street Mills is commemorated through interpretation panels, heritage displays and through a published article. These can be secured through a planning condition.
- 14.4 The proposals would not therefore have any significant adverse effect upon any potential archaeology and the addition of relevant conditions will ensure appropriate surveys and interpretation is provided on the site to ensure the long term understanding and context for the building in line with guidance contained within the NPPF and PPG.

15. CONCLUSION

- 15.1 At the heart of the NPPF is a presumption in favour of sustainable development, this requires planning applications that accord with the development plan to be approved without delay and where the development plan is absent, silent or out of date granting permission unless the adverse impacts of doing so would significantly and demonstrably outweigh the benefits when assessed against the policies in the framework as a whole or specific policies in the framework indicate that development should be restricted.
- 15.2 In this instance, the application site is designated for residential use under UDP Policy E2 (8) and as the redevelopment of the site would bring about a range of benefits including re-use of a prominent and redundant town centre/ conservation area site; visual amenity improvements; additional planting / ecological improvements; contribution to delivery of new homes; and additional expenditure in the local shops and services. There are no significant factors which outweigh these benefits and so the proposal is therefore recommended for approval subject to the conditions.

16. RECOMMENDATION

Grant full planning permission for the Works to the Grade II listed Engine House including; removal of the east elevation and roof, reduction in wall height of the north, south and west elevations to maximum 3.4m; and internal area to be made good and landscaped to form a garden area. Subject to conditions

1. The development must be begun not later than the expiration of three years beginning with the date of this permission.
2. No development, other than work to undertake site clearance, remediation and ground engineering works, shall take until such time until samples and/or a full specification of materials to be used externally have been submitted to and approved in writing by the Local Planning Authority. Such details shall include the type, colour and texture of the materials. The development shall be constructed in accordance with the approved details.
3. The approved scheme of landscaping scheme shall be implemented before the development is first occupied or in accordance with a programme agreed previously with the local planning authority. Any newly planted trees or plants forming part of the approved scheme which, within a period of 5 years from the completion of the planting, are removed, damaged, destroyed or die shall be replaced in the next appropriate planting season with others of similar size and species by the developer unless the local planning authority gives written consent to any variation
4. No development shall take place, other than site clearance and site compound set up, until the implementation of a programme of archaeological works in accordance with a Written Schemes of Investigation (WSI) submitted to and approved in writing by the Local Planning Authority. The WSIs shall cover the following:
 - i phased programme and methodology of investigation and recording to include: evaluation trenching; detailed archaeological excavation (informed by trenching); an archaeological watching brief
 - ii A programme for post investigation assessment to include: analysis of the site investigation records and finds; production of a final report on the significance of the archaeological and historical interest represented.
 - iii A scheme to commemorate the site's industrial heritage
 - iv Dissemination of the results commensurate with their significance.
 - v Provision for archive deposition of the report and records of the site investigation.
 - vi Nomination of a competent person or persons/organisation to undertake the works.The development shall be carried out in accordance with the approved details.
5. The developer shall afford access at all reasonable times to any archaeologist nominated by the Local Planning Authority, and shall allow them to observe the excavations and record items of interest.
6. No development, other than site clearance and site compound set up, shall commence until such time as the following information has been submitted in writing and written permission at each stage has been granted by the Local Planning Authority.
 - i) A preliminary risk assessment to determine the potential for the site to be contaminated shall be undertaken and approved by the Local Planning Authority. Prior to any physical site investigation, a methodology shall be approved by the Local Planning Authority. This shall include an assessment to determine the nature and extent of any contamination affecting the site and the potential for off-site migration.
 - ii) Where necessary a scheme of remediation to remove any unacceptable risk to human health, buildings and the environment (including controlled waters) shall be approved by the Local Planning Authority prior to implementation.

iii) Any additional or unforeseen contamination encountered during development shall be notified to the Local Planning Authority as soon as practicably possible and a remedial scheme to deal with this approved by the Local Planning Authority.

iv) Upon completion of any approved remediation schemes, and prior to occupation, a completion report demonstrating that the scheme has been appropriately implemented and the site is suitable for its intended end use shall be approved in writing by the Local Planning Authority.

The discharge of this planning condition will be given in writing by the Local Planning Authority on completion of the development and once all information specified within this condition and other requested information have been provided to the satisfaction of the Local Planning Authority and occupation/use of the development shall not commence until this time, unless otherwise agreed by the Local Planning Authority.

7. During demolition, ground engineering and construction periods, no works (including vehicle and plant movements, deliveries, loading and unloading) shall take place outside the hours of 07:30 and 18:00 Mondays to Fridays and 08:00 to 13:00 Saturdays. No work shall take place on Sundays and Bank Holidays.
8. The development hereby permitted shall be carried out in accordance with the following approved plans: Heritage Impact Assessment and Design Addendum, Structural Inspection Report, SSL:17617B:50:1:1:ELV, 4654-01_1001, 4654-01_1003, C19709/03 and 2016/59 V1 Archaeological Assessment received by the council on 4 January 2018

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Application Number: 17/01058/FUL
Former Site Of 10 - 12 Castle Street Stalybridge

Photo 1

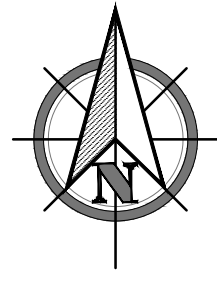


Photo 2



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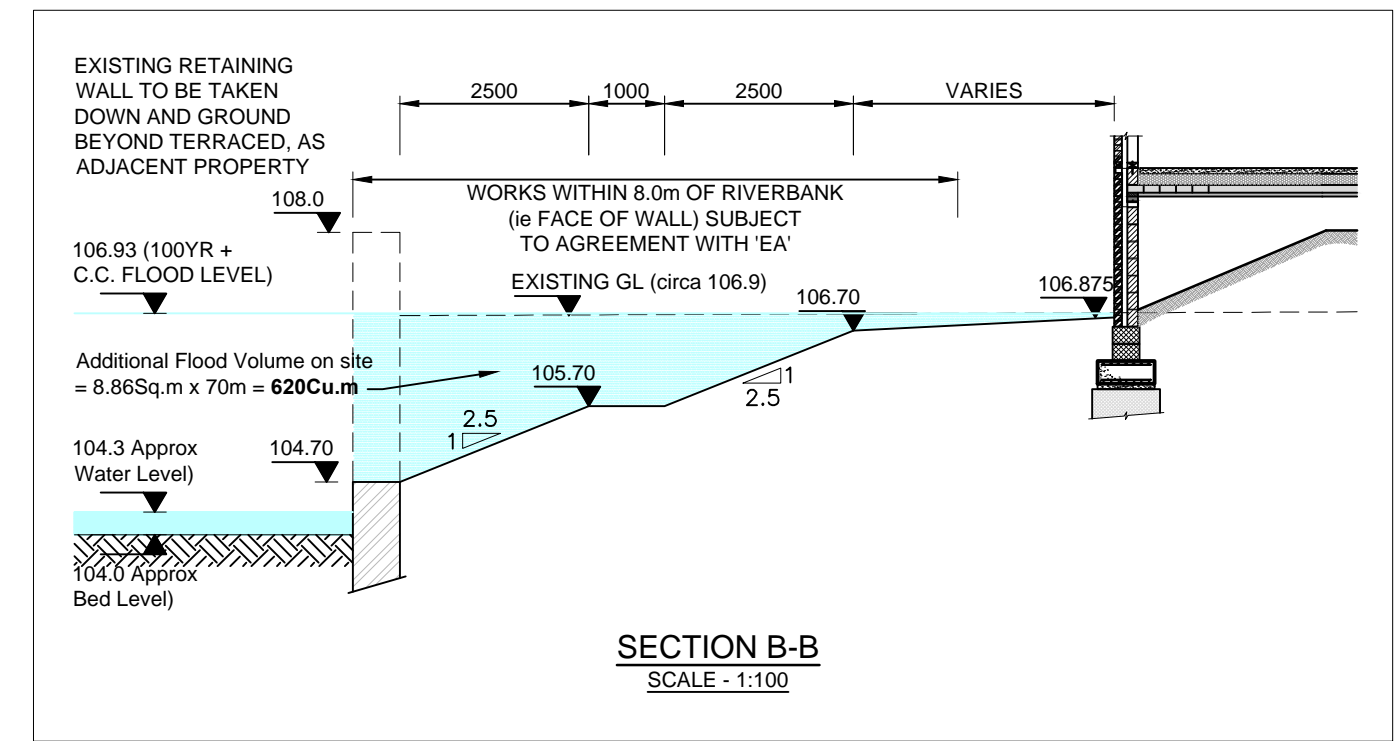
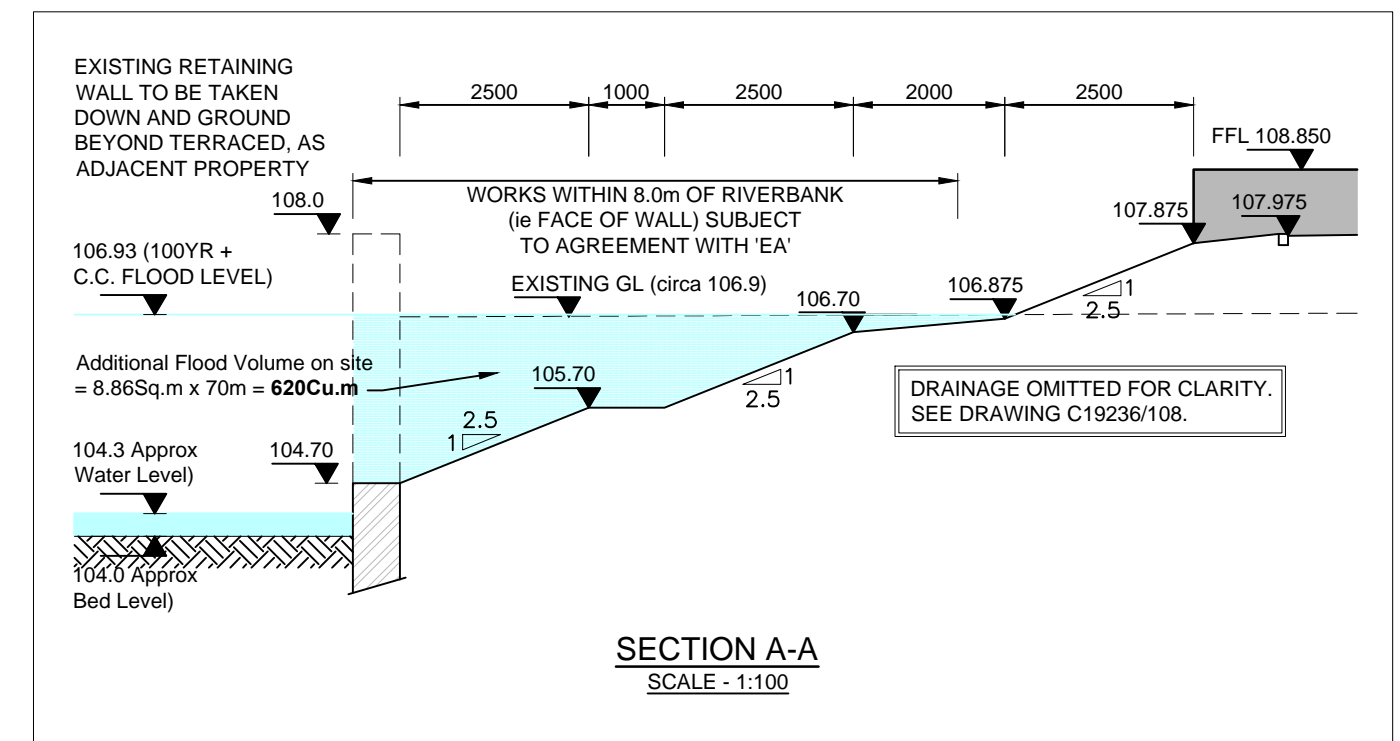


KERB TYPES

- Indicates 125x255 Half Batter Kerb (HB2) - 100mm
- Indicates 125x150 Bullnose Kerb (BN) - 25mm
- Indicates 125x150 Bullnose Kerb (BN) - Flush
- Indicates Drop Kerb (1 @ 1:12)
- Indicates 50x200 Flat Top Edging (EF)
- Indicates tangent point & Kerb Radius

GENERAL

- PROPOSED LEVELS
- PROPOSED HARDSTANDING GRADIENTS
- SLOPE CHANGE (ie RIDGE, VALLEY or FOLD)



REV	DATE	DRN	DESCRIPTION

PURPOSE OF ISSUE: INFORMATION

CLIENT: ROWLINSON CONSTRUCTIONS LTD

PROJECT: CASTLE STREET STALYBRIDGE (PHASE 2)

TITLE: EXTERNAL WORKS G.A. - FALLS & LEVELS

JOB No.	DRAWING No.	REVISION		
C19709	/ 03	~		
DRAWN	CHECKED	SCALE	@ SHEET SIZE	DATE
TW	KM	1:200	A1	DEC 2017

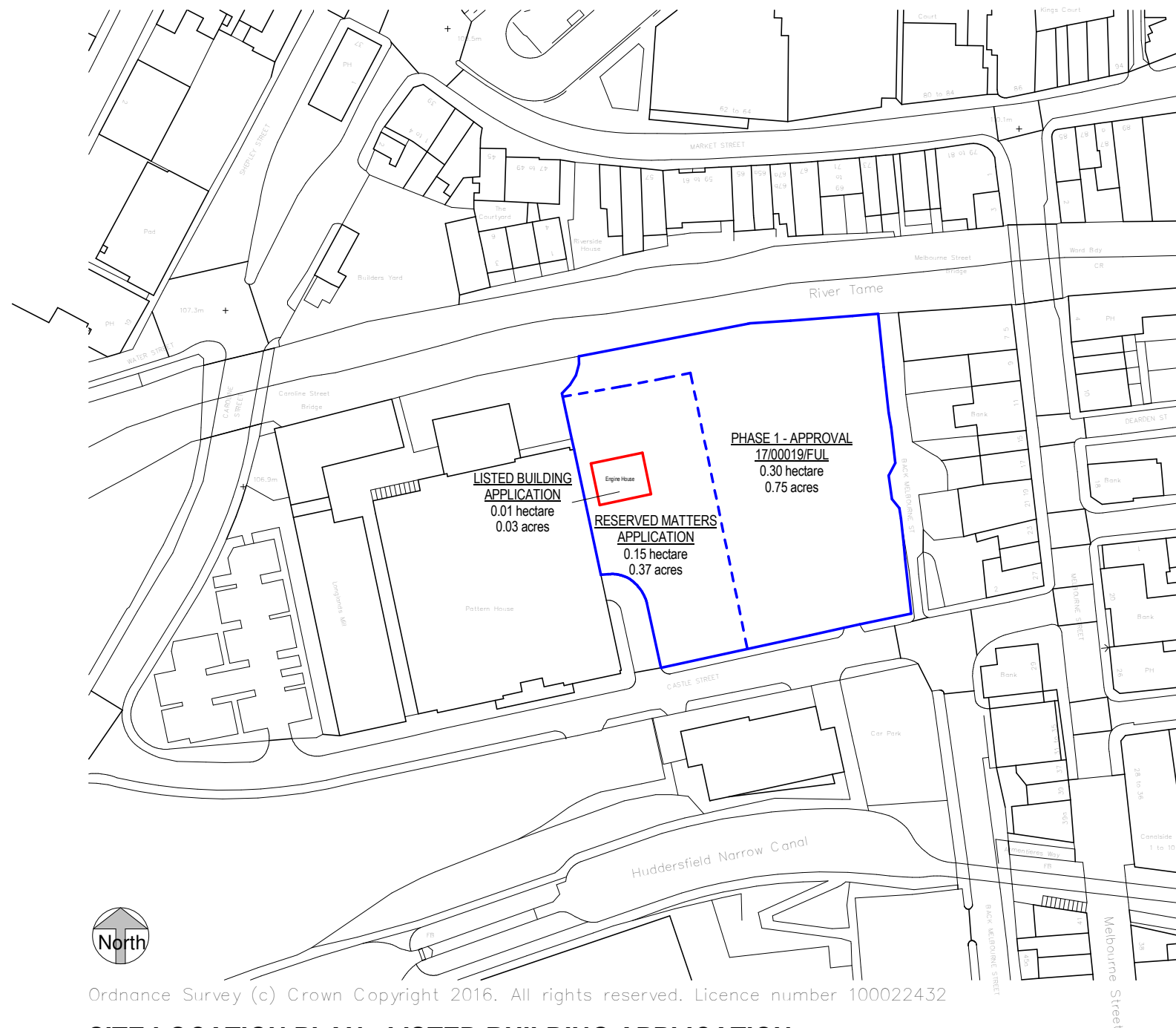
DRAWING NOT TO BE SCALED

atkinson consulting engineers
peck

Watson House, 45 Waterloo Road, Stockport, Cheshire, SK1 3BJ
Telephone +44 (0)161 480 2833, facsimile +44 (0)161 477 3939
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SITE LOCATION PLAN - LISTED BUILDING APPLICATION
1 : 1250

Notes
The Contractor is to check all dimensions and conditions on site before commencing. Do not scale from this drawing.
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Scale Bars TO BE USED ONLY AS GUIDANCE

1:100	mm	1000	3000	5000
1:200	mm	5000	8000	
1:500	M	5	15	25
1:1250	M	10	30	50 70

KEY

- Site boundary
- Ownership boundary (Phase 1 - 17/00019/FUL & Phase 2 - Reserved matters)

SITE AREAS

Name	Area
RESERVED MATTERS APPLICATION	0.15 hectare
LISTED BUILDING APPLICATION	0.01 hectare
PHASE 1 - APPROVAL 17/00019/FUL	0.30 hectare

Rev	Description	By	Date
Client	MOSSCARE ST VINCENTS' HOUSING GROUP		
Job	CASTLE STREET PHASE 2, STALYBRIDGE		
Drawing	SITE LOCATION PLAN - LISTED BUILDING		
BSRef	POZ Ref: 4654-01_1001	Rev	
Date	07/12/17	Scale	1 : 1250 @ A3
3D CoOrd	EMc	2D CoOrd	EMc Approved TW
Project Status	Planning	Tender	Construction
	Design	Contract	As Built



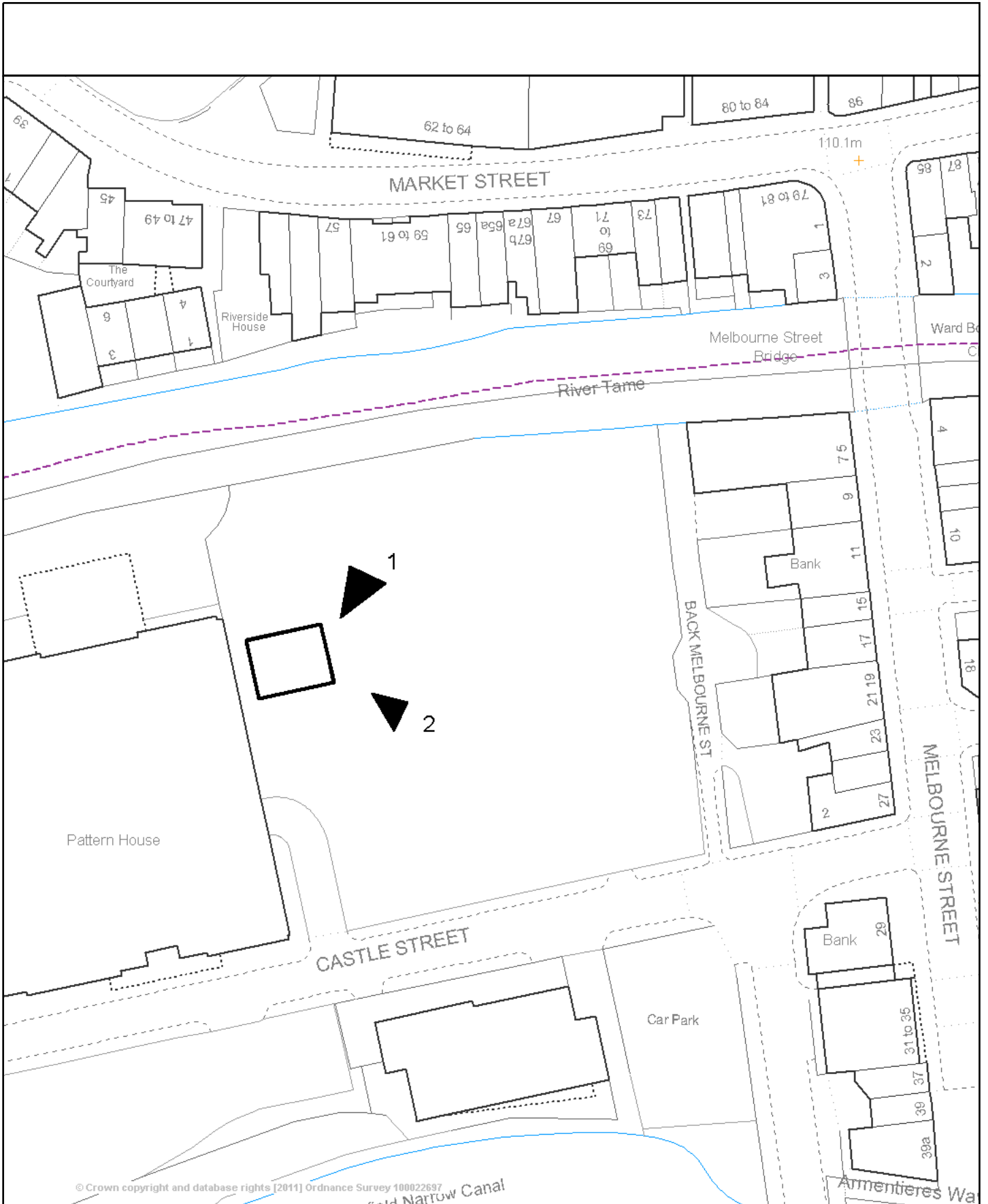
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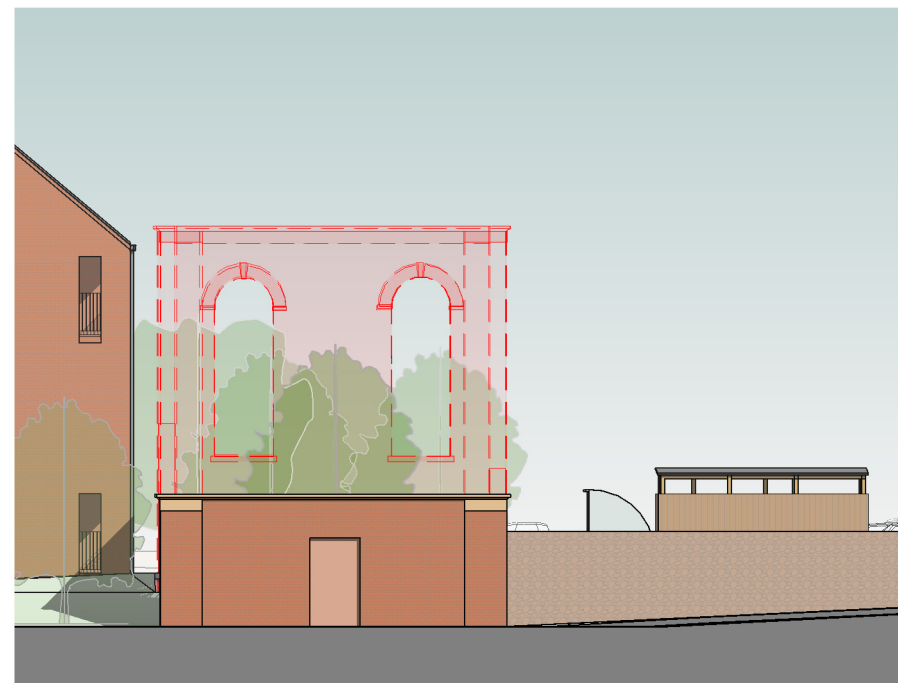
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Scale 1/800 Date 15/5/2018
 Centre = 396217 E 398492 N

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Notes
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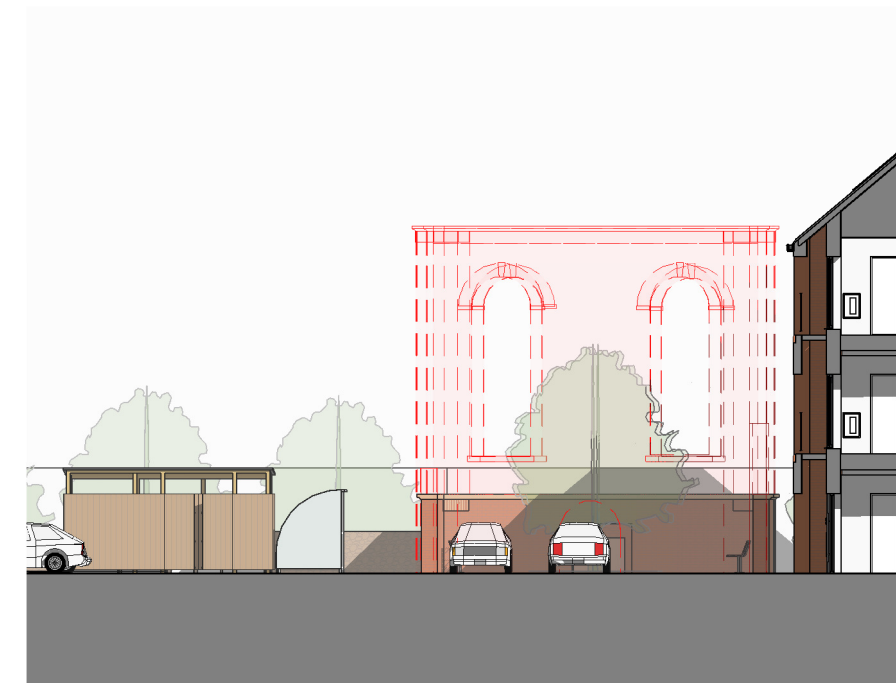
Scale Bars TO BE USED ONLY AS GUIDANCE			
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1:200	mm	0 1000 2000 3000	0 5 10
1:500	m	0 10 20 30	0 5 10
1:1250	m	0 10 20 30	0 5 10



WEST ELEVATION - FACING PATTERN HOUSE
1 : 200



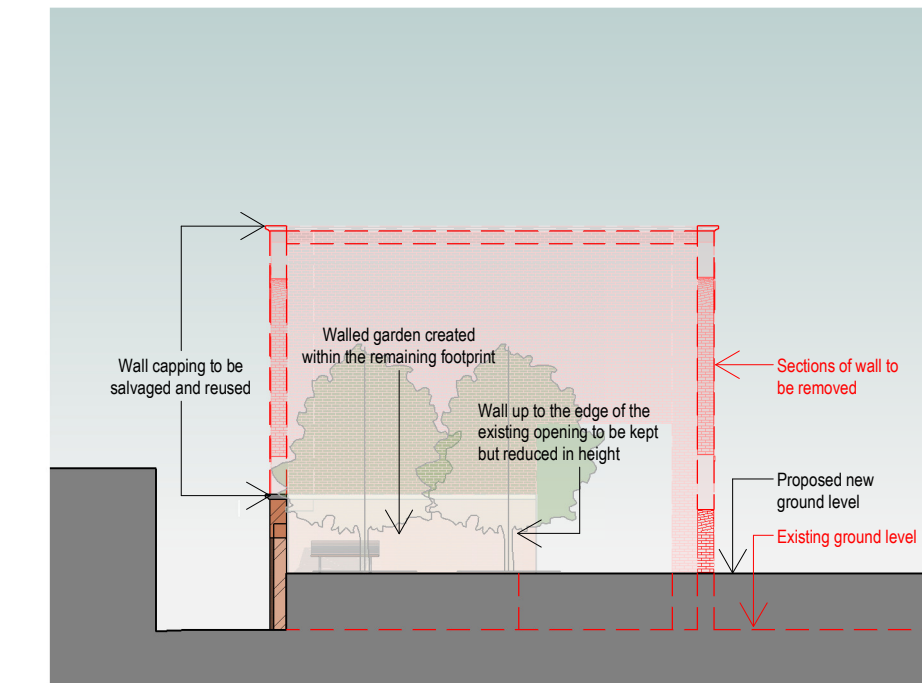
NORTH ELEVATION - FACING BLOCK C
1 : 200



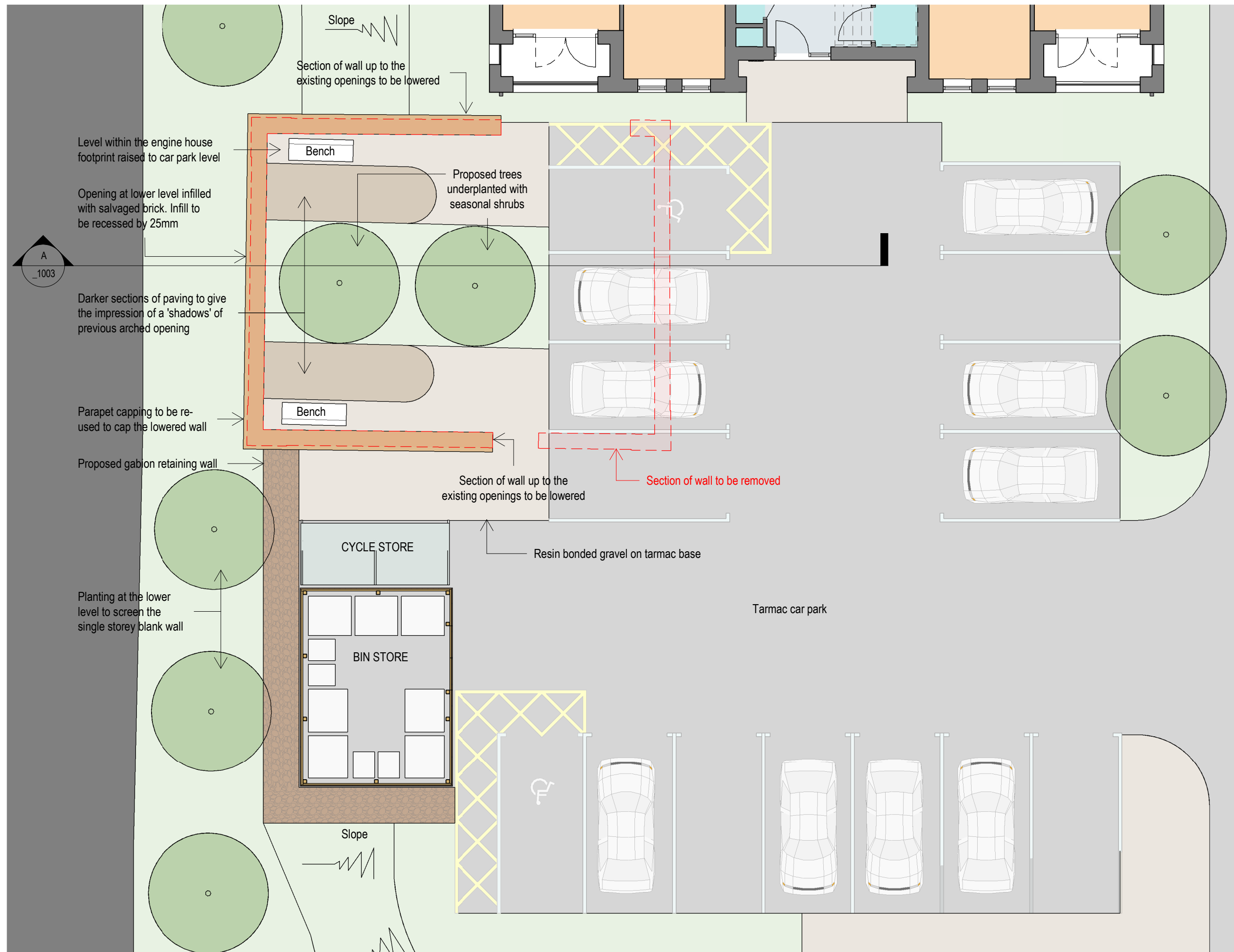
EAST ELEVATION - FACING BLOCK A (PHASE 1)
1 : 200



SOUTH ELEVATION - FACING BLOCK B
1 : 200



SECTION A THROUGH THE ENGINE HOUSE
1 : 200



PROPOSED PLAN
1 : 100



AERIAL VIEW FROM THE PATTERN HOUSE



VIEW FROM PATTERN HOUSE CAR PARK ENTRANCE



VIEW FROM THE ENTRANCE OF BLOCK C



VIEW FROM THE CORNER OF BLOCK B



VIEW FROM THE RIVER TAME

MATERIALS (LISTED BUILDING)
EXISTING STRUCTURE:
Walls - Brick, red
Roof - Vaulted brick arch with cast iron supports
Windows & Doors - Not intact
PROPOSED:
1. Walls - Height of existing walls to be reduced. Existing stone capping to be salvaged and utilised for the top of the wall. Existing brick to be salvaged for infilling the opening on the west elevation.
2. Roof - n/a roof to be removed
3. Windows & Doors - n/a, no windows and doors proposed
4. Pathways - Resin bonded gravel on a tarmac base, golden in colour for the main pathways and grey for the shadow effect within the engine house garden

PLEASE NOTE: It is intended that this drawing be read in conjunction with the Heritage Impact Assessment and the Heritage Statement Design Addendum. For existing elevations please refer to survey drawing 17617B-ELV - A1 - 50 by Survey Systems

Rev	Description	By	Date
Client	MOSSCARE ST VINCENTS' HOUSING GROUP		
Job	CASTLE STREET PHASE 2, STALYBRIDGE		
Drawings	PROPOSED ALTERATIONS TO ENGINE HOUSE		
Ref	4654-01_1003		
Scale	1:1003		
Date	07/12/17	As indicated	@ A1
Author	EMc	EMc	TW
Check	Planning	Tender	Construction
Issue	Design	Contract	As Built



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Application Number 17/01059/LBC

Proposal	Works to the Grade II listed Engine House including; removal of the east elevation and roof, reduction in wall height of the north, south and west elevations to maximum 3.4m; and internal area to be made good and landscaped to form a garden area. (See also associated Full planning application 17/01058/FUL)
Site Location	Former Site of 10 - 12 Castle Street, Stalybridge Tameside
Applicant	Mossclare Housing and Rowlinson Construction
Recommendation	Grant Listed Building Consent Grant subject to conditions
Reason for report	A Speakers Panel decision is required because the application is part of the wider major redevelopment scheme for the Castle Street Site.

1. APPLICATION DESCRIPTION

- 1.1 This is a listed building consent for works to the Grade II listed Engine House for works to remove the east elevation and roof, reduction in wall height of the north, south and west elevations to maximum 3.4m; and internal area to be made good and landscaped to form a communal garden area to form part of the garden areas for the surrounding residential schemes.
- 1.2 The site is partially within the boundary of the former Castle Street Mills, which were listed in entirety as a single grade 2 listed building entry on the National Heritage List for England in December 1990. Most of the mill complex was demolished in the early 2000s except for a substantial 4-storey stone mill building, outside of the current proposals site, which was repaired and converted to residential use in 2009. The engine house which is the subject of this application is now in a very dilapidated condition and is located on the west boundary of the site.
- 1.3 The garden amenity space located within the secure development will occupy the footprint of the Engine House and will be accessible from all three apartment blocks.
- 1.4 To retain the historic character of the building the stone capping at roof eaves level will be removed to facilitate the lowering of the walls, then reused to cap the wall garden enclosure. The remaining walls will be lowered to below the arched openings at the west elevation. but it is proposed that 'shadows' of these openings are created in the hard landscaping to reference the existing structure.
- 1.5 The application has been supported by the following assessments and documentation -
 - Heritage Impact Assessment
 - Remediation & Enabling Works Strategy (Ref: 11-493-r3)
 - Archaeological Assessment (Ref: 2016/59 V1)

2. SITE & SURROUNDINGS

- 2.1 The application site itself is located within Stalybridge Town Centre and lies within the Stalybridge Conservation area. It comprises of 0.5 ha of previously developed land fronting Castle Street to the south and the River Tame to the north and contains mostly cleared land and rubble. There are existing commercial properties across the river to the north and east, a large restaurant to the south and residential development to the west. The character of the surrounding area is largely determined by its position on the fringe of Stalybridge Town Centre but comprising mainly residential uses with a mix of retail, restaurants and car parking nearby.
- 2.2 The site was previously used as part of the Castle Street Mills, which have now largely been cleared with the exception of the Grade II Listed engine house the subject of this application and is the only current built feature on the site. The engine house is not specifically mentioned in the listing but the features of architectural interest on the engine house are as follows: Corner pilaster details, Brick-vaulted roof and tall arched opening is the East and West elevations.
- 2.3 In terms of topography, the land slopes gently down to a retaining wall on the northern boundary with the River Tame running below. The site boundaries consist of 2m high palisade fencing along the south, east and northern edge of the site with wire mesh fencing, brick and wooden board fencing along the boundary with Pattern House to the west.
- 2.4 The sites is located within 200m of Stalybridge Bus Station offering numerous services to the whole of Tameside and Manchester and 300m of Stalybridge Train Station providing regular services to Leeds, Liverpool and the rest of Greater Manchester to the west. The site is within 300m of the nearest primary school, 200m of the nearest doctors' surgery and, given its town centre location, in close proximity to a wide range of shops, services and facilities.

3. PLANNING HISTORY

- 3.1 The site has a long planning history but the most relevant to this application is the most recent permission 17/00019/FUL granted in March 2017 for the Hybrid planning application seeking; Full permission for ground works and remediation of entire site; Full permission for works to masonry wall and terracing of land adjacent to river bank; Full permission for the erection of a three storey block comprising 38 dwellings with associated car parking and landscaping on parcel A of the site and Outline permission with all matters reserved except from access for residential development up to 24 dwellings on Parcel B.

Other permissions on the site and surrounding include:

06/00117/OUT A mixed use development across a larger site comprising of up to 275 dwellings, 3,000 square metres commercial floorspace (Use Classes A1, A2, A3, B1, D1 and D2) and a hotel up to 2,500 square metres and up to a maximum of 175 parking spaces October 2006

03/01099/R3D Realignment of Castle Street (involving exhumation of former Caroline Street Methodist Church Burial Ground), closure of redundant section of Castle Street and provision of car parking area for proposed Longlands Mill development September 2003

04/00245/LBC Seek Listed Building Consent for demolition of blocks E, H2 and H3 and the east part of H Approved May 2004

04/01864/LBC Seek Listed Building Consent for partial demolition of buildings April 2005

05/00545/R3D Provision of temporary car park and new boundary fencing on two side to match existing Approved June 2005

06/00105/LBC Listed Building Consent for a mixed use development comprising of internal and external alterations, part demolition and conversion of the grade II listed Longlands Mill buildings to create 44 residential dwellings and new buildings to provide 60 dwellings and 1750m2 gross commercial floor space to be used as either use classes A1,A2,A3,A4,B1,D1 and D2 and 267 car parking spaces, highways alterations to Castle Street and other associated works. Approved October 2006

06/00104/FUL Mixed use development comprising of alterations and conversion of the grade II listed Longlands Mill Buildings to create 44 residential dwellings (32 x one bed and 12 x two bed units) and new buildings to provide 60 dwellings (29 x one bed 31 x two bed units) and 1750m2 gross commercial floor space to be used as either use classes A1, A2, A3, A4, B1, D1 and D2 and 267 car parking spaces, highways alterations to Castle Street and other associated works Approved October 2006

08/00388/FUL 2no. additional apartments within development approved under application 06/00104/FUL and rearrangement of service space Approved July 2007

11/00712/LBC Brick up rear openings in ground floor rear wall Approved October 2011

4. RELEVANT PLANNING POLICIES

Tameside Unitary Development Plan (UDP) Allocation Stalybridge Town Centre
Stalybridge Town Centre Conservation Area

Tameside UDP

Part 1 Policies

1.3: Creating a Cleaner and Greener Environment.
1.12: Ensuring an Accessible, Safe and Healthy Environment

Part 2 Policies

E2 (8) Development Opportunity Areas Castle Street/ Longlands Mill
S1 Town Centre Improvement
C1: Townscape and Urban Form
C2 Conservation Areas
C4 Development in or Adjoining Conservation Areas
C6 Setting of Listed Buildings
C10 Development Affecting Archaeological Sites
N4: Trees and Woodland.
N5: Trees Within Development Sites.
N6 Protection and Enhancement of Waterside Areas
N7: Protected Species

OL7 Potential of Water Areas
OL9 Derelict Land Reclamation
OL10 Landscape Quality and Character
OL15 Openness and Appearance of River Valleys
MW11: Contaminated Land.

National Planning Policy Framework (NPPF)

Section 2 Ensuring the vitality of town centres
Section 7 Requiring good design
Section 8 Promoting healthy communities
Section 12: Conserving and enhancing the historic environment

Planning Practice Guidance (PPG)

This is intended to complement the NPPF and to provide a single resource for planning guidance, whilst rationalising and streamlining the material. Almost all previous planning Circulars and advice notes have been cancelled. Specific reference will be made to the PPG or other national advice in the Analysis section of the report, where appropriate.

5. PUBLICITY CARRIED OUT

- 5.1 As part of the planning application process 184 notification letters were sent out to neighbouring properties on 24th January 2018 a notice was also posted at the site and displayed in a local newspaper on the 1st February 2018.

6. RESPONSES FROM CONSULTEES

Greater Manchester Ecology Unit

The habitat creation plan which also shows the biodiversity enhancements (bird and bat boxes) (TEP drawing no D6325.002B dated 28/03/2019) together with the lighting plan (Kingfisher Lighting drawing no D30180/JM/A dated 29/03/2017) submitted with the application appear to be adequate for the development. The plans should therefore be implemented in full.

Greater Manchester Archaeological Advisory Service (GMAAS)

Concerned that the favoured proposal is to reduce the engine walls to a low level and remove the features that make it distinctive as an engine house. The resultant 'garden wall' will have no relevance as an industrial heritage structure and will surely lead to loss of listed status as it will have lost all its significance. We encourage the Council to do all in its powers to secure the long term future of the 1840s engine house through a scheme of shell repair to support the development proposals and ensure the amenity of future residents for whom the building currently presents an eyesore

Recommend conditions requiring a programme of below-ground investigation and recording is undertaken to complement the previous survey of the extant historic fabric and that the industrial heritage of the Castle Street Mills is commemorated through interpretation panels, heritage displays and a published document.

Historic England

The application should be considered in line with national policy and legislation.

7. SUMMARY OF THIRD PARTY RESPONSES RECEIVED

In response to the original notifications 2 objections have been received from households both from Pattern House. The grounds given for objecting are:

Objection to significantly changing the Grade 2 listed Engine House

8. ASSESSMENT

The principal issues in determining this application are:

- Principle of Development
- Layout and Design
- Heritage assets conservation and listed buildings
- Amenity
- Ecology
- Archaeology

9. PRINCIPLE OF DEVELOPMENT

- 9.1 The Planning (Listed Buildings and Conservation Areas) Act 1990, states that the primary duty of the Local Planning Authority in relation to listed buildings is to have special regard to the desirability of preserving the building, its setting and any features of special architectural or historic interest which it possesses.
- 9.2 Policy C5 in the Local Plan requires that alterations to listed buildings should not adversely affect the essential character of the building, should be in keeping with its architectural style and features, and should harmonise with its surroundings. Furthermore, it requires that such development is of a high quality design, and that the external appearance, design and materials will be expected to match as near as possible those of the existing building. Policy C7 permits the restoration of Listed Buildings where the development will secure the long term future of the heritage asset.
- 9.3 The National Planning Policy Framework (NPPF) is relevant as a material planning consideration and should be afforded significant weight given it post-dates the Local Plan. Paragraph 128 requires the applicant to describe the significance of the heritage asset including any contribution made by their setting. Significance is defined within the NPPF as being the value of a heritage asset to this and future generations because of its heritage interest and can include archaeological, architectural, artistic or historic features.
- 9.4 The proposed development would bring about the final part of the redevelopment of a site long-identified as a 'Development Opportunity Area' under policy E2 (8) of the UDP and would also finally secure the beneficial re-use of all of a large, derelict and contaminated site within Stalybridge Town Centre and which has detracted from the Stalybridge Conservation Area for many years.
- 9.5 The proposals would also make a valuable contribution to the reuse of the site and allow for a derelict building to become part of an active site in a well-designed scheme and part of the wider redevelopment of the whole site and give the structure a use.

- 9.6 The application has been accompanied by a heritage statement which details the assessment and contribution of the engine house and the proposal would accord with the aims and policy objectives of the adopted UDP and having particular regard to the guidance contained within the NPPF and Practice Guidance and should be supported. There are however a number of detailed matters which also need to be considered and which are now discussed in more detail below.

10. LAYOUT AND DESIGN

- 10.1 In terms of layout and design of the scheme, the partial demolition and repurposing of the engine house as a communal amenity garden will secure the long term future of the structure without causing any further damage to its context. Whilst the proposed works include the loss of the western facing wall and lowering of the remaining walls these works will enable the retention of the remaining structure, creating a heritage asset of interest.
- 10.2 The new use as an amenity garden space will ensure residents can enjoy a retained industrial and historical asset whilst maintaining the character and appearance of the area. The engine house will be surrounded by residential development on all sides and is located between the new blocks on the Phase 2 development. Both the blocks have their entrances facing the car park and the engine house block
- 10.3 In terms of scale and mass, it is also considered that the development is acceptable and would fit within the surrounding proposed residential block whilst resulting in a building which is sympathetic and compatible with the character of the locality but with interesting design features that would complement the character and appearance of the area in accordance with UDP and SPD policies and is acceptable.
- 10.4 In overall terms the development is acceptable taking into account the site characteristics and relationships with neighbouring buildings and would comply with the design and conservation based policies in the UDP, SPD and would accord with the guidance contained within the NPPF and PPG.

11. HERITAGE ASSETS: CONSERVATION AREA AND LISTED BUILDING

- 11.1 The principles within the NPPF and UDP policies permits the restoration of Listed Buildings where the development will secure the long term future of the heritage asset. The proposal for the engine house respects this position and through a number of works will retain the historic character and features of the building.
- 11.2 The Engine House has been cut off from its original historical setting by The Pattern House, removing it from its context and leading to it becoming isolated within the site. As such, its historical use is no longer evident, and the building structure and architectural features have become dilapidated. At present the building is in very poor repair rendering it isolated and forgotten. The machinery which was originally housed within the building has been removed, and other architectural elements of interest have fallen into poor condition.
- 11.3 In terms of the main features of interest they have been assessed in terms of the treatment of them for the development.
- 11.4 The brick pilaster with profiled stone parapet capping are attractive features that contribute to the 'classical temple' appearance of the building. The proposed works

are to undertake detailed photographic and measured building survey of existing elevations, Remove parapet stones and retain, Western pilasters to be reduced in height and flanking walls to the north and south retained up to the existing openings. Eastern elevation and flanking walls removed to open the walled garden to the courtyard and Re-lay capping. The structure is in a dangerous state but the stone capping looks to be in reasonable condition. As these stones are the appropriate depth to cap the parapet they will provide a suitable adequately sized capping to the garden walls.

- 11.5 The brick-vaulted roof supported by cast iron beams embraces the structural technology of the time. The proposed works are to Undertake detailed photographic and measured building survey before Removing the roof. The structure is in a dilapidated state and requires significant investment to make it safe. To make the space habitable would require considerable further investments. The buildings location, the limited number of openings and the lack of a floor to the building makes it impractical to convert to a desirable use.
- 11.6 The original tall elegant arched window openings to allow plenty of light and ventilation for the engine house. Elements of the window frames remain but in a very dilapidated state. These are attractive features that contribute to the 'classical temple' appearance of the building. The proposed work is for the walls to be lowered below the cills of the arched openings on the west elevation, the eastern elevation and flanking walls removed to open the walled garden to the courtyard. The structure is in a dilapidated state and requires significant investment to make it safe. To make the space habitable would require considerable further investments. The buildings location, the limited number of openings and the lack of a floor to the building makes it impractical to convert to a desirable use.
- 11.7 In terms of external appearance, it is considered that the design of the proposed scheme is acceptable and would result in a positive feature in the conservation area which is sympathetic to the special historic character of the conservation area and provide a positive reuse for the remaining historic building. This approach is consistent with paragraph 131 of the NPPF which require account to be take of the desire of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation and have regard to a positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality.
- 11.8 The response from Historic England stated that the redevelopment of the property should be assessed whether clear and convincing justification has been provided any harm or loss to the listed building and weighs the harm against the public benefits in line with paragraph 132 and 134 of the NPPF.
- 11.19 Paragraph 132 states that Significance can be harmed or lost through alteration or destruction of the heritage asset or development within its setting. In this instance there has already been significant harm caused by the existing development son the site cutting the building off from its setting and remaining historic buildings. Paragraph 134 states that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal, including securing its optimum viable use.
- 11.10 For the reasons outlines above and due to its setting and previous removal of the remaining historic features that originally surrounded the building, there is considered to be less than substantial harm caused to the heritage asset. In line with the NPPF

this coupled with the significant public benefit by ensuring the positive redevelopment and reuse of the whole area and allowing the structure to partly remain and be a visible connection to the history of the site concludes that the proposed work to this building is acceptable and accords with the relevant policy requirements.

12. ECOLOGY

- 12.1 The application is accompanied by an Ecological assessment which states there is one dilapidated building (the engine house) which has negligible bat roost potential. This has been considered by GMEU who support the opportunities for improving the natural habitat in this area shown on the proposed plans. They agree that the proposed biodiversity enhancements (bird and bat boxes) together with the proposed lighting plan are adequate for the development and raise no objection to the scheme on ecological grounds.
- 12.2 The proposals would not therefore have any adverse effect upon protected species and are thus in accordance with policy N7: Protected Species and the guidance contained within the NPPF and PPG.

14. ARCHAEOLOGY

- 14.1 The application is accompanied by an Heritage Impact assessment and Structural Inspection report. An archaeological survey has previously been undertaken of the engine house as part of a wider study of Longlands Mill in 2003, this along with an additional archaeological survey and an archaeological desk based assessment undertaken in December 2016 for the adjacent former Foundry development site identified that there will be below-ground archaeological interest within the engine house once the current floor levels are removed. This will relate to the foundations of the 1840s steam engine.
- 14.2 The Greater Manchester Archaeological Advisory Service (GMAAS) are concerned that the reducing the engine walls to a low level and removing the features that make it distinctive as an engine house and will lose the relevance as an industrial heritage structure and state that it is a shame that the engine house has been previously isolated from the remaining mill building
- 14.3 GMAAS recommend that a programme of below-ground investigation and recording is undertaken to complement the previous survey of the extant historic fabric. And that the industrial heritage of the Castle Street Mills is commemorated through interpretation panels, heritage displays and through a published article. These can be secured through a planning condition.
- 14.4 The proposals would not therefore have any significant adverse effect upon any potential archaeology and the addition of relevant conditions will ensure appropriate surveys and interpretation is provided on the site to ensure the long term understanding and context for the building in line with guidance contained within the NPPF and PPG.

15. CONCLUSION

- 15.1 At the heart of the NPPF is a presumption in favour of sustainable development, this requires planning applications that accord with the development plan to be approved

without delay and where the development plan is absent, silent or out of date granting permission unless the adverse impacts of doing so would significantly and demonstrably outweigh the benefits when assessed against the policies in the framework as a whole or specific policies in the framework indicate that development should be restricted.

- 15.2 In this instance, the application site is designated for residential use under UDP Policy E2 (8) and as the redevelopment of the site would bring about a range of benefits including re-use of a prominent and redundant town centre/ conservation area site; visual amenity improvements; additional planting / ecological improvements; contribution to delivery of new homes; and additional expenditure in the local shops and services. There are no significant factors which outweigh these benefits and so the proposal is therefore recommended for approval subject to the conditions.

16. RECOMMENDATION

Grant listed building consent for the Works to the Grade II listed Engine House including; removal of the east elevation and roof, reduction in wall height of the north, south and west elevations to maximum 3.4m; and internal area to be made good and landscaped to form a garden area. Subject to conditions

1. The development must be begun not later than the expiration of three years beginning with the date of this permission.
2. No development, other than work to undertake site clearance, remediation and ground engineering works, shall take until such time until samples and/or a full specification of materials to be used externally have been submitted to and approved in writing by the Local Planning Authority. Such details shall include the type, colour and texture of the materials. The development shall be constructed in accordance with the approved details.
3. No development shall take place, other than site clearance and site compound set up, until the implementation of a programme of archaeological works in accordance with a Written Schemes of Investigation (WSI) submitted to and approved in writing by the Local Planning Authority. The WSIs shall cover the following:
 - i phased programme and methodology of investigation and recording to include: evaluation trenching; detailed archaeological excavation (informed by trenching); an archaeological watching brief
 - ii A programme for post investigation assessment to include: analysis of the site investigation records and finds; production of a final report on the significance of the archaeological and historical interest represented.
 - iii A scheme to commemorate the site's industrial heritage
 - iv Dissemination of the results commensurate with their significance.
 - v Provision for archive deposition of the report and records of the site investigation.
 - vi Nomination of a competent person or persons/organisation to undertake the works. The development shall be carried out in accordance with the approved details.
4. The developer shall afford access at all reasonable times to any archaeologist nominated by the Local Planning Authority, and shall allow them to observe the excavations and record items of interest.

5. During demolition, ground engineering and construction periods, no works (including vehicle and plant movements, deliveries, loading and unloading) shall take place outside the hours of 07:30 and 18:00 Mondays to Fridays and 08:00 to 13:00 Saturdays. No work shall take place on Sundays and Bank Holidays.
6. The development hereby permitted shall be carried out in accordance with the following approved plans: Heritage Impact Assessment and Design Addendum, Structural Inspection Report, SSL:17617B:50:1:1:ELV, 4654-01_1001, 4654-01_1003, C19709/03 and 2016/59 V1 Archaeological Assessment received by the council on 4 January 2018

Application Number: 17/01059/LBC
Former Site Of 10 - 12 Castle Street Stalybridge

Photo 1



Photo 2

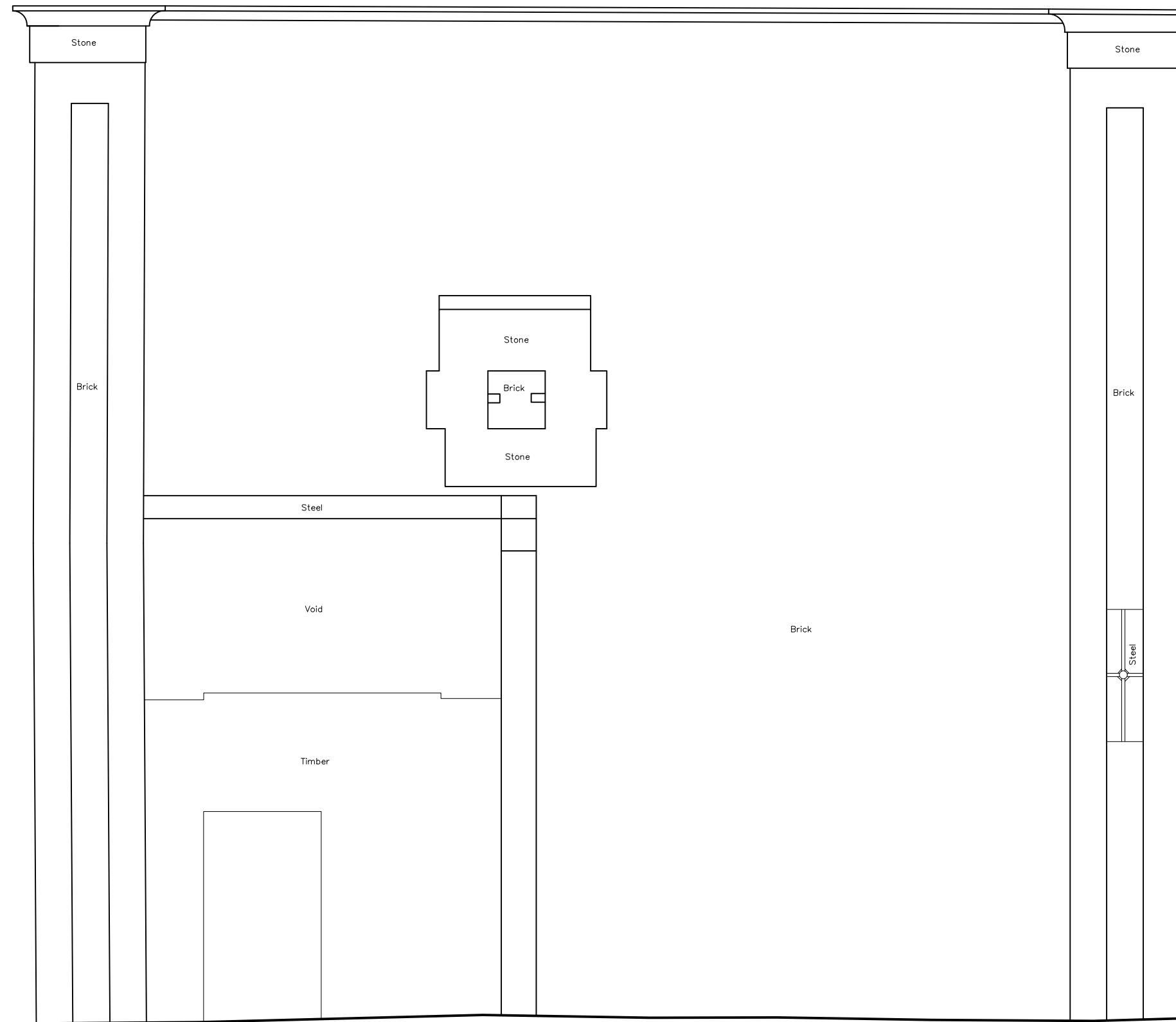


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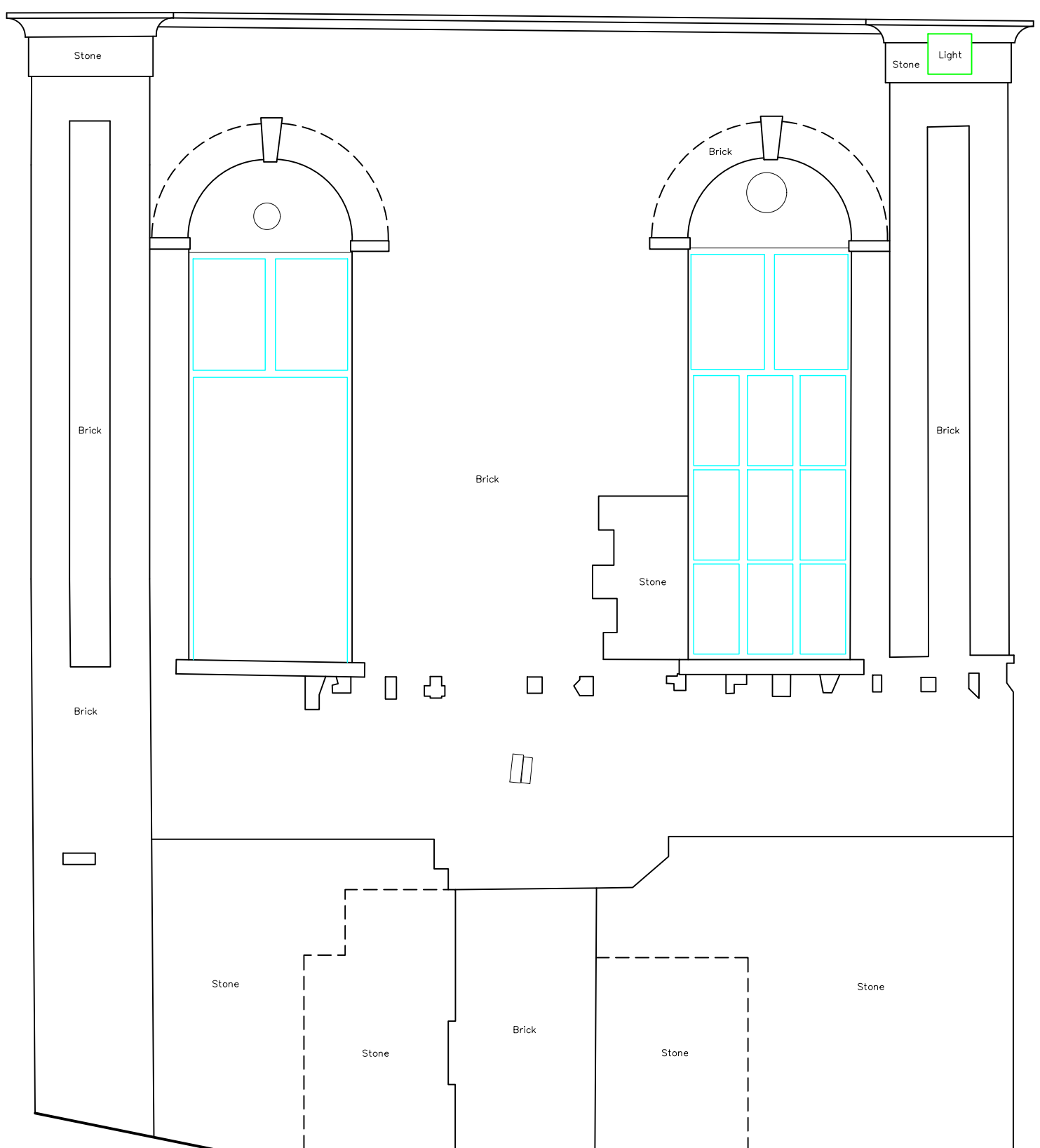
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Elevation 1



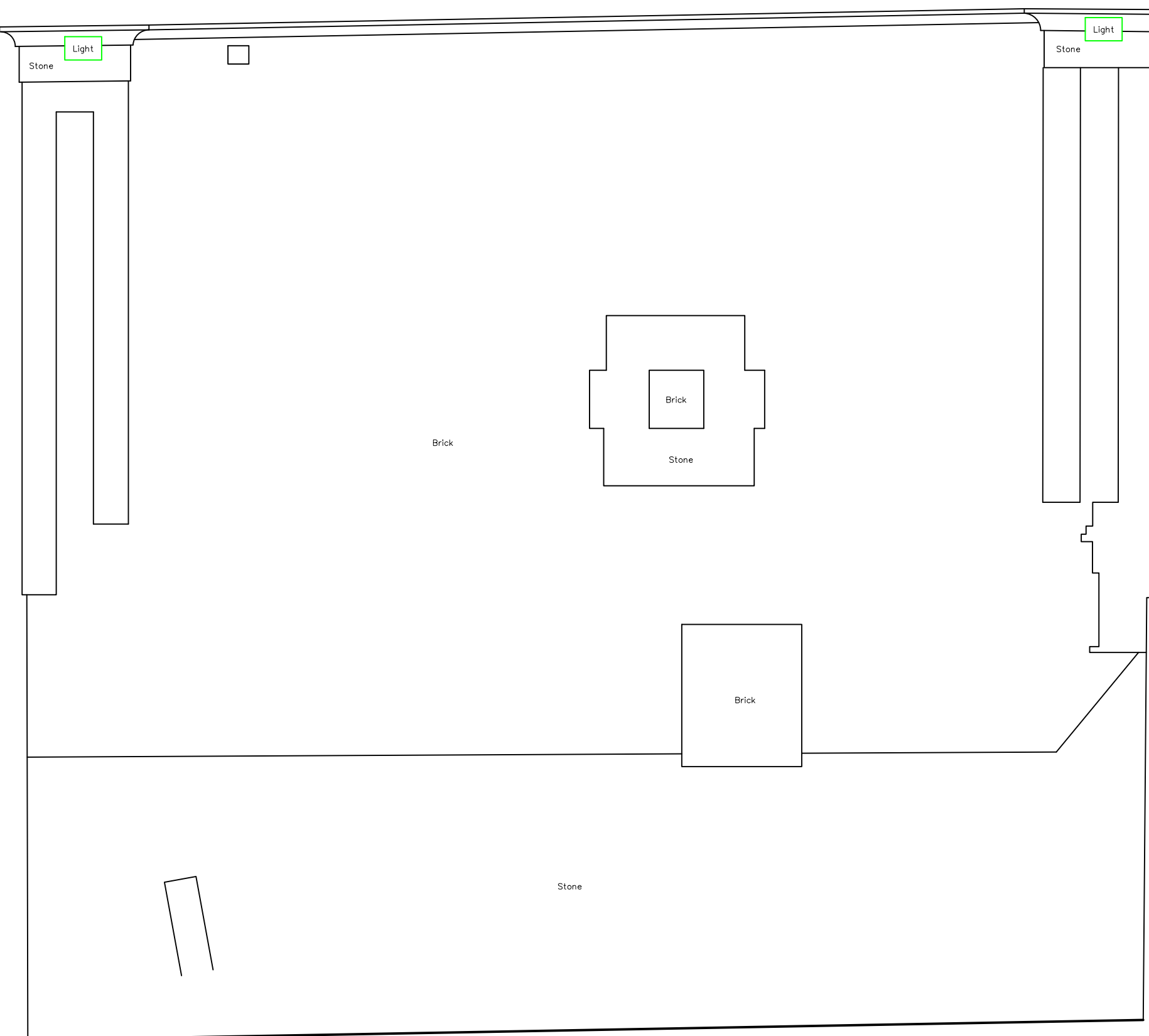
Datum 105.00m

Elevation 2



Datum 105.00m

Elevation 3



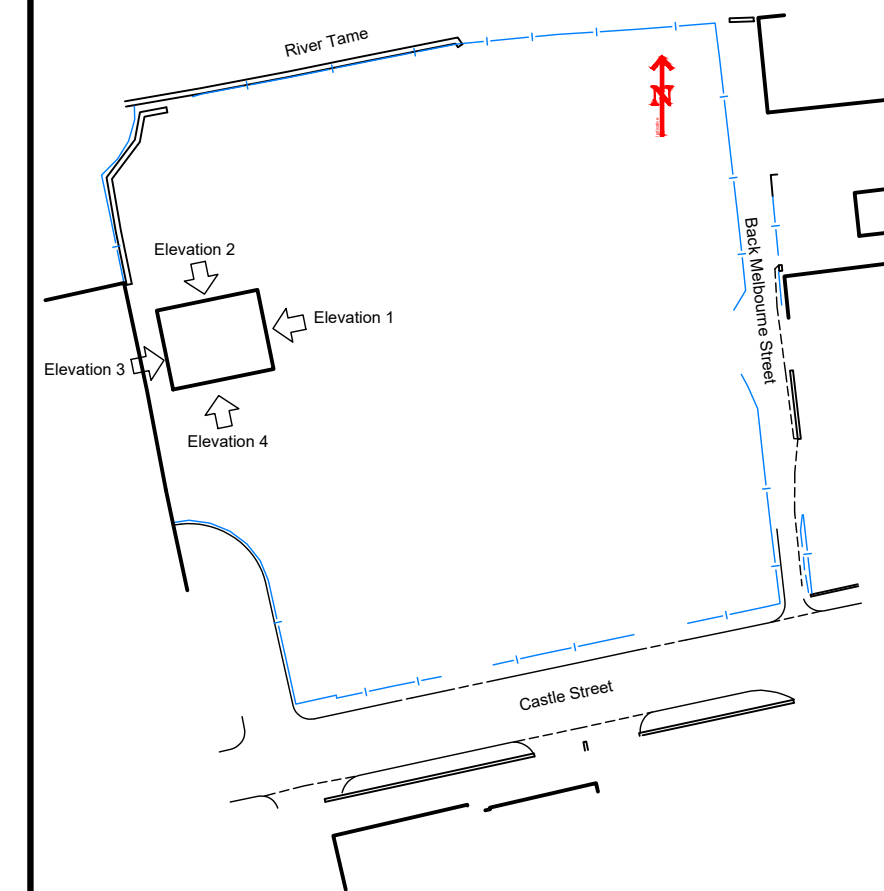
Datum 105.00m

Elevation 4

Building Survey Legend

Height Profile Descriptions	
Beam	Beam
Change in Ceiling Height	Change in Ceiling Height
Change in Floor Surface	Change in Floor Surface
Ducting	Ducting
Partition	Partition
Pipe	Pipe
Wall	Wall
Window	Window
Stairs/Ramp	Stairs/Ramp
Roof Pitch	Roof Pitch
Sloping Ceiling	Sloping Ceiling
Registration Overlay	Registration Overlay
Level Above Datum	Level Above Datum

Key Plan



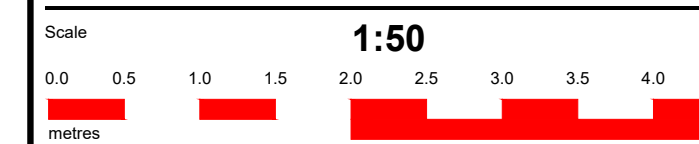
Please note:

Any discrepancy found between an Ordnance Survey benchmark and an OSGB15 computed orthometric height is likely to be due to bench mark subsidence or uplift and, assuming precise GPS survey has been carefully carried out, the orthometric height given by OSGB15 should be considered correct in preference to archive benchmark heights. OSGB15 converts GPS ellipsoid heights to orthometric heights above mean sea level.

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Levels are related to OSGB15
 Value n/a

Coordinates are related to National Grid (GPS)



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Client **Rowlinson Construction Ltd**

Title
Elevations
Castle Street
Stalybridge
SK15 1NX

Surveyed **PS** Drawn **AH**

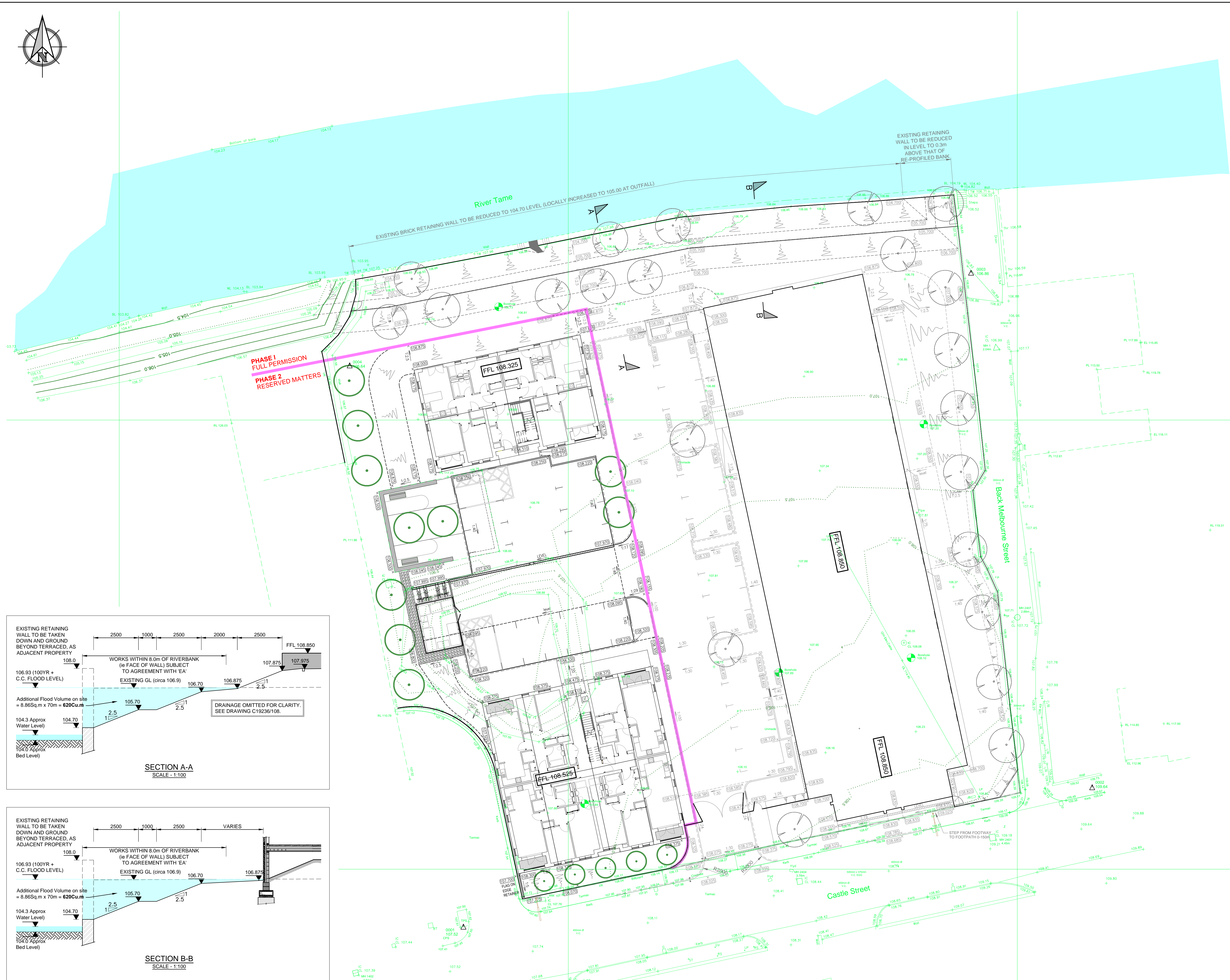
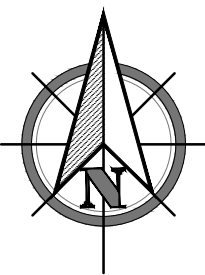
Date **December 2017** Checked **PS**

File name **17617B-ELV** Revision No. Sheet size **A1**

Drawing No. **SSL:17617B:50:1:1:ELV**



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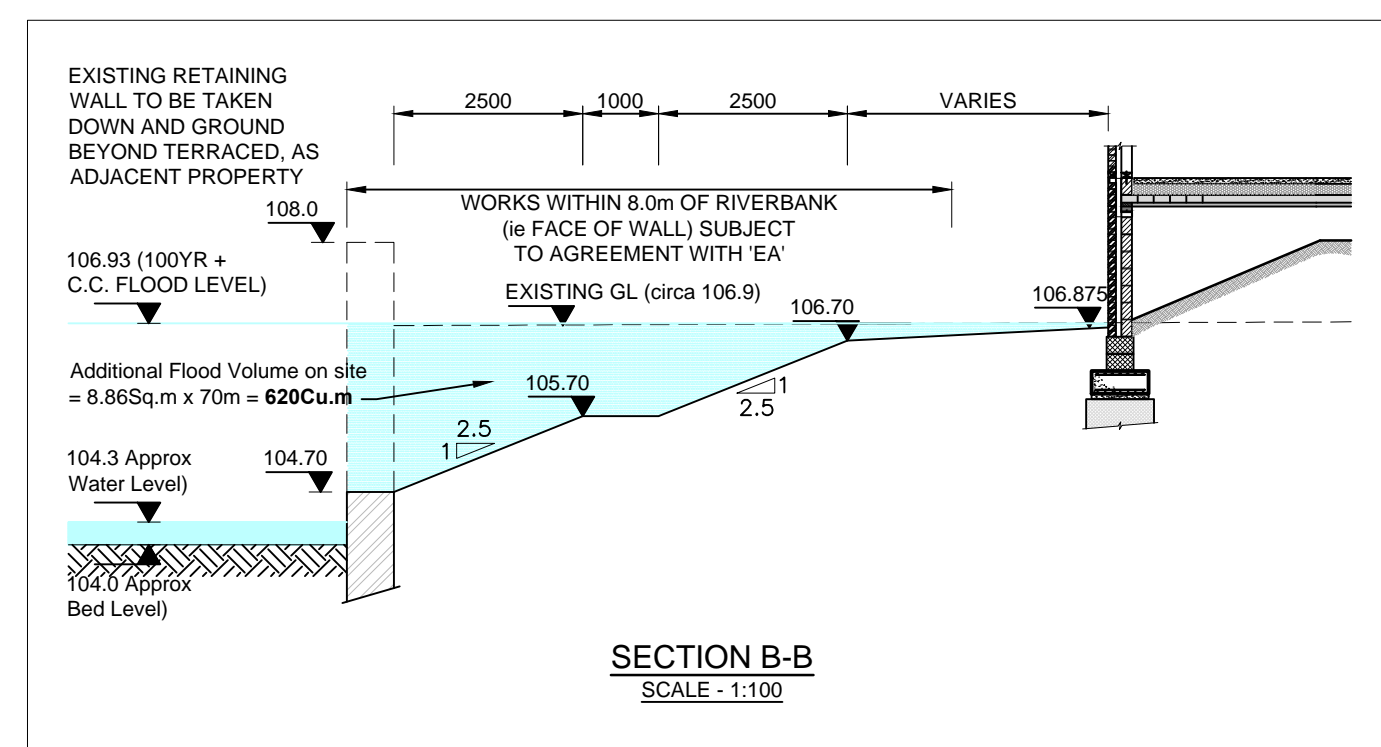
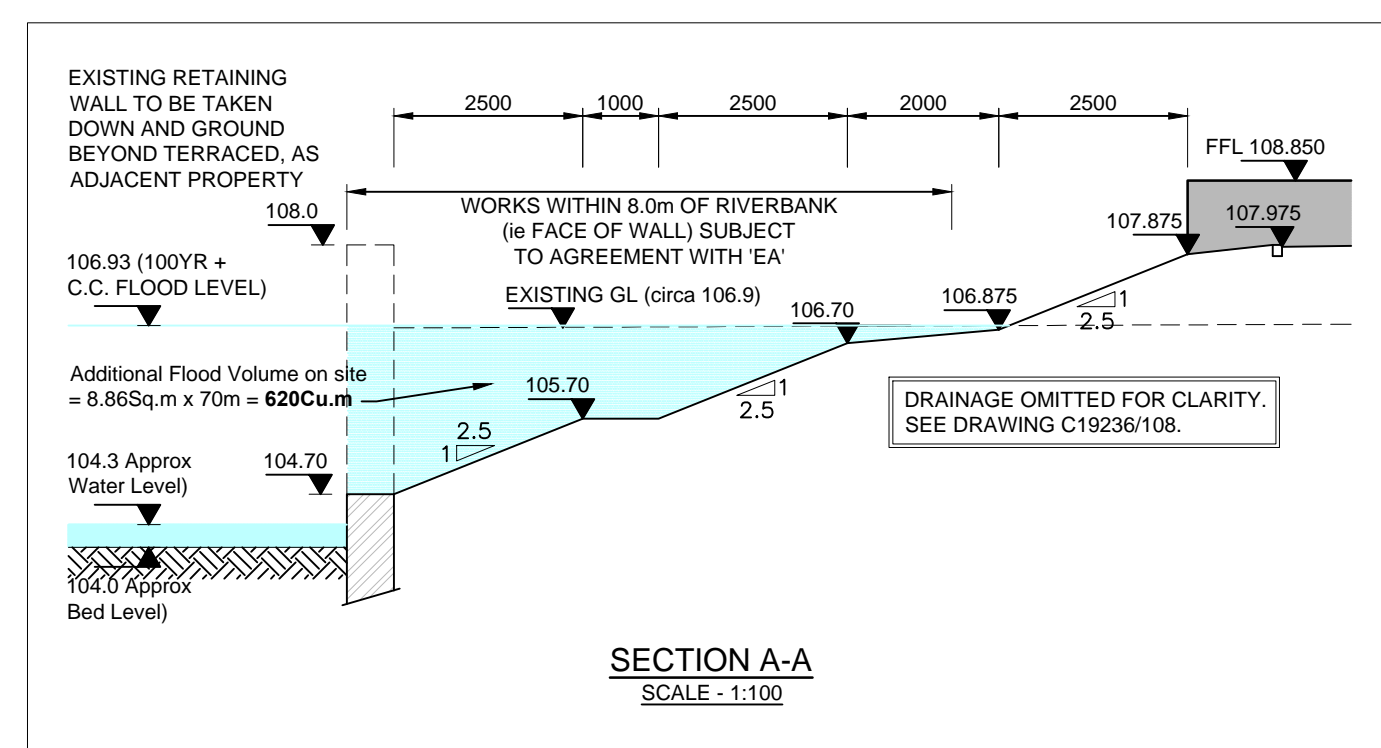


KERB TYPES

	Indicates 125x255 Half Batter Kerb (HB2) - 100mm
	Indicates 125x150 Bullnose Kerb (BN) - 25mm
	Indicates 125x150 Bullnose Kerb (BN) - Flush
	Indicates Drop Kerb (1 @ 1:12)
	Indicates 50x200 Flat Top Edging (EF)
	Indicates tangent point & Kerb Radius

GENERAL

	PROPOSED LEVELS
	PROPOSED HARDSTANDING GRADIENTS
	SLOPE CHANGE (ie RIDGE, VALLEY or FOLD)



REV	DATE	DRN	DESCRIPTION

PURPOSE OF ISSUE	INFORMATION
------------------	-------------

CLIENT	ROWLINSON CONSTRUCTIONS LTD
--------	-----------------------------

PROJECT	CASTLE STREET STALYBRIDGE (PHASE 2)
---------	-------------------------------------

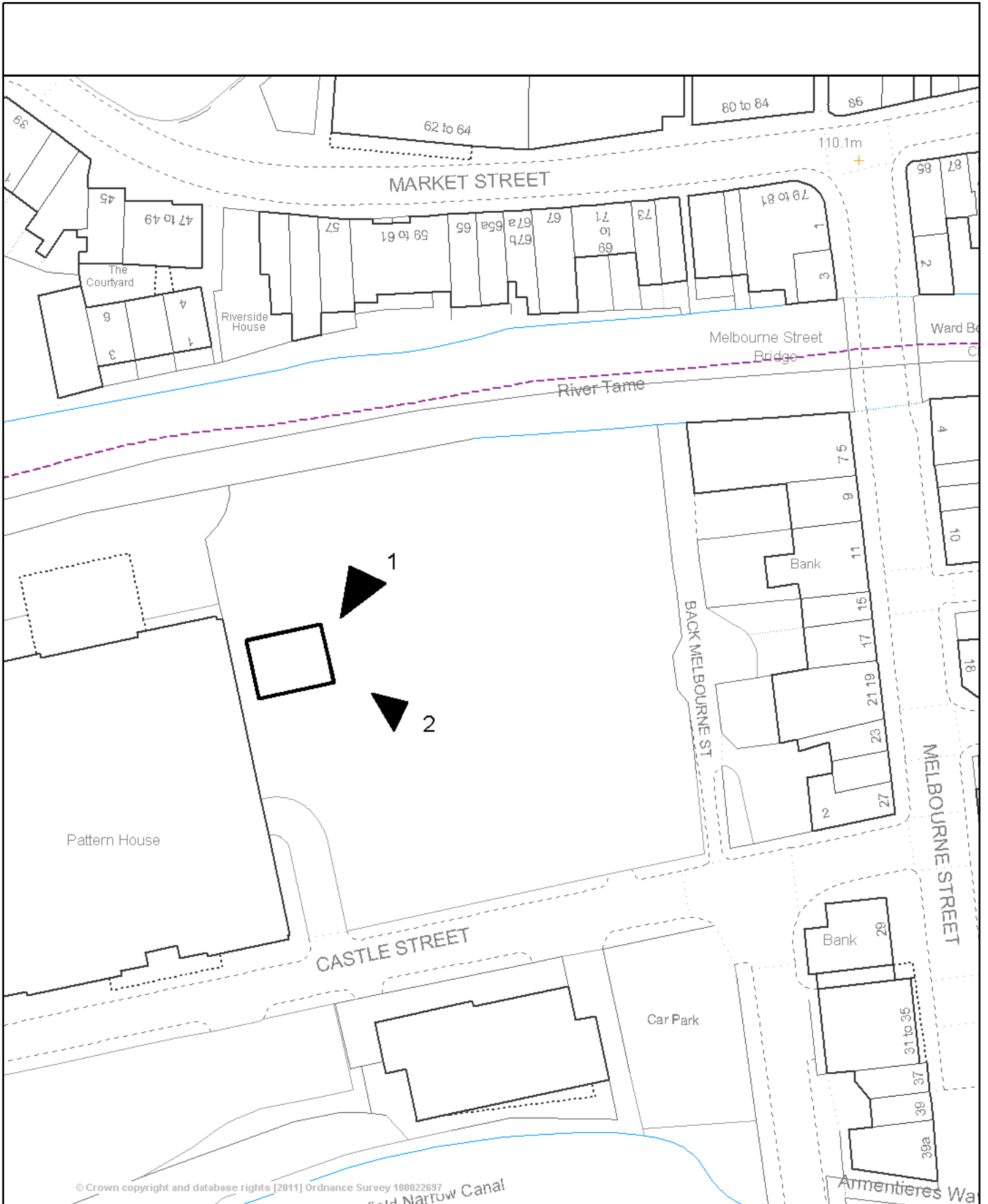
TITLE	EXTERNAL WORKS G.A. - FALLS & LEVELS
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JOB No.	DRAWING No.	REVISION		
C19709	/ 03	~		
DRAWN	CHECKED	SCALE	@ SHEET SIZE	DATE
TW	KM	1:200	A1	DEC 2017
DRAWING NOT TO BE SCALED				

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Scale 1/800 Date 15/5/2018

Centre = 396217 E 398492 N

Scale 1/800 Date 15/5/2018

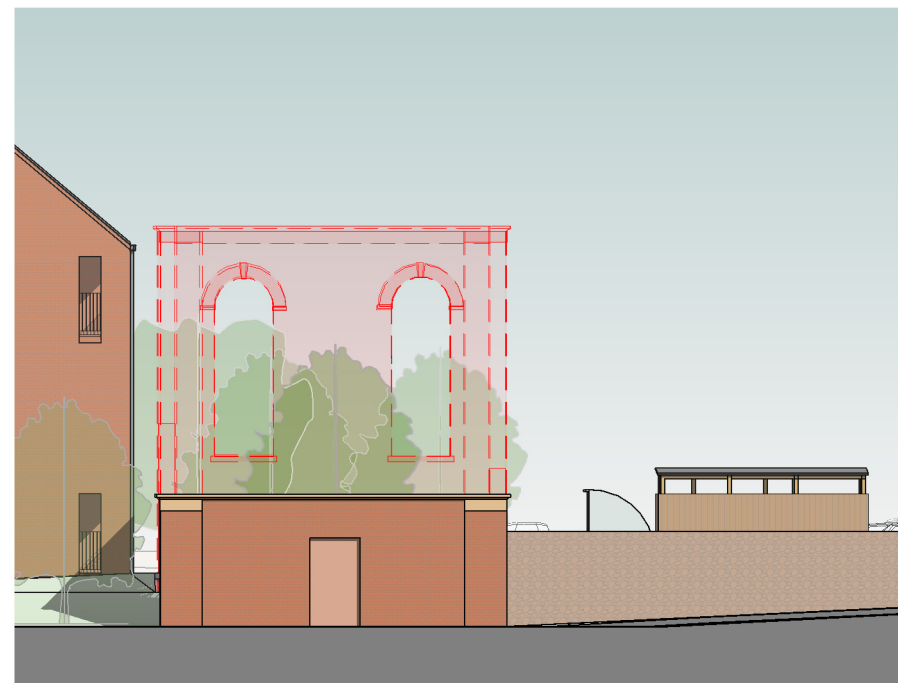
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Notes
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Scale Bars TO BE USED ONLY AS GUIDANCE

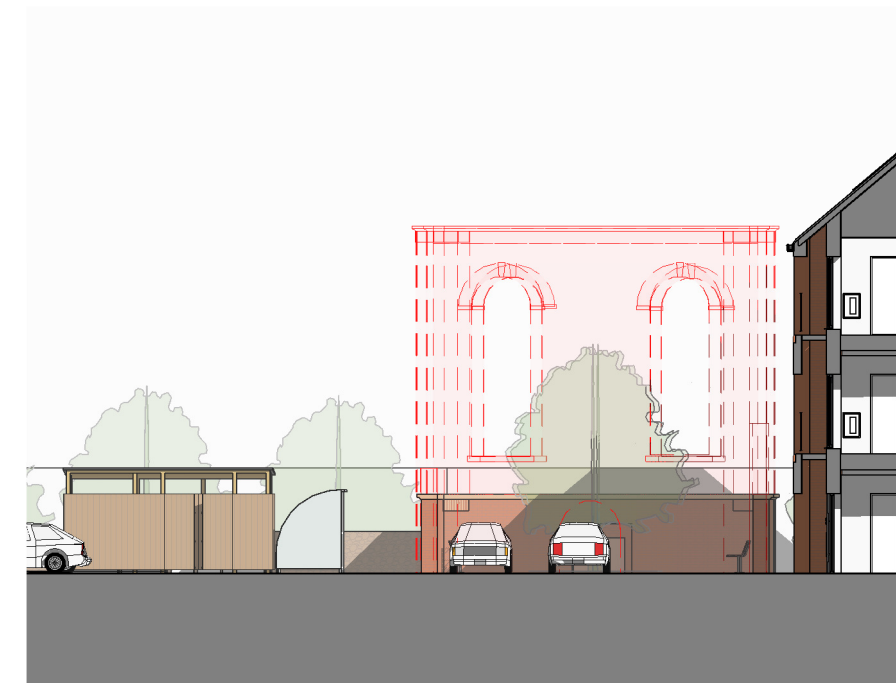
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1:200	mm	0	1000	2000	3000	4000
1:500	M	0	10	20	30	40
1:1250	M	0	10	20	30	40



WEST ELEVATION - FACING PATTERN HOUSE
 1 : 200



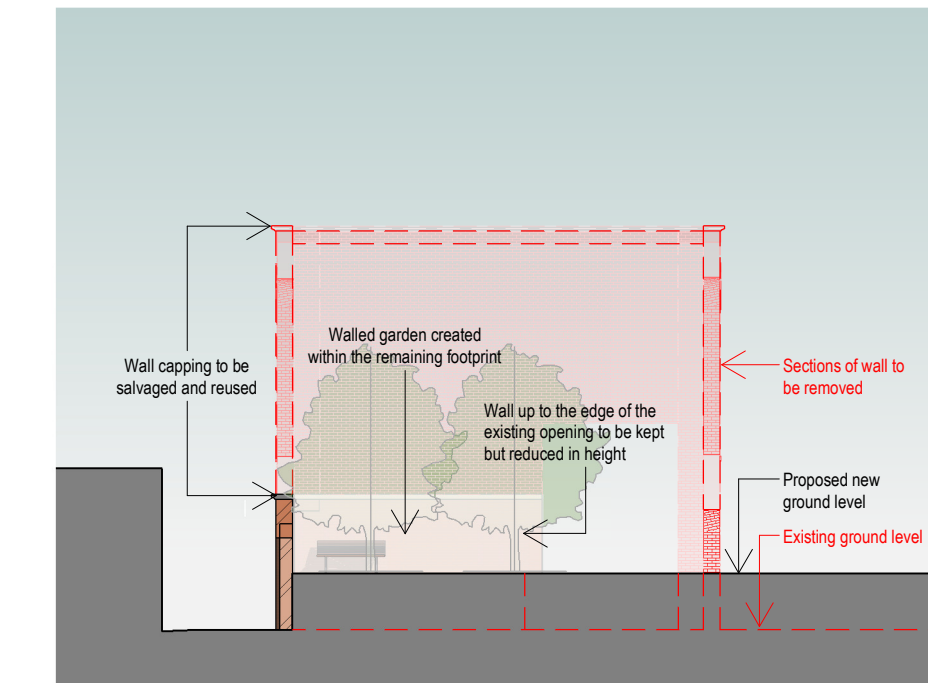
NORTH ELEVATION - FACING BLOCK C
 1 : 200



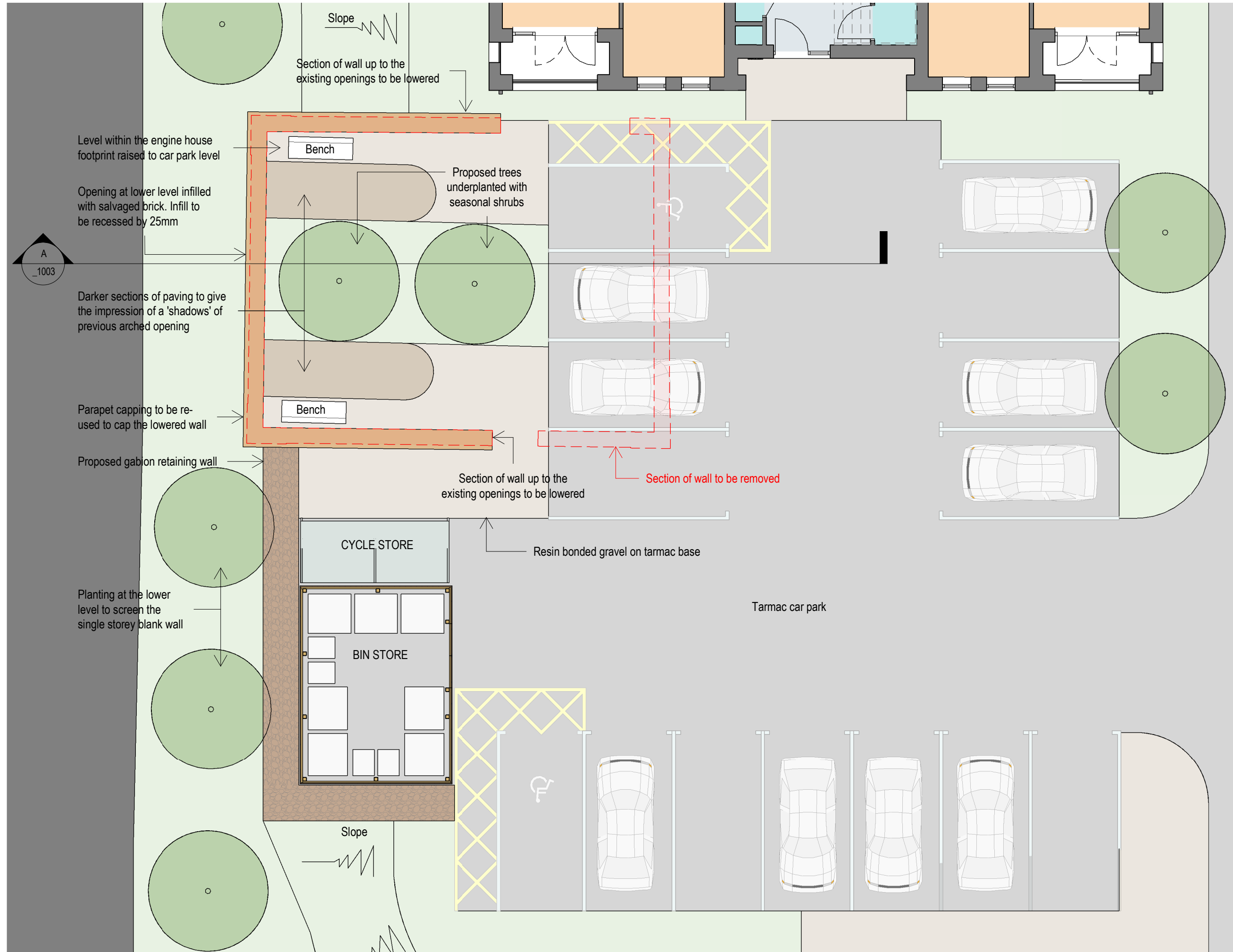
EAST ELEVATION - FACING BLOCK A (PHASE 1)
 1 : 200



SOUTH ELEVATION - FACING BLOCK B
 1 : 200



SECTION A THROUGH THE ENGINE HOUSE
 1 : 200



PROPOSED PLAN
 1 : 100



AERIAL VIEW FROM THE PATTERN HOUSE



VIEW FROM PATTERN HOUSE CAR PARK ENTRANCE



VIEW FROM THE ENTRANCE OF BLOCK C



VIEW FROM THE CORNER OF BLOCK B



VIEW FROM THE RIVER TAME

MATERIALS (LISTED BUILDING)

EXISTING STRUCTURE:
 Walls - Brick, red
 Roof - Vaulted brick arch with cast iron supports
 Windows & Doors - Not intact

PROPOSED:
 1. Walls - Height of existing walls to be reduced. Existing stone capping to be salvaged and utilised for the top of the wall. Existing brick to be salvaged for infilling the opening on the west elevation.
 2. Roof - n/a roof to be removed
 3. Windows & Doors - n/a, no windows and doors proposed
 4. Pathways - Resin bonded gravel on a tarmac base, golden in colour for the main pathways and grey for the shadow effect within the engine house garden

PLEASE NOTE: It is intended that this drawing be read in conjunction with the Heritage Impact Assessment and the Heritage Statement Design Addendum. For existing elevations please refer to survey drawing 17617B-ELV - A1 - 50 by Survey Systems

Rev	Description	By	Date
Client	MOSSCARE ST VINCENTS' HOUSING GROUP		
Job	CASTLE STREET PHASE 2, STALYBRIDGE		
Drawings	PROPOSED ALTERATIONS TO ENGINE HOUSE		
Ref	4654-01_1003		
Scale	1:100		
Date	07/12/17	As indicated	@ A1
Author	EMc	EMc	TW
Checked	Planning	Tender	Construction
Approved	Design	Contract	As Built



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Application Number 17/01060/REM

Proposal	Approval of reserved matters for the erection 18 apartments with associated landscaping, open space, and car parking (scale, layout, appearance and landscaping) following outline consent granted under 17/00019/FUL (Parcel B).
Site Location	Former Site of 10 - 12 Castle Street, Stalybridge Tameside
Applicant	Mossclare Housing and Rowlinson Construction
Recommendation	Subject to no objections being received from the Environment Agency, Grant Reserved Matters consent subject to conditions
Reason for report	A Speakers Panel decision is required because the application proposes a major development, as defined by The Town and Country Planning (Development Management Procedure) (England) Order 2015.

1. APPLICATION DESCRIPTION

- 1.1 This is the reserved matters application following on from the earlier hybrid planning application which granted outline consent on parcel A of the site for residential development with only details of access provided at that stage. This application seeks permission to develop the site with two new three storey apartment blocks comprising of 18 homes with associated car parking and landscaping with detailed approval of appearance, landscaping, layout and scale.
- 1.2 The proposed 3-storey apartment blocks would be located at both the northern and southern boundaries of the site and would comprise of 2 apartment block. The northern one with a dual pitched roof measuring approximately 12m long, 19m wide and 13.8m to the ridge and 8.8m to the eaves containing 6no. two-bedroom apartments and the southern building located on the Castle Street frontage with a double dual pitched roof measuring approximately 19m long, 19m wide 12.5m to the ridge and 9.3m to the eaves, containing 3no. one-bedroom apartments and 9no. two-bedroom apartments.
- 1.3 The materials proposed for the buildings will be predominantly red/brown brickwork with grey roof covering and grey powder coated aluminium window surrounds. The apartments would include Juliet balcony balustrades, window cowls, parapets and patio door panels in various opaque shades of glass.
- 1.4 Vehicular Access is proposed from Castle Street using the existing access approved under 17/00019/FUL with the submitted plan showing an additional 15 car parking spaces being situated centrally between the two apartment blocks. The previous scheme included a secure gated access for vehicles with a fob controlled vehicular access. A separate pedestrian access gate is proposed to the south of the site off Castle Street.
- 1.5 The application has also been supported by a comprehensive suite of supporting assessments and documentation. These include: -

Topographical Survey (Ref.: SSL:17617A:200:1:1)
Stormwater Drainage Appraisal (Ref.: C19709/01)
Phase II Stormwater Drainage Addendum (Ref.: KM/C19236)
External Works G.A. - Surface Water & Foul Drainage Plan (Ref.: C19709/02)
Falls and Levels Survey (Ref.: C19709/03)
Typical Drainage Details (Ref.: C19709/04)
Flood Risk & SUDs Assessment (Ref.: KM/C19236, Rev A – 11/01/17)

Addendum to Flood Risk & SUDs Assessment (Ref.: KM/C19236 – 23/02/17)
Soft Landscaping Plan (Ref.: D6325.002)
Hard Landscaping Plan (Ref.: D6325.001A)
Waste Management Plan
Bin Storage Provision – Phase 2 (4654/EMc)
Sustainability Statement (Mosaic Town Planning)
Crime Impact Statement
Heritage Impact Assessment
Phase 1 Geo-Environmental Site Assessment (Ref.: 11-493-r1-RevA)
Phase 2 Geo-Environmental Site Assessment (Ref.: 11-493-r2-RevA)
Remediation & Enabling Works Strategy (Ref.: 11-493-r3)
Lighting Plan (Ref.: D30180/JM/A)
External Lighting Design Summary (Pozzoni)

2. SITE & SURROUNDINGS

- 2.1 The application site itself is located within Stalybridge Town Centre and lies within the Stalybridge Conservation area. It comprises of 0.5 ha of previously developed land fronting Castle Street to the south and the River Tame to the north and contains mostly cleared land and rubble. There are existing commercial properties across the river to the north and east, a large restaurant to the south and residential development to the west. The character of the surrounding area is largely determined by its position on the fringe of Stalybridge Town Centre but comprising mainly residential uses with a mix of retail, restaurants and car parking nearby.
- 2.2 The site was previously used as part of the Castle Street Mills, which have now largely been cleared with the exception of the Grade II Listed engine house which still remains and is the only current built feature on the site.
- 2.3 In terms of topography, the land slopes gently down to a retaining wall on the northern boundary with the River Tame running below. The site boundaries consist of 2m high palisade fencing along the south, east and northern edge of the site with wire mesh fencing, brick and wooden board fencing along the boundary with Pattern House to the west.
- 2.4 The sites is located within 200m of Stalybridge Bus Station offering numerous services to the whole of Tameside and Manchester and 300m of Stalybridge Train Station providing regular services to Leeds, Liverpool and the rest of Greater Manchester to the west. The site is within 300m of the nearest primary school, 200m of the nearest doctors' surgery and, given its town centre location, in close proximity to a wide range of shops, services and facilities.

3. PLANNING HISTORY

- 3.1 The site has a long planning history but the most relevant to this application is the most recent permission 17/00019/FUL granted in March 2017 for the Hybrid planning application seeking; Full permission for ground works and remediation of entire site; Full permission for works to masonry wall and terracing of land adjacent to river bank; Full permission for the erection of a three storey block comprising 38 dwellings with associated car parking and landscaping on parcel A of the site and Outline permission with all matters reserved except from access for residential development up to 24 dwellings on Parcel B.

Other permissions on the site and surrounding include:

06/00117/OUT A mixed use development across a larger site comprising of up to 275 dwellings, 3,000 square metres commercial floorspace (Use Classes A1, A2, A3, B1, D1

and D2) and a hotel up to 2,500 square metres and up to a maximum of 175 parking spaces October 2006

03/01099/R3D Realignment of Castle Street (involving exhumation of former Caroline Street Methodist Church Burial Ground), closure of redundant section of Castle Street and provision of car parking area for proposed Longlands Mill development September 2003

04/00245/LBC Seek Listed Building Consent for demolition of blocks E, H2 and H3 and the east part of H Approved May 2004

04/01864/LBC Seek Listed Building Consent for partial demolition of buildings April 2005

05/00545/R3D Provision of temporary car park and new boundary fencing on two side to match existing Approved June 2005

06/00105/LBC Listed Building Consent for a mixed use development comprising of internal and external alterations, part demolition and conversion of the grade II listed Longlands Mill buildings to create 44 residential dwellings and new buildings to provide 60 dwellings and 1750m² gross commercial floor space to be used as either use classes A1,A2,A3,A4,B1,D1 and D2 and 267 car parking spaces, highways alterations to Castle Street and other associated works. Approved October 2006

06/00104/FUL Mixed use development comprising of alterations and conversion of the grade II listed Longlands Mill Buildings to create 44 residential dwellings (32 x one bed and 12 x two bed units) and new buildings to provide 60 dwellings (29 x one bed 31 x two bed units) and 1750m² gross commercial floor space to be used as either use classes A1, A2, A3, A4, B1, D1 and D2 and 267 car parking spaces, highways alterations to Castle Street and other associated works Approved October 2006

08/00388/FUL 2no. additional apartments within development approved under application 06/00104/FUL and rearrangement of service space Approved July 2007

11/00712/LBC Brick up rear openings in ground floor rear wall Approved October 2011

4. RELEVANT PLANNING POLICIES

Tameside Unitary Development Plan (UDP) Allocation Stalybridge Town Centre
Stalybridge Town Centre Conservation Area

Tameside UDP

Part 1 Policies

1.3: Creating a Cleaner and Greener Environment.

1.4: Providing More Choice and Quality Homes.

1.5: Following the Principles of Sustainable Development

1.12: Ensuring an Accessible, Safe and Healthy Environment

Part 2 Policies

E2 (8) Development Opportunity Areas Castle Street/ Longlands Mill

H7: Mixed Use and Density.

S1 Town Centre Improvement

T1: Highway Improvement and Traffic Management. T11: Travel Plans.

C1: Townscape and Urban Form

C2 Conservation Areas

C4 Development in or Adjoining Conservation Areas
C6 Setting of Listed Buildings
C10 Development Affecting Archaeological Sites
N4: Trees and Woodland.
N5: Trees Within Development Sites.
N6 Protection and Enhancement of Waterside Areas
N7: Protected Species
OL7 Potential of Water Areas
OL9 Derelict Land Reclamation
OL10 Landscape Quality and Character
OL15 Openness and Appearance of River Valleys
MW11: Contaminated Land.
U3: Water Services for Developments

Other Policies

The Greater Manchester Joint Waste Development Plan Document April 2012
The Greater Manchester Joint Minerals Development Plan Document April 2013
Residential Design Supplementary Planning Document
Trees and Landscaping on Development Sites SPD adopted in March 2007.

National Planning Policy Framework (NPPF)

Section 1 Delivering sustainable development
Section 2 Ensuring the vitality of town centres
Section 4 Promoting sustainable transport
Section 6 Delivering a wide choice of high quality homes
Section 7 Requiring good design
Section 8 Promoting healthy communities
Section 10 Meeting the challenge of climate change, flooding and coastal change
Section 12: Conserving and enhancing the historic environment

Planning Practice Guidance (PPG)

This is intended to complement the NPPF and to provide a single resource for planning guidance, whilst rationalising and streamlining the material. Almost all previous planning Circulars and advice notes have been cancelled. Specific reference will be made to the PPG or other national advice in the Analysis section of the report, where appropriate.

5. PUBLICITY CARRIED OUT

5.1 As part of the planning application process 279 notification letters were sent out to neighbouring properties on 24th January 2018 a notice was also posted at the site and displayed in a local newspaper on the 15th February 2018.

6. RESPONSES FROM CONSULTEES

Arboricultural Officer

The trees within the site are of low amenity value. A landscape design should include suitable planting.

United Utilities

No objection to the proposed development subject to conditions requiring details of foul and surface water drainage are attached to any approval. Makes note that a public sewer

crosses the site and if a diversion of the affected public sewer is required this will need to be done at the applicant's expense.

Greater Manchester Ecology Unit

The habitat creation plan which also shows the biodiversity enhancements (bird and bat boxes) (TEP drawing no D6325.002B dated 28/03/2019) together with the lighting plan (Kingfisher Lighting drawing no D30180/JM/A dated 29/03/2017) submitted with the application appear to be adequate for the development. The plans should therefore be implemented in full.

Greater Manchester Archaeological Advisory Service

GMAAS are satisfied that, apart from the engine house which is a significant relict of the site's rich industrial heritage there is no further archaeological work required on site. No objections subject to conditions recommending that a programme of below-ground investigation and recording is undertaken to complement the previous survey of the extant historic fabric and that the industrial heritage of the Castle Street Mills is commemorated through interpretation panels, heritage displays and a published document..

Historic England

The application should be considered in line with national policy and legislation.

Environment Agency

No objection in principle, but advise the remedial targets for controlled waters require updating and re-submitting along with copies of the Rev B site investigation reports

Greater Manchester Police Design For Security

No objections subject to the proposed development being designed and constructed in accordance with the recommendations contained within the submitted Crime Impact Statement

Head of Environmental Services Environmental Health

Contaminated Land: recommend that a standard contaminated land condition is attached to any planning approval granted for residential development at the site. The information provided to date will go some way towards satisfying the requirements of this planning condition.

Head of Environmental Services Highways

In Highways terms the site is in a sustainable location within the town centre accessible by both bus and rail, therefore the level of car parking suggested is acceptable and raise no objections subject to conditions.

Head of Environmental Services – Waste

Advised that the following bin storage will be required 2 x 1100 litre Eurobins & 2 x wheelie bins for domestic waste, 3 x 1100 litre Eurobins for co-mingled glass, plastic and tins, 2 x 1100 litre Eurobins for paper and cardboard and 2 x 240 litre wheeled bins for food waste.

7. SUMMARY OF THIRD PARTY RESPONSES RECEIVED

In response to the original notifications 2 objections have been received from households both from Pattern House. The grounds given for objecting are:

Amenity

Loss of views and light to properties in Pattern House.

Disruption from building / construction traffic.

Loss of privacy due to the close proximity of the proposed building.

Financial
Devalue Surrounding Properties.
Loss of earning due to disturbance

Heritage
Objection to significantly changing the Grade 2 listed Engine House

8. ASSESSMENT

The principal issues in determining this application are:

- Principle of Development
- Conservation Area and Listed Buildings
- Layout and Design
- Amenity
- Highway Safety and Accessibility
- Ground Conditions
- Ecology
- Trees and Landscaping
- Drainage, Flood Risk
- Environmental Health
- Planning Obligations
- Refuse
- Other Matters

9. PRINCIPLE OF DEVELOPMENT

- 9.1 The proposed development would bring about the final part of the redevelopment of a site long-identified for residential use as a 'Development Opportunity Area' under policy E2 (8) of the UDP and would also finally secure the beneficial re-use of all of a large, derelict and contaminated site within Stalybridge Town Centre and which has detracted from the Stalybridge Conservation Area for many years. The location has excellent access to a wide range of shops and services within the town centre as well as the public transport network via the bus interchange and rail station that provides good access to both Greater Manchester and Yorkshire.
- 9.2 The proposals would also make a valuable contribution to housing land supply within the borough and would provide a mix of one and two bed homes in a well-designed scheme and part of the wider redevelopment of the whole site.
- 9.3 In overall terms, the delivery of new homes in this location would accord with the aims and policy objectives of the adopted UDP and having particular regard to the guidance contained within the NPPF and Practice Guidance and should be supported. There are however a number of detailed matters which also need to be considered and which are now discussed in more detail below.

10. HERITAGE ASSETS: CONSERVATION AREA AND LISTED BUILDING

- 10.1 The principles within SPD policy RD2 aligns with UDP policy C1 and C2 require development to respect the distinct settlement pattern and townscape of the area.
- 10.2 In this respect, the proposed massing and orientation of the apartment blocks is reminiscent of the industrial/mill buildings which once occupied this site and the apartment

blocks have been designed to align with the neighbouring residential development at Pattern House and Longlands Mill and that approved under 17/00019/FUL to reinforce this character. Furthermore, the proposed transition in scale from the five storey Pattern Houses to the existing 19th century, three storey buildings along Melbourne Street to the east is also considered to be acceptable and would also help to integrate the development with the properties on the adjacent river bank along Market Street.

- 10.3 In terms of external appearance, it is considered that the design of the proposed scheme is acceptable and would result in a new additions to the conservation area which is both sympathetic to the special historic character of the conservation area and its historic buildings and yet which incorporates contemporary features to ensure a scheme which adds to the character and quality of the area by assisting with the transition between old (Melbourne Street) and new (Pattern House). This is achieved through the use of a traditional brick construction but with inclusion of a range of contemporary features including window cowls, balcony's, coloured glazing, and double dual pitched roof rather than simply replicating the entirely traditional building materials and styles including stone detailing and pitched slate roofs found within the wider conservation area.

11. ENGINE HOUSE

- 11.1 The application site occupies an area which once housed Castle Street Mills which has been cleared apart from the Longlands Mill building converted into residential apartments and the remaining engine house which is now separated from the original mill building by the modern Pattern House residential block. The Engine House is curtilage listed (Grade II) with the Longlands Mill building. The associated applications considered elsewhere on this agenda (17/01058/FUL and 17/01059/LBC) pertain to both planning and listed building consent for works to partially demolish the engine house leaving low level walls to three sides to create a communal garden area.
- 11.2 The proposed works to the engine house are not considered as part of this application and have been excluded from the site edged red. The residential blocks the subject of this application have been designed to fit around the remains of the engine house within the site which allows that area to be used as part of the landscaping for the site.
- 11.3 Taking into account the above, it is considered that the proposed development would enhance the special character of the conservation area and would preserve and enhance the setting and appearance of the listed building and the proposal would therefore accord with UDP Policies C1, C2, C4 and C6 and NPPF paragraph that an acceptable scheme at reserved matters stage could be achieved.

12. LAYOUT AND DESIGN

- 12.1 In terms of layout and design of the scheme, the proposed layout of the scheme around a central court yard means that both sides of the proposed apartment blocks would benefit from some degree of solar gain and continues the strong building line already established on Castle Street. Furthermore, long views through the site to the river cutting can also be maintained.
- 12.2 In terms of scale and mass, it is also considered that the development is acceptable and would fit within the surrounding area and assisting with the transition between surrounding properties from Pattern House, to those currently under construction on the east of the site and those further east on Melbourne Street and on the opposite side of the river bank.
- 12.3 As set out in the previous section, the proposed palette of materials using red brick and grey details to the roof, fenestration and windows coupled with areas of glazing with

coloured sections would also result in a building which is sympathetic and compatible with the character of the locality but with interesting design features that would complement the character and appearance of the area in accordance with UDP and SPD policies and is acceptable.

- 12.4 In overall terms the development is acceptable taking into account the site characteristics and relationships with neighbouring buildings and would comply with the design and conservation based policies in the UDP, SPD and would accord with the guidance contained within the NPPF and PPG.

13. RESIDENTIAL AMENITY

- 13.1 In protecting the amenities of both future and existing occupiers of residential properties, minimum separation distances are required between buildings to help achieve this. Separation distances are considered to be necessary in cases where it is appropriate to ensure privacy due to overlooking of windows and gardens might be significant.
- 13.2 The minimum separation distances are set out in SPD policy RD5 which also makes allowances for degree of angle, height of buildings and differences in site levels.
- 13.3 The policy confirms a minimum separation distance between habitable room windows on two storey developments of 21 metres where habitable room windows directly overlook. Added to this should be 3 metres for every additional storey and 1m for every 1m difference in ground level. This distance is reduced to 14 metres across street frontages.
- 13.4 The proposed west elevations of the building will maintain a 22.6m separation to the elevation of in Pattern House and an average of 14m across the access road and car park from the under construction properties in Block A within the site to the east. These distances to both the existing residents in Pattern House and those within the site ensures that the proposed development would meet the requirements of this policy which should ensure there is no significant impact on the occupiers of neighbouring properties.
- 13.5 The site layout also shows that sufficient spaces exists around the blocks for residential amenity for the proposed dwellings and this is in line with the councils requirements in the SPD.
- 13.6 Whilst neighbours have commented on the likely impact of the development on neighbouring properties in relation to loss of views and privacy what should be considered is the proposed scheme is a three storey block (ground, 1st and 2nd floor and pitched roof) whilst the neighbouring Pattern House is substantially higher with the residential properties sitting above a ground floor level car park deck. With the separation distance achieved it is not considered that there will be any substantial loss of amenity to these residents. Furthermore, the site was always identified for further future development both at earlier planning application stage when the development including Pattern House was approved and also as part of the UDP allocation under E2 (8).

14. HIGHWAY SAFETY AND ACCESSIBILITY

- 14.1 The site is located in a highly accessible location within the town centre and close to the train station, bus interchange, local shops and two national supermarkets whilst also providing safe and secure cycle parking provision. Consequently the proposed car parking provision of around 80% is considered to be entirely acceptable in this location. As a result of this, it is not predicted that the amount of development proposed would have any significant impact on the local highway network. Likewise, the low volume of traffic expected and good accessibility also means that a travel plan condition is not required.

- 14.2 In terms of highway safety, the proposed vehicular access from Castle Street will utilise an existing access point and is considered to be safe and satisfactory by the Head of Environmental Services - Highways.
- 14.3 The development includes provision for cycle storage and this has been confirmed as 20 spaces, utilising a two-tier cycle storage system. This brings the level of cycle storage over 100% which is a positive contribution to the scheme and considered to be acceptable in this location.
- 14.3 Taking these factors into account, officers are satisfied that the proposed development is acceptable in terms of access, highway safety and parking provision and the development complies with UDP Policies T1, T7, T10 and T11 as well as Section 4 of the NPPF.

15. GROUND CONDITIONS

- 15.1 The application is supported by a contaminated land risk assessment which considers the work already undertaken as part of the 2017 permission and has been reviewed by Environmental Health and The Environment Agency. Copies of revised site investigation reports have been requested however to date comments on these have not yet been received.
- 15.2 With respect to risk to controlled waters the Environment Agency has commented that although they are satisfied with the remediation strategy proposals the targets for controlled waters require updating due to the use of the out of date Remedial Targets Worksheet. It has been requested that the remedial target calculations be updated and resubmitted. This has not yet been received.
- 15.3 A condition was placed on the outline consent requiring further work regarding contamination to be undertaken prior to the commencement of development. Subject to this condition being satisfied and confirmation that the further information requested by the Environment Agency is satisfactory, officers can then be satisfied that the requirements of the NPPF in terms of contaminated land can be addressed and the site developed without any unacceptable impacts on human health or controlled waters.
- 15.4 In this instance, the site is not in an area at risk from former coal workings and the development is acceptable in accordance with policy MW11: Contaminated Land.

16. ECOLOGY

- 16.1 The application is accompanied by an Ecological assessment which has been considered by GMEU who support the opportunities for improving the natural habitat in this area shown on the proposed plans. They agree that the proposed biodiversity enhancements (bird and bat boxes) together with the proposed lighting plan are adequate for the development and raise no objection to the scheme on ecological grounds.
- 16.2 The proposals would not therefore have any adverse effect upon protected species and are thus in accordance with policy N7: Protected Species and the guidance contained within the NPPF and PPG.

17. TREES AND LANDSCAPING

- 17.1 The Council's tree officer has been consulted and comments that the application site currently contains no trees of any amenity value.

- 17.2 The key features of the submitted landscape plan include;
Row of proposed trees along the site boundary with Castle Street
Row of proposed trees along the site boundary with Pattern House
Tree planting within car parking area
Tree planting along river frontage
Different coloured surfaces around footpaths and car parking areas within the site.
- 17.3 The inclusion of the line of trees along the southern boundary to Castle Street and between the existing Pattern House building will benefit the site and area generally and it is considered the proposed development and landscape proposals will enhance both the development and character of the area and would comply with UDP policies N4 and N5.

18. FLOOD RISK AND DRAINAGE.

- 18.1 As the application site falls within draft flood zone 2, it must be assessed against the requirements of the NPPF which aims to ensure that inappropriate development is avoided in areas at risk of flooding which has been demonstrated through this process, In this regard, the Environment Agency have raised no objection.
- 18.2 In terms of the site drainage, United Utilities state they have no objection to the proposed development subject to conditions requiring further details of the foul and surface water drainage. No concerns have been raised regarding sewer capacity in the area.
- 18.3 In the absence of any technical objection the proposal is now considered to accord with policy U3 and the guidance contained within the NPPF and PPG subject to satisfactory compliance with conditions.

19. CONSTRUCTION NOISE

- 19.1 Comments have been received from neighbours regarding the potential for disruption at construction stage whilst the development is undertaken and built out. Unfortunately, some degree of disruption is an inevitable consequence of development and this cannot be used as a reason to resist planning proposals; however, the local authority can impose conditions to assist with mitigating some of the impacts including agreeing a site construction management plan with the developers and also restricting the hours in which construction works can be carried out.

20. DEVELOPER OBLIGATIONS

- 20.1 The application site is located on Council owned land and therefore there is no requirement for any S106 obligations in this instance.

21. REFUSE

- 21.1 The waste services team were consulted on the application and have advised that the following bin storage will be required for the 18 units; 2 x 1100 litre Eurobins & 2 x wheelie bins for domestic waste, 3 x 1100 litre Eurobins for co-mingled glass, plastic and tins, 2 x 1100 litre Eurobins for paper and cardboard and 2 x 240 litre wheeled bins for food waste.
- 21.2 The submitted plans show that this can be provided within the bin store area within the car parking area and there is level access both internally and externally to the store. The refuse vehicle will access the site and collect the bins from the bins store. There is space

provided in the opening to the Phase 2 car park for turning a refuse vehicle and the surface will be of suitable specification for a refuse vehicle.

22. OTHER MATTERS

- 22.1 Objections received regarding the concern that the proposals have the potential to devalue the existing neighbouring properties or loss of earnings during the construction phase are not material to the determination of the application under planning legislation and case law.

23. CONCLUSION

- 23.1 At the heart of the NPPF is a presumption in favour of sustainable development, this requires planning applications that accord with the development plan to be approved without delay and where the development plan is absent, silent or out of date granting permission unless the adverse impacts of doing so would significantly and demonstrably outweigh the benefits when assessed against the policies in the framework as a whole or specific policies in the framework indicate that development should be restricted.
- 23.2 In this instance, the application site is designated for residential use under UDP Policy E2 (8) and as the redevelopment of the site would bring about a range of benefits including re-use of a prominent and redundant town centre/ conservation area site; visual amenity improvements; additional planting / ecological improvements; contribution to delivery of new homes; and additional expenditure in the local shops and services. There are no significant factors which outweigh these benefits and so the proposal is therefore recommended for approval subject to the conditions.

24. RECOMMENDATION

Subject to no objection being raised by the Environment Agency in respect of the revised remediation strategy and site investigation report, grant the Approval of Reserved Matters for the erection of 18 apartments with associated landscaping, open space, and car parking (scale, layout, appearance and landscaping) following outline consent granted under 17/00019/FUL (Parcel B) subject to the following conditions:

1. The development must be begun not later than the expiration of three years beginning with the date of this permission.
2. No development, other than work to undertake site clearance, remediation and ground engineering works, shall take until such time until samples and/or a full specification of materials to be used externally on the hereby-approved apartment block have been submitted to and approved in writing by the Local Planning Authority. Such details shall include the type, colour and texture of the materials including window frames and balcony detailing. The development shall be constructed in accordance with the approved details.
3. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 1995 (as amended by the Town and Country Planning (General Permitted Development) (Amendment) (No.2) (England) Order 2008 (or any Order revoking or re-enacting or amending that Order with or without modification), no television / radio aerial / satellite dish or other form of antenna shall be installed / affixed on the exterior of any building forming part of the development hereby permitted.
4. Prior to first occupation, precise details of the type, siting, design and materials of all boundary treatments shall be submitted to and approved in writing by the Local Planning Authority. The approved boundary treatments shall be fully completed in accordance with

the approved details prior to first occupation of any of the hereby approved residential units.

5. The approved scheme of landscaping shall be implemented before the development is first occupied or in accordance with a programme agreed previously with the local planning authority. Any newly planted trees or plants forming part of the approved scheme which, within a period of 5 years from the completion of the planting, are removed, damaged, destroyed or die shall be replaced in the next appropriate planting season with others of similar size and species by the developer unless the local planning authority gives written consent to any variation
6. No development, other than site clearance and site compound set up, shall commence until such time as the following information has been submitted in writing and written permission at each stage has been granted by the Local Planning Authority.
 - i) A preliminary risk assessment to determine the potential for the site to be contaminated shall be undertaken and approved by the Local Planning Authority. Prior to any physical site investigation, a methodology shall be approved by the Local Planning Authority. This shall include an assessment to determine the nature and extent of any contamination affecting the site and the potential for off-site migration.
 - ii) Where necessary a scheme of remediation to remove any unacceptable risk to human health, buildings and the environment (including controlled waters) shall be approved by the Local Planning Authority prior to implementation.
 - iii) Any additional or unforeseen contamination encountered during development shall be notified to the Local Planning Authority as soon as practicably possible and a remedial scheme to deal with this approved by the Local Planning Authority.
 - iv) Upon completion of any approved remediation schemes, and prior to occupation, a completion report demonstrating that the scheme has been appropriately implemented and the site is suitable for its intended end use shall be approved in writing by the Local Planning Authority.

The discharge of this planning condition will be given in writing by the Local Planning Authority on completion of the development and once all information specified within this condition and other requested information have been provided to the satisfaction of the Local Planning Authority and occupation/use of the development shall not commence until this time, unless otherwise agreed by the Local Planning Authority.

7. Prior to first occupation of any residential unit, the hereby approved car parking and circulation areas indicated on approved plan (insert planning ref) shall be fully constructed, drained, marked out and be available for use and thereafter kept unobstructed and available for its intended purpose.
8. Prior to occupation, the cycle parking indicated on the approved plan (4654-01_1002 rev A) shall be fully completed and be available for use and thereafter kept unobstructed and available for its intended purpose. The area shall be maintained and kept available for the parking of cycles at all times.
9. Prior to occupation, the refuse storage area indicated on the approved plan (4654-01_1002 rev A) shall be fully completed and be available for use and thereafter kept unobstructed and available for its intended purpose. The area shall be maintained and kept available for the storage of bins at all times.
10. Foul and surface water shall be drained on separate systems.
11. No development, other than site clearance, remediation and cut and fill operations, shall commence until a surface water drainage scheme, based on the hierarchy of drainage options in the National Planning Practice Guidance with evidence of an assessment of the site conditions, shall be submitted to and approved in writing by the Local Planning Authority. The surface water drainage scheme must be in accordance with the Non-

Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement national standards and unless otherwise agreed in writing by the Local Planning Authority, no surface water shall discharge to the public sewerage system either directly or indirectly. The development shall be completed in accordance with the approved drainage scheme.

12. Prior to occupation of the development a sustainable drainage management and maintenance plan for the lifetime of the development shall be submitted to the local planning authority and agreed in writing. The sustainable drainage management and maintenance plan shall include as a minimum:
 - A: Arrangements for adoption by an appropriate public body or statutory undertaker, or, management and maintenance by a resident's management company; and
 - B: Arrangements for inspection and ongoing maintenance of all elements of the sustainable drainage system to secure the operation of the surface water drainage scheme throughout its lifetime.The development shall be carried out in accordance with the approved details.
13. During demolition, ground engineering and construction periods, no works (including vehicle and plant movements, deliveries, loading and unloading) shall take place outside the hours of 07:30 and 18:00 Mondays to Fridays and 08:00 to 13:00 Saturdays. No work shall take place on Sundays and Bank Holidays.
14. The development hereby permitted shall be carried out in accordance with the following approved plans: 4654_1000, 4654_1001, 4654_1002, 4654_1003, 4654_1004, AP_C19236_02C_External works GA - surface water & foul drainage, 5751.05.002 Ecological Assessment V2, 16-0932, 11-493-r1-Rev A-Phase I Report and 11-493-r2-RevA-Phase II Site Investigation and Geo-Environmental Site Assessment, C19236_03A, SSL:17617A:200:1:1, 2016/59 V1 Archaeological Assessment, Planning Statement, Sustainability Statement and C19236 - Flood Risk Assessment - Rev A (11/01/17) and Addendum (23/2/17)

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Application Number: 17/01060/REM

Former Site Of 10 - 12 Castle Street Stalybridge

Photo 1 – View from Leech Street looking north across Castle Street towards the site



Photo 2 - View looking across River Tame looking south west towards the site and Pattern House



Photo 3 - View looking across the site towards the Engine House and Pattern House and showing the backs of properties on Market Street



Photo 4 - View looking south east across River Tame towards the site, showing relationship between Longlands Mill, Pattern House and the river



Notes
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Scale Bars TO BE USED ONLY AS GUIDANCE

1:100	mm	0	1000	2000	3000	4000	5000
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1:500	m	0	5	10	15	20	25
1:1250	m	0	10	20	30	40	50

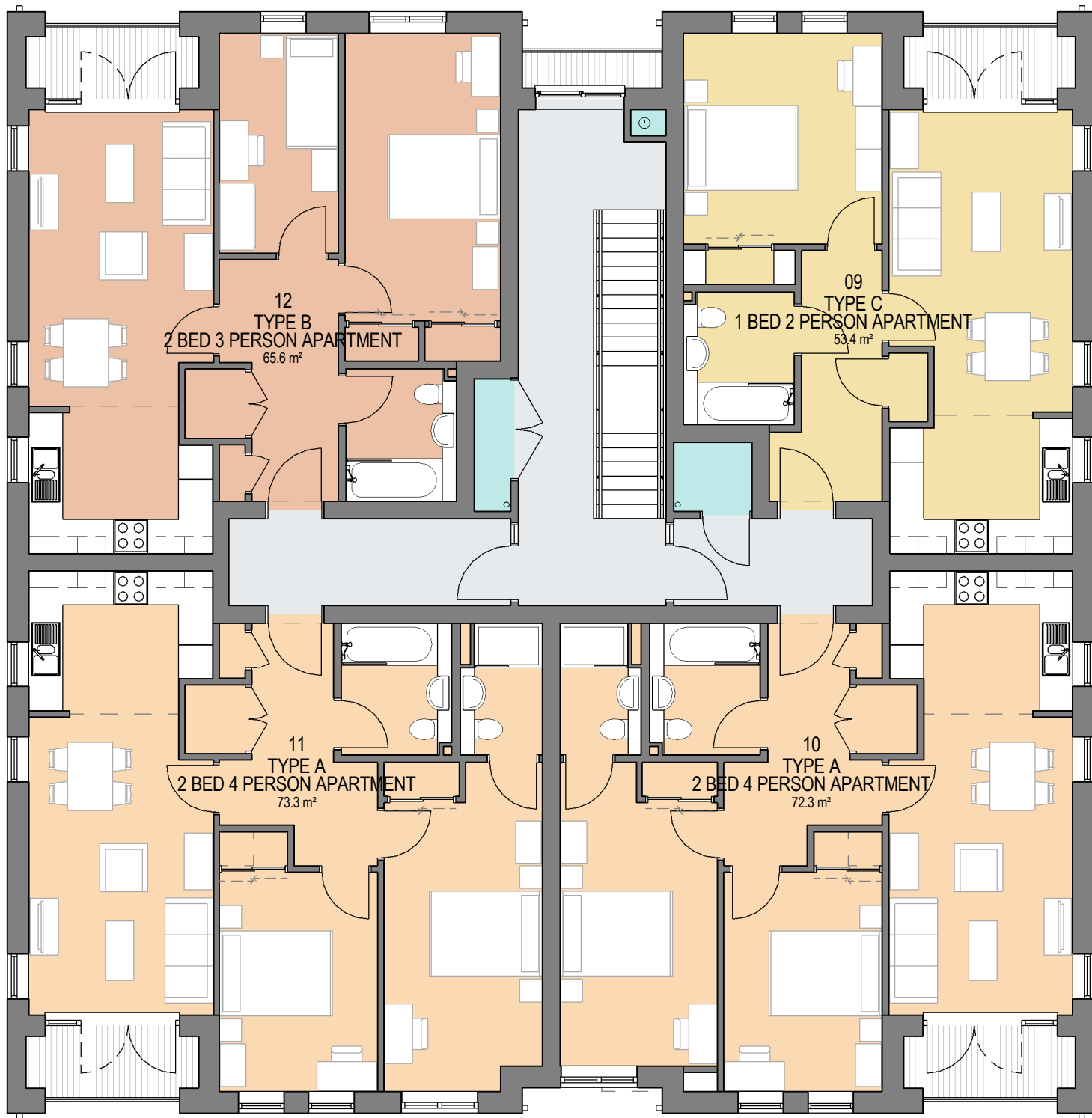
APARTMENT SUMMARY

Type	Count
1 BED 2 PERSON APARTMENT	3
2 BED 3 PERSON APARTMENT	3
2 BED 4 PERSON APARTMENT	6

AREA SCHEDULE (GIFA)

Level	Area
00 - Ground Floor	303.7 m ²
01 - First Floor	303.7 m ²
02 - Second Floor	303.7 m ²
Grand total	911.1 m ²

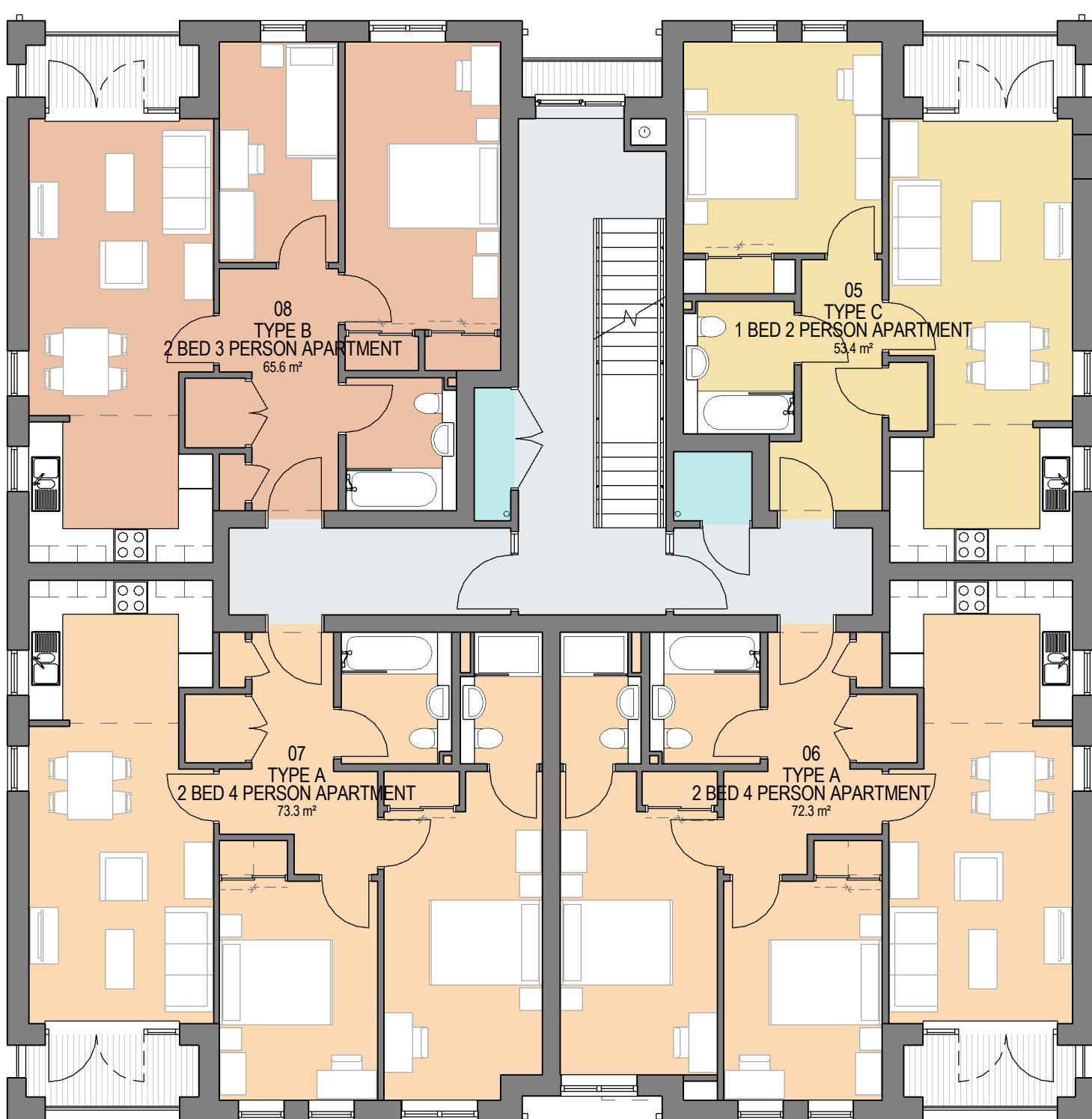
- MATERIALS (RESERVED MATTERS)**
- APARTMENTS:**
- Walls - Brick, multi red to match Phase 1. Areas of feature brickwork to be Flemish bond in the same red multi brick
 - Roof - Artificial slate effect tile
 - Windows & Patio Doors - UPVC, grey
 - Apartment entrance door - Aluminium, grey
 - Balcony balustrade - polyester powder coated metal guarding, grey



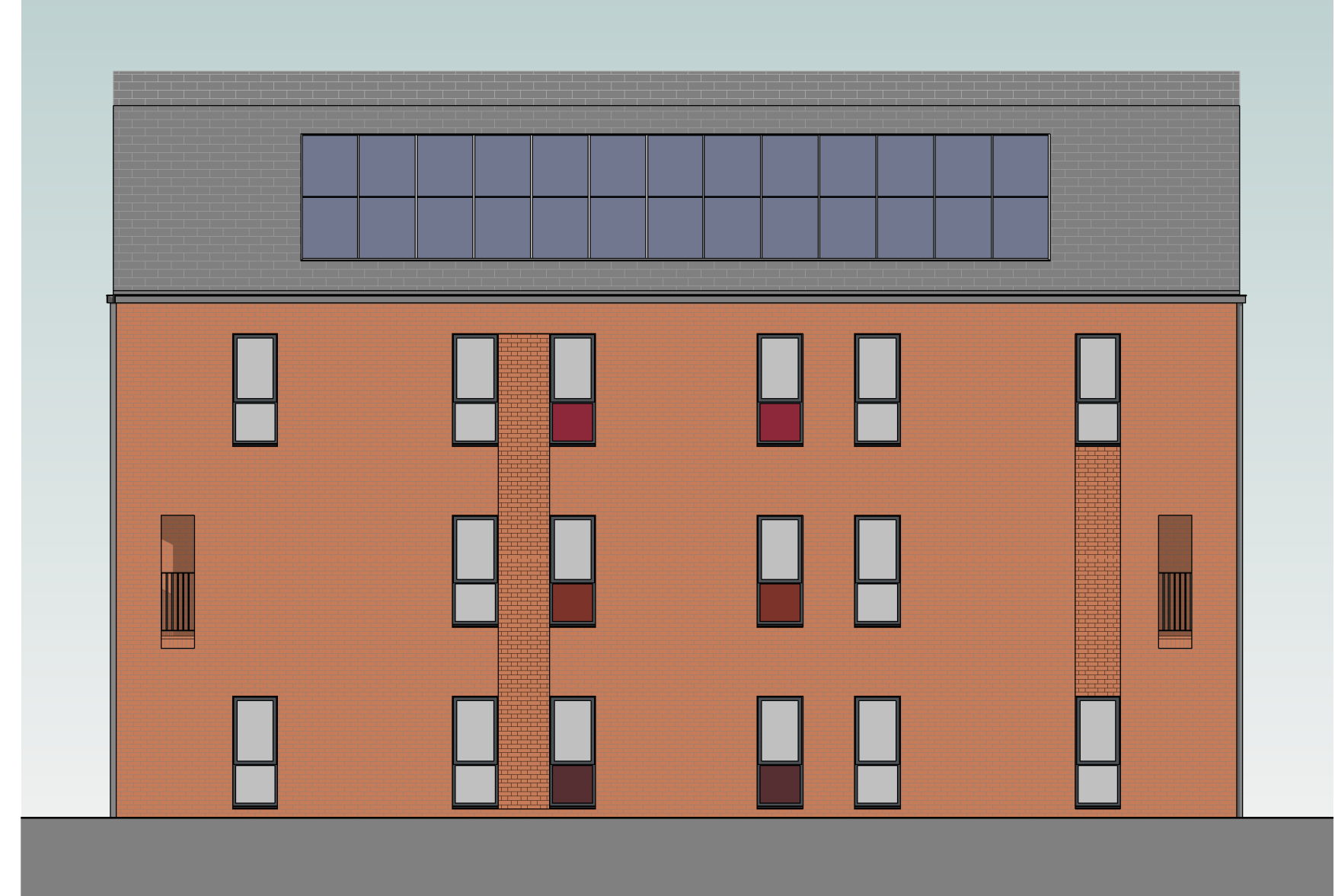
SECOND FLOOR PLAN
1: 100



STREET ELEVATION - CASTLE STREET
1: 100



FIRST FLOOR PLAN
1: 100



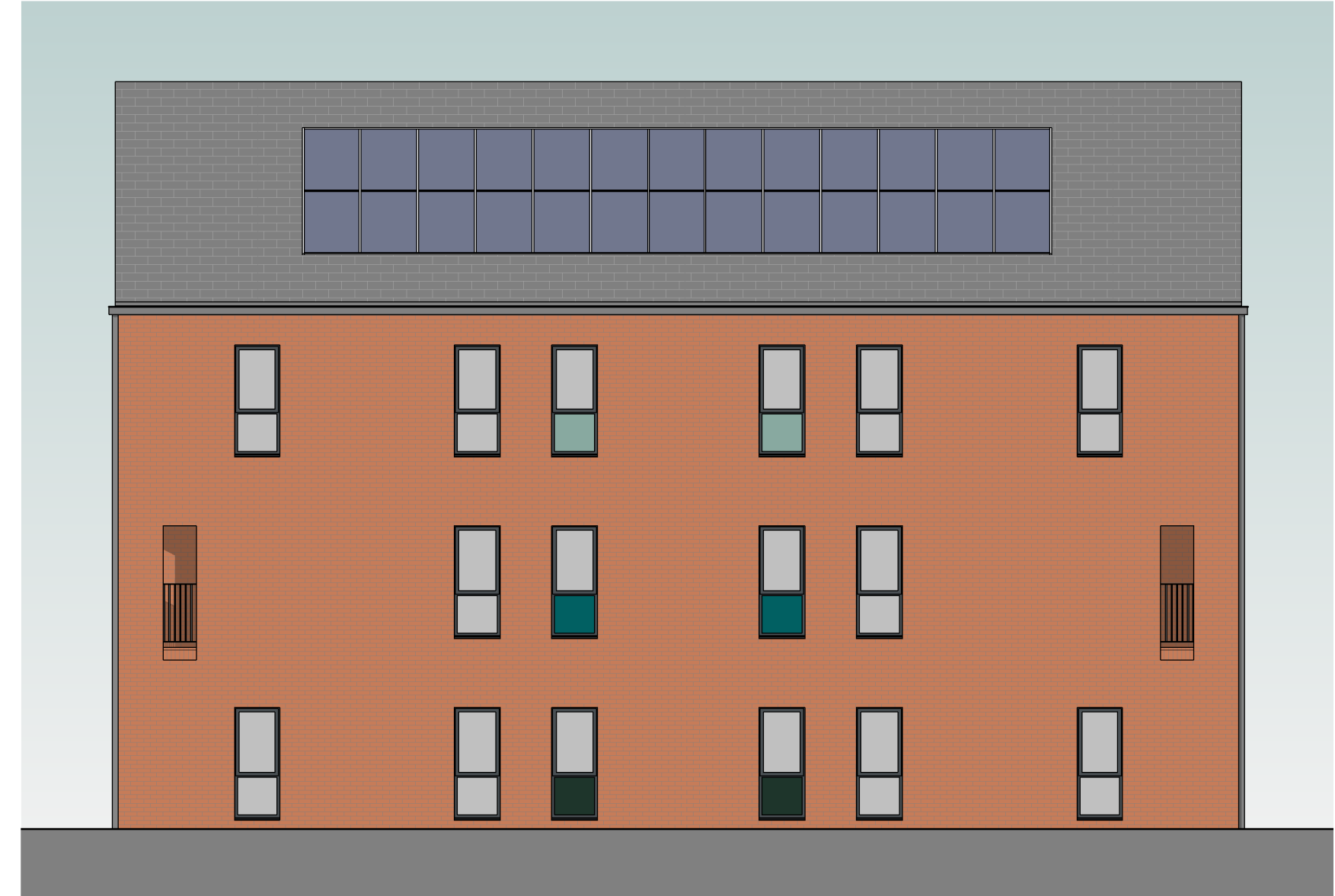
SIDE ELEVATION - FACING PHASE 1
1: 100



GROUND FLOOR PLAN
1: 100



FRONT ELEVATION - FACING COURTYARD PARKING
1: 100



SIDE ELEVATION - FACING THE PATTERN HOUSE
1: 100

Rev	Description	By	Date
1	MOSS CARE ST VINCENTS' HOUSING GROUP		
2	CASTLE STREET PHASE 2, STALYBRIDGE		
3	BLOCK B - FLOOR PLANS & ELEVATIONS		
4	4654-01_1004		
5	1004		
6	02/11/17	Scale	1: 100 @ A1
7	EMc	EMc	TW
8	Planning	Tender	Construction
9	Design	Contract	As Built



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1:500	m	0	10	20	30
1:1250	m	0	5	10	15

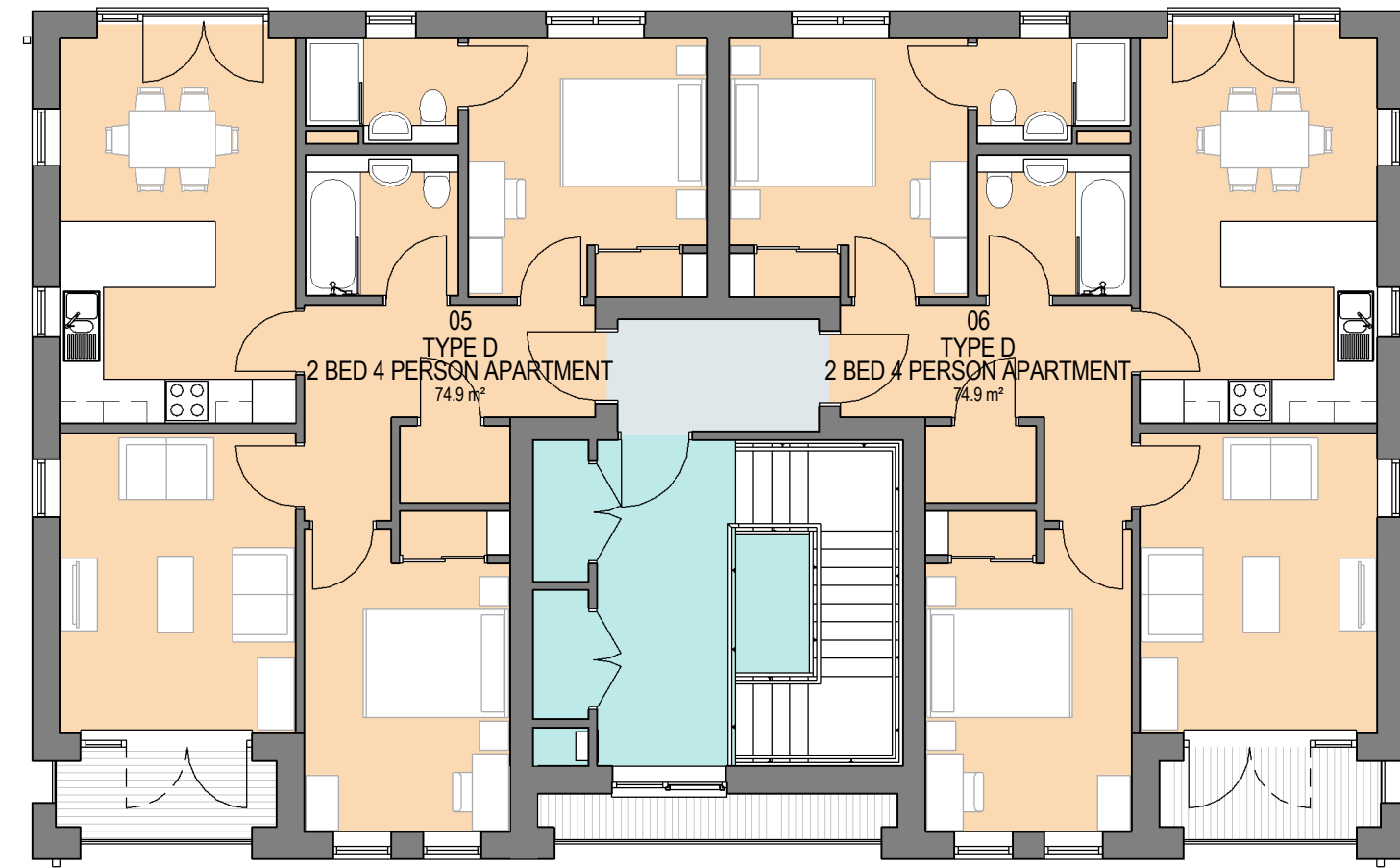
APARTMENT SUMMARY

Type	Count
2 BED 4 PERSON APARTMENT	6

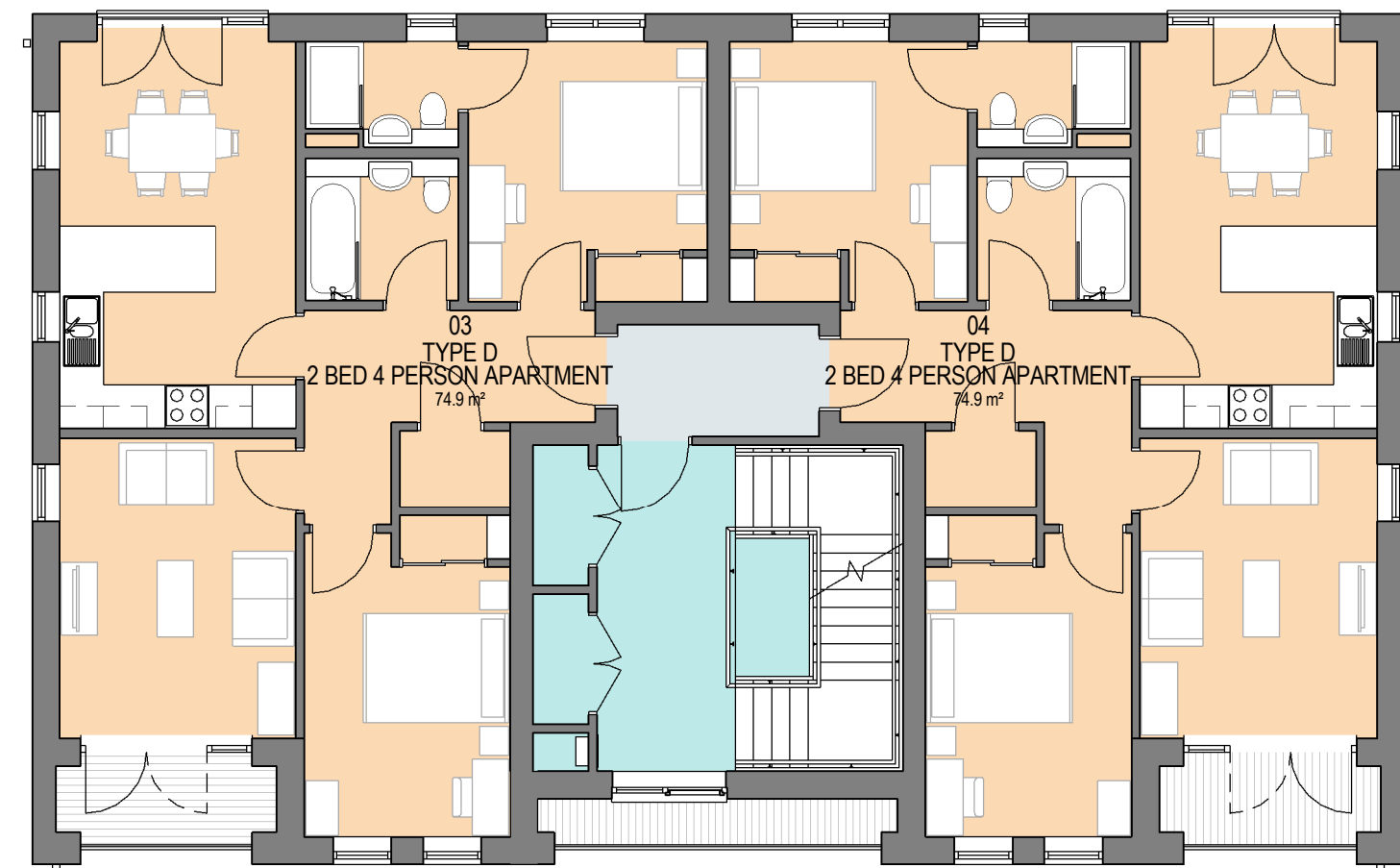
AREA SCHEDULE (GIFA)

Level	Area
00 - Ground Floor	184.0 m ²
01 - First Floor	184.0 m ²
02 - Second Floor	184.0 m ²
Grand total	551.9 m ²

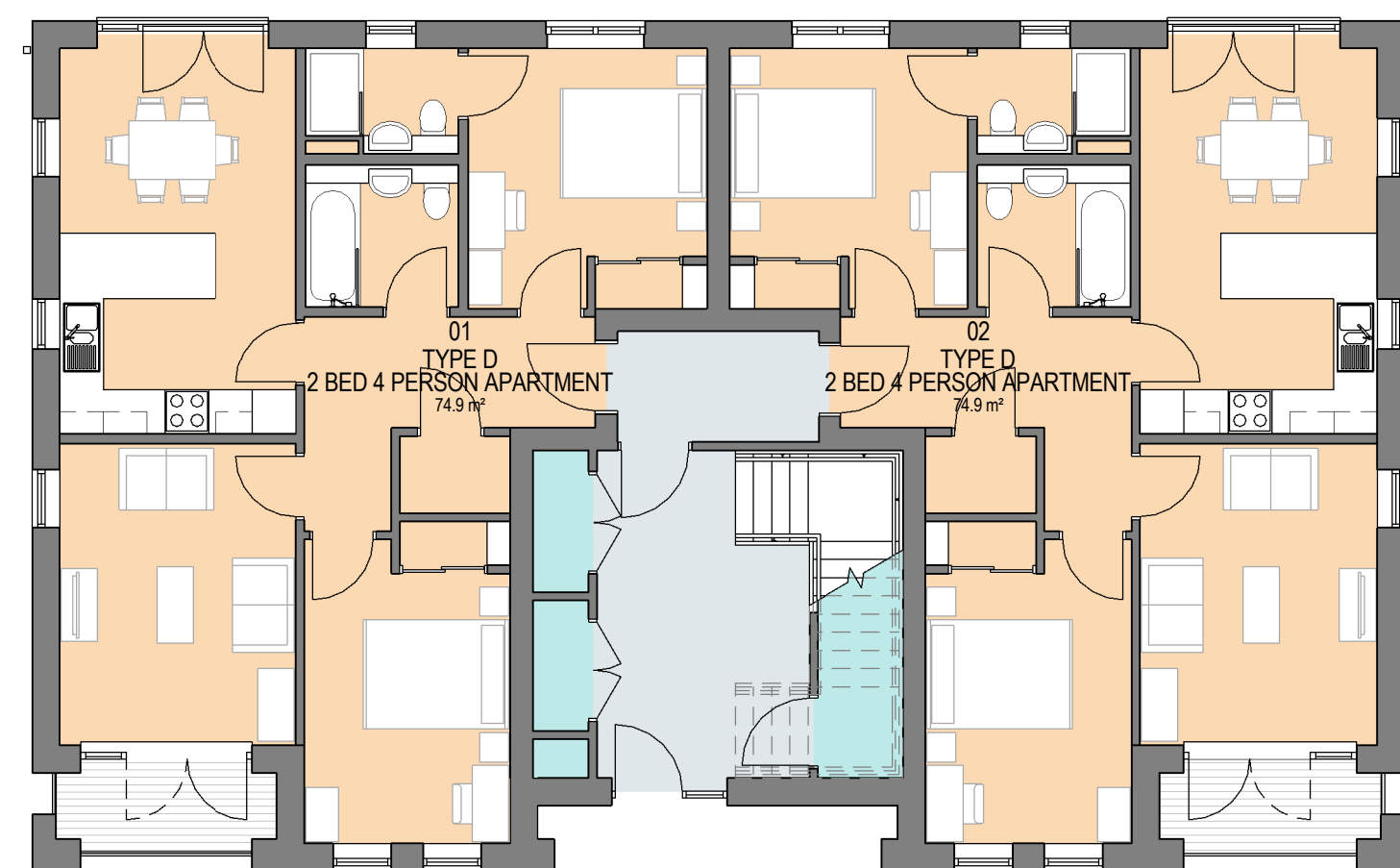
- MATERIALS (RESERVED MATTERS)**
- APARTMENTS:**
1. Walls – Brick, multi red to match Phase 1. Areas of feature brickwork to be fleemish bond in the same red multi brick
 2. Roof - Artificial slate effect tile
 3. Windows & Patio Doors - UPVC, grey
 4. Apartment entrance door - Aluminium, grey
 5. Balcony balustrade – polyester powder coated metal guarding, grey



SECOND FLOOR PLAN
1:100



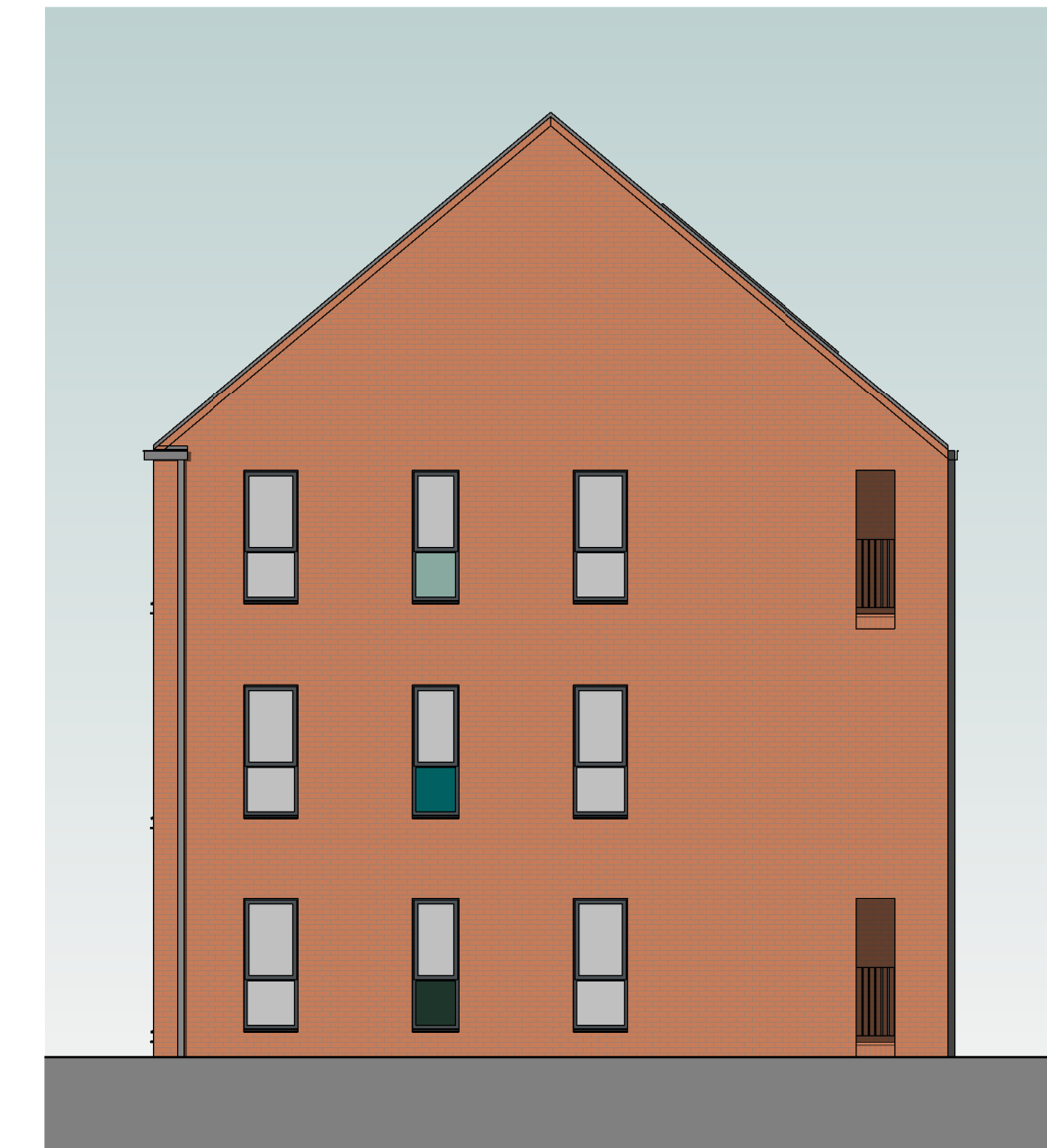
FIRST FLOOR PLAN
1:100



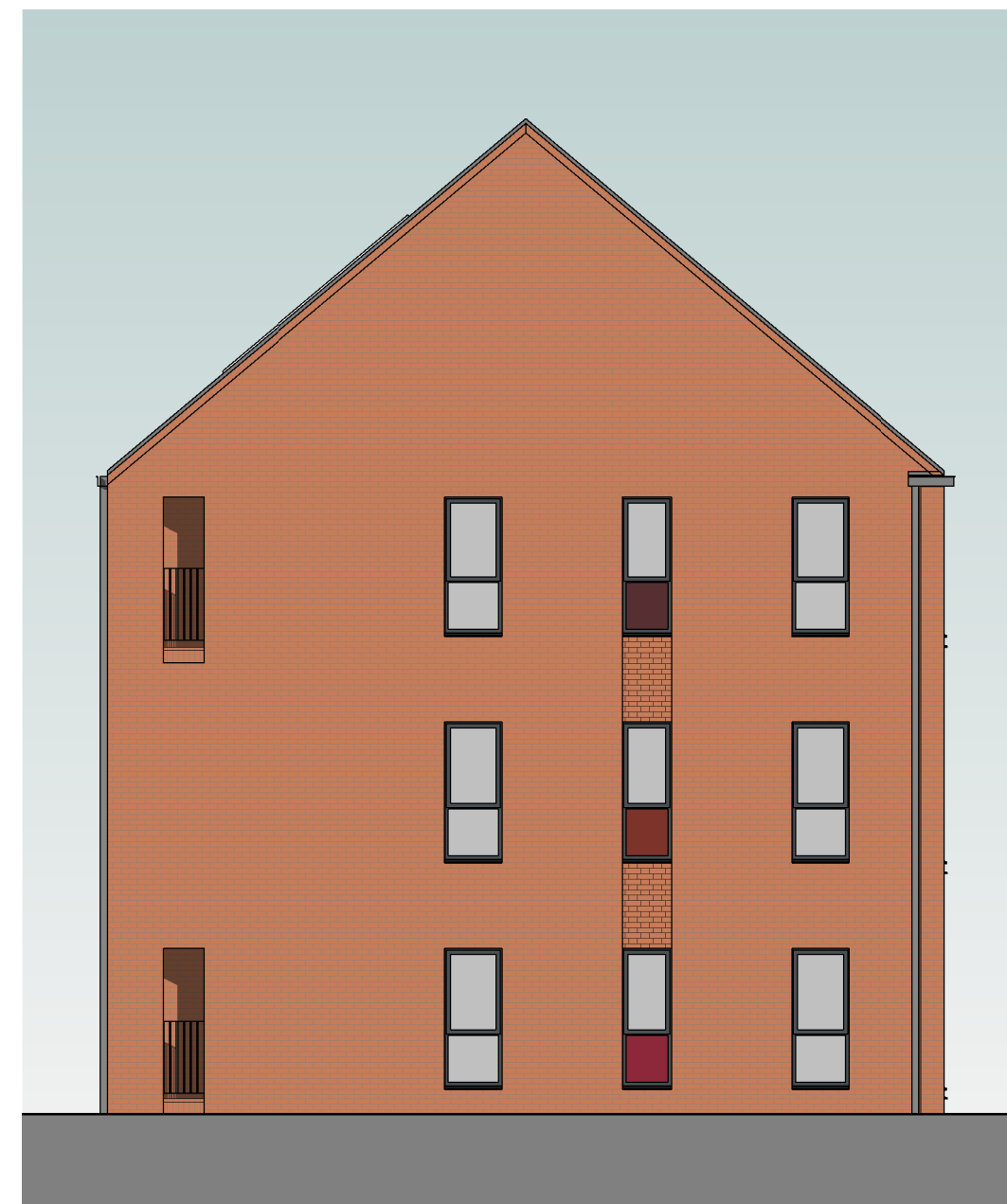
GROUND FLOOR PLAN
1:100



FRONT ELEVATION FACING COURTYARD PARKING
1:100



SIDE ELEVATION - FACING PATTERN HOUSE
1:100



SIDE ELEVATION - FACING PHASE 1
1:100



REAR ELEVATION - FACING THE RIVER TAME
1:100

Rev	Description	By	Date
01	MOSSCARE ST VINCENTS' HOUSING GROUP		
02	CASTLE STREET PHASE 2		
03	BLOCK C - FLOOR PLANS & ELEVATIONS		
04	4654-01_1005		
05	1005		
06	07/12/17	Scale	1:100 @ A1
07	EMc	EMc	TW
08	Planning	Tender	Construction
09	Design	Contract	As Built



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- Key**
- Asphalt concrete surface (Vehicular)
 - Pressed concrete paving slab surface 450mm x 450mm x 50mm (colour Grey)
 - Self binding gravel surface
 - Trellis
 - Tree planting
 - Hedgerow planting
 - Native shrub planting
 - Ornamental planting
 - Bulb planting
 - Amenity grassland
 - Species-rich grassland

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Rev	Description	Drawn	Approved	Date
B	Amenity grass mowing strips removed from footpath within buffer zone	SW	RJC	26.05.17
A	Amendment to paving slabs	SW	RJC	21.04.17

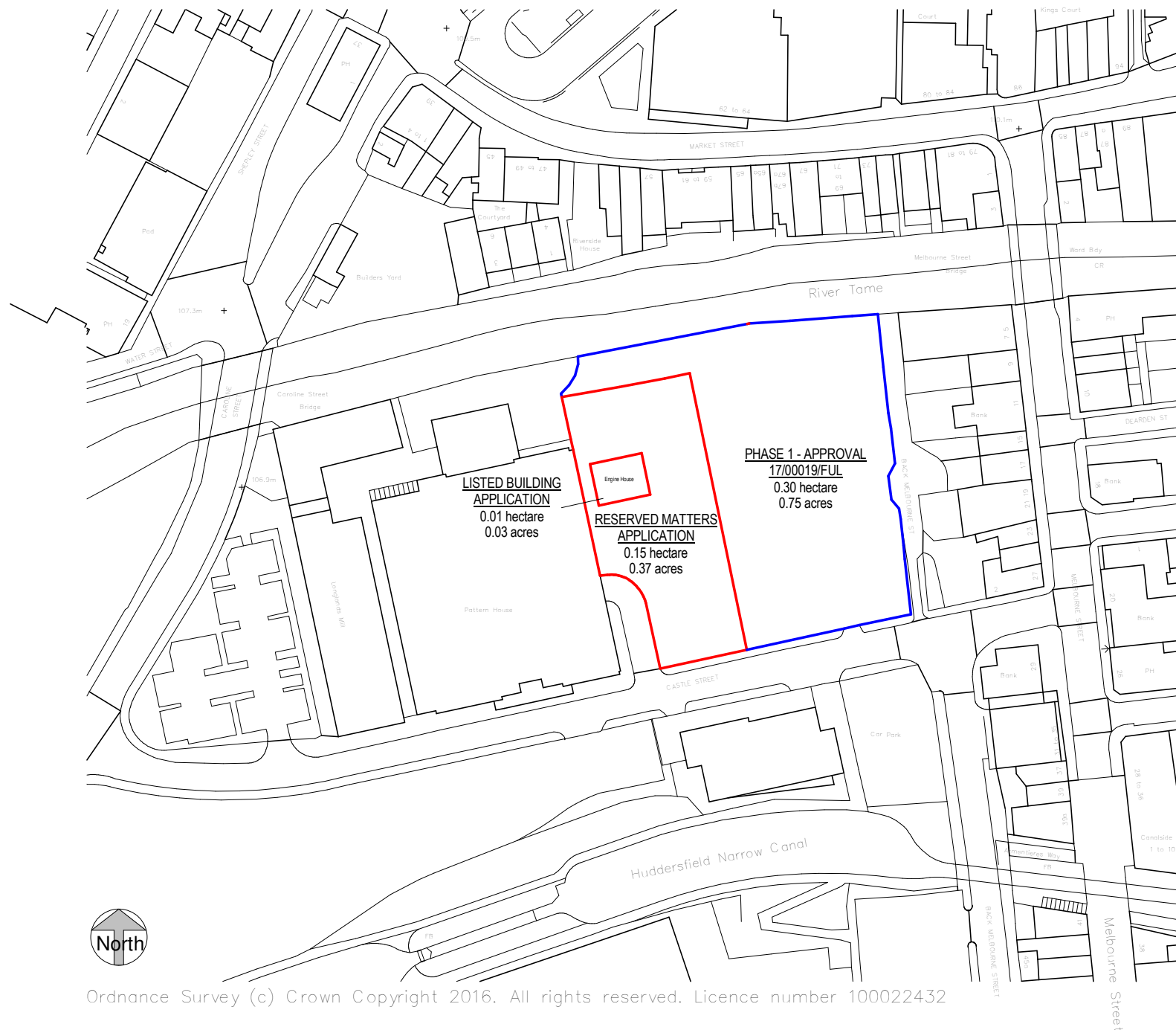
TEP THE ENVIRONMENT PARTNERSHIP
 Genesis Centre, Birchwood Science Park, Warrington WA3 7BH
 Tel 01925 844004 e-mail tep@tep.uk.com www.tep.uk.com

Project
Castle Street Stalybridge
 Title
Hard Landscape Plan

Drawing Number
D6325.001B

Scale 1:200@ A1	Date 27/03/2017
Drawn SW	Checked JTS
	Approved RJC

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SITE LOCATION PLAN - RESERVED MATTERS APPLICATION

1 : 1250

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1:200	mm	5000	8000		
1:500	M	5	15	25	
1:1250	M	10	30	50	70

KEY	
—	Site boundary
—	Ownership boundary (Phase 1 - 17/00019/FUL)

SITE AREAS	
Name	Area
RESERVED MATTERS APPLICATION	0.15 hectare
LISTED BUILDING APPLICATION	0.01 hectare
PHASE 1 - APPROVAL 17/00019/FUL	0.30 hectare

Rev	Description	By	Date
Client	MOSSCARE ST VINCENTS' HOUSING GROUP		
Job	CASTLE STREET PHASE 2, STALYBRIDGE		
Drawing	SITE LOCATION PLAN - RESERVED MATTERS		
BSRef	POZ Ref: 4654-01_1000	Rev	
Date	07/12/17	Scale	1 : 1250 @ A3
3D CoOrd	EMc	2D CoOrd	EMc Approved TW
Project Status	Planning	Tender	Construction
	Design	Contract	As Built



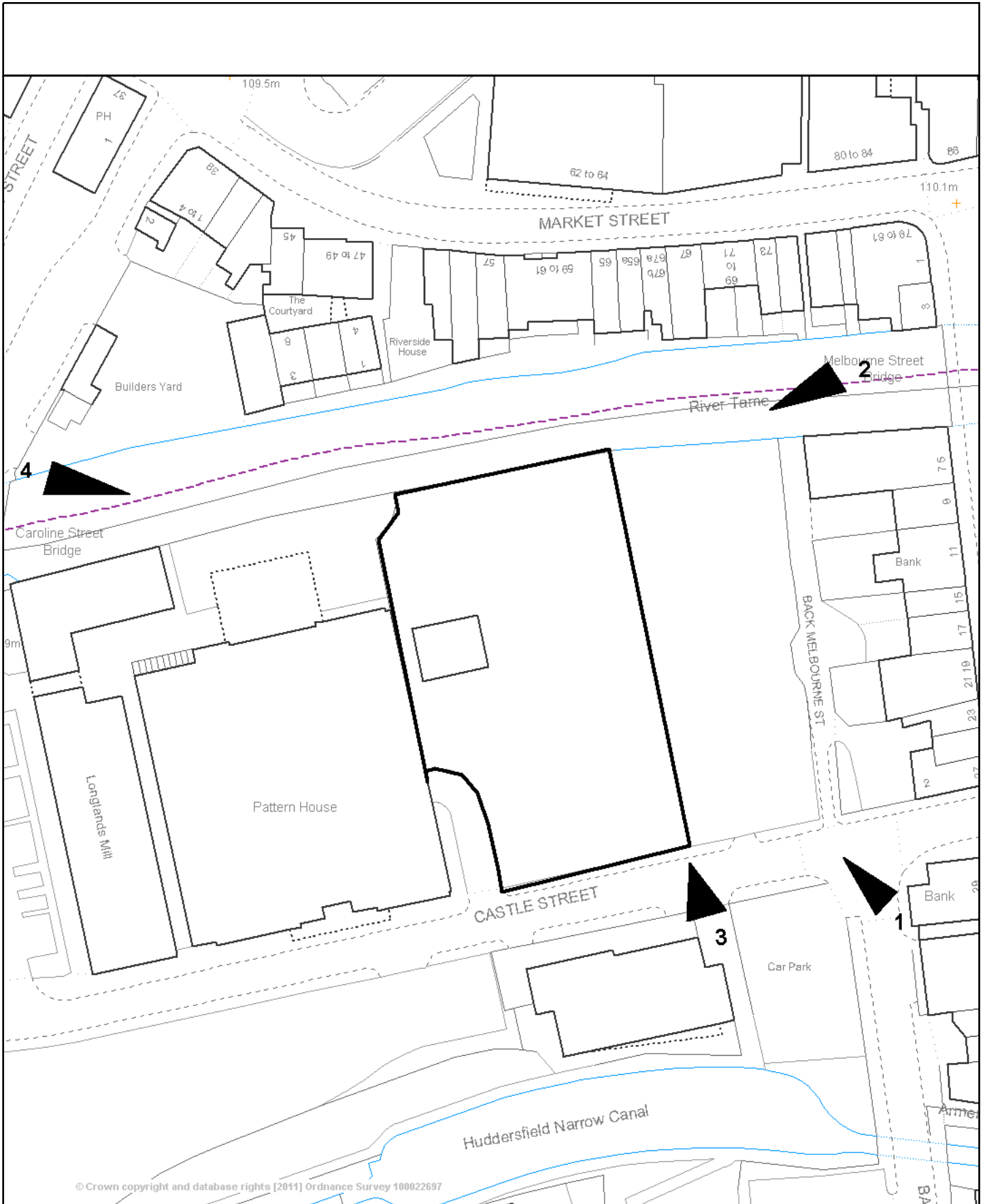
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Scale 1/900 Date 15/5/2018
 Centre = 396193 E 398490 N

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Soft Landscaping Programme	
Item	Date
	(Assumes completion of building works February/March 2018)
Implementation: Planting	During the external works (February/March 2018)
Defects planting: Year 1	February/March 2019
Defects planting: Year 2	February/March 2020
Defects planting: Year 3	February/March 2021

Plant Schedule												
Name	Qty	Height	Form	Girth	CrStm	Brks	Cntr	Ctrs	Rt Condition	Age	Mix Percentage	
Acer campestre 'Streetwise'	4	400-450cm	Standard (Extra heavy)	14-16cm	175-200cm			0.000	RB	3x	0.00	
Achillea filipendulena 'Gold Plate'	11	20-30cm					3L	0.450	C		0.00	
Alnus glutinosa	11	400-450cm	Standard (Extra heavy)	14-16cm	Min 200cm	5		0.000	RB	3x	0.00	
Betula pendula	4	400-450cm	Standard (Extra heavy)	14-16cm	min 200cm	5		0.000	RB	3x	0.00	
Carex flacca	35	30-40cm					3L	0.450	C		0.00	
Carex nigra	17	30-40cm					3L	0.450	C		0.00	
Caryopteris clandonensis	9	20-30cm					3L	0.450	C		0.00	
Ceanothus 'Autumnal Blue'	12	30-40cm				4	3L	0.580	C		0.00	
Choisya ternata	15	40-60cm				5	5L	0.580	C		0.00	
Clematis montana	2	60-80cm				2	3L	1.500	C		0.00	
Deschampsia cespitosa	26	30-40cm					5L	0.450	C		0.00	
Escallonia 'Crimson Spire'	18	40-60cm				5	5L	0.450	C		0.00	
Euonymus fortunei 'Emerald Gaiety'	33	25-30cm	Bushy			9	6L	0.450	C		0.00	
Fuchsia magellanica	11	30-40cm				5	3L	0.450	C		0.00	
Geranium macrorrhizum	11	20-30cm					5L	0.450	C		0.00	
Hebe 'Great Orme'	26	30-40cm	Bushy			5	5L	0.450	C		0.00	
Hebe pinguifolia 'Page'	10	30-40cm				7	5L	0.450	C		0.00	
Hedera helix	2	40-50cm				2	3L	1.500	C		0.00	
Ilex aquifolium	97	60-80cm					5L	0.200	C		0.00	
Lavandula angustifolia	62	30-40cm				5	5L	0.450	C		0.00	
Malus 'Rudolf'	2	425-600cm	Standard (Extra heavy)	14-16cm	175-200			0.000	RB	3x	0.00	
Miscanthus sinensis 'Morning Light'	10	30-40cm					5L	0.450	C		0.00	
Miscanthus sinensis 'Variegatus'	7	40-60cm					5L	0.450	C		0.00	
Pachysandra terminalis	30	15-20cm (D)				9	3L	0.450	C		0.00	
Panicaria bistorta	5	20-30cm					3L	0.450	C		0.00	
Pittosporum tenuifolium 'Tom Thumb'	18	30-40cm				7	10L	0.450	C	1+1	0.00	
Rosmarinus officinalis	27	40-60cm				5	5L	0.450	C		0.00	
Sarcococca confusa	27	30-40cm				6	5L	0.450	C		0.00	
Sedum spectabile	3	20-30cm					5L	0.450	C		0.00	
Sorbus aucuparia 'Cardinal Royal'	1	425-600cm	Standard (Extra heavy)	14-16cm	175-200cm			0.000	RB	3x	0.00	
Stachys lanata	5	20-30cm					5L	0.450	C		0.00	
Stipa arundinacea	6	15-20cm					5L	0.450	C		0.00	
Vinca minor	25	20-30cm (D)				3	3L	0.450	C		0.00	

Native Bulb Mix												
Name	Qty	Height	Form	Girth	CrStm	Brks	Cntr	Ctrs	Rt Condition	Age	Mix Percentage	
Crocus nudiflorus	251							0.224			20.00	
Galanthus nivalis (Single)	376							0.224			30.00	
Narcissus pseudonarcissus	376	5+ (Topsize)						0.224			30.00	
Ornithogalum angustifolium	251	6+cm (Topsize)						0.224			20.00	
	1836											

Native Shrub Mix												
Name	Qty	Height	Form	Girth	CrStm	Brks	Cntr	Ctrs	Rt Condition	Age	Mix Percentage	
Cornus sanguinea	23	60-80cm	Seedling (undercut)					1.000	B	1u/1	15.00	
Corylus avellana	16	60-80cm	Transplant - cutting raised			4		1.000	B	0/1/1	10.00	
Crataegus monogyna	39	60-80cm	Seedling - undercut					1.000	B	1u/1	25.00	
Rosa canina	16	60-80cm				2		1.000	B	1+1 or 1/1	10.00	
Salix caprea	23	60-80cm				3		1.000	B	1+2	15.00	
Salix cinerea	23	40-60cm				5	3L	1.000	C		15.00	
Sambucus nigra	16	60-80cm				3		1.000	B		10.00	
	156											
	1992											

Species Rich Grassland Mix

Supplier: Germinal Seeds Ltd
 Mix: Traditional Hay Meadow (MG5 Grassland)
 Sowing rate: 5.00 g/m2

- 1.5% Achillea millefolium (Yarrow)
- 2% Anthoxanthum odoratum (Sweet Vernal Grass)
- 1.5% Centaurea nigra (Common Knopweed)
- 1.5% Galium verum (Lady's Bedstraw)
- 1% Leontodon hispidus (Rough Hawkbit)
- 1.5% Leucanthemum vulgare (Ox-eye Daisy)
- 0.5% Lotus corniculatus (Birdsfoot Trefoil)
- 2% Plantago lanceolata (Ribwort Plantain)
- 1% Agrimonia eupatorium (Agrimony)
- 1.5% Prunella vulgaris (Self-Heal)
- 1.5% Ranunculus acris (Meadow Buttercup)
- 1.5% Ranunculus bulbosus (Bulbous Buttercup)
- 1% Rhinanthus minor (Yellow Rattle)
- 1.5% Filipendula ulmaria (Meadow Sweet)
- 0.5% Sanguisorba minor (Salad Burnet)
- 14% Agrostis capillaris (Common Bent)
- 18% Cynosurus cristatus (Crested Dogstail)
- 14% Dactylis glomerata (Cocksfoot)
- 22% Festuca rubra ssp litoralis (Slender Creeping Red Fescue)
- 6% Festuca pratensis (Meadow Fescue)
- 6% Trisetum flavescens (Yellow Oat-Grass)

Amenity Grassland (open space areas)

Supplier: Germinal Seeds Ltd
 Mix: Low Maintenance Areas
 Sowing rate: 50.00 g/m2

- 40% CORAIL (Strong Creeping Red Fescue)
- 30% RAISA (Chewings Fescue)
- 25% ABERFLEECE (Sheeps Fescue)
- 5% HIGHLAND (Browntop Bent)

Key

- Trellis
- Tree planting
- Hedgerow planting
- Native shrub planting
- Ornamental planting
- Bulb planting
- Amenity grassland
- Species-rich grassland
- Climbing Plant
- 2FR Schwegler Bat Tube (Interlinking) (3nr.)
- Schwegler 1MF Bat and bird box (2nr.)
- Schwegler sparrow terrace (1nr.)
- Woodpecker/Starling Nest Box (2nr.)

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Rev	Description	Drawn	Approved	Date
B	Amenity grass mowing strips removed from footpaths within buffer zone	SW	RJC	26.05.17
A	Planting timetable added	SW	RJC	25.04.17

THE ENVIRONMENT PARTNERSHIP
 Genesis Centre, Birchwood Science Park, Warrington WA3 7BH
 Tel 01925 844004 e-mail tep@tep.uk.com www.tep.uk.com

Project
Castle Street Stalybridge
 Title
Detailed Planting and Habitat Creation Plan
 Drawing Number
D6325.002B

Scale	Date
1:200 @ A1	28/03/2017
Drawn WYC	Checked SW Approved JTS



Schwegler sparrow terrace



2FR Schwegler Bat Tube (Interlinking)



Schwegler 1MF Bat and bird box



Woodpecker/Starling Nest Box

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Notes
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Scale Bars TO BE USED ONLY AS GUIDANCE

1:100	mm	0	1000	2000	3000	4000	5000
1:200	mm	0	500	1000	1500	2000	2500
1:500	m	0	5	10	15	20	25
1:1250	m	0	10	20	30	40	50



- MATERIALS (RESERVED MATTERS)**
- APARTMENTS:**
1. Walls – Brick, multi red to match Phase 1. Areas of feature brickwork to be Flemish bond in the same red multi brick
 2. Roof - Artificial slate effect tile
 3. Windows & Patio Doors - UPVC, grey
 4. Apartment entrance door - Aluminium, grey
 5. Balcony balustrade – polyester powder coated metal guarding, grey
- HARD LANDSCAPING & BOUNDARIES:**
6. Car Park and Parking bays - Macadam, black
 7. Pathways within the site – Resin bonded gravel on a tarmac base, golden in colour for the main pathways and grey for the shadow effect within the engine house garden
 8. Railings - Polyester powder coated metal, black
 9. Bin store - treated timber clad structure with metal roof



STREET ELEVATION - FACING CASTLE STREET
 1 : 200



SIDE ELEVATION - FACING BLOCK A
 1 : 200



SIDE ELEVATION FACING THE PATTERN HOUSE
 1 : 200



REAR ELEVATION FACING THE RIVER TAME
 1 : 200



FRONT ELEVATION - BLOCK B
 1 : 200



FRONT ELEVATION - BLOCK C
 1 : 200

Rev	Description	By	Date
Client	MOSSCARE ST VINCENTS' HOUSING GROUP		
Job	CASTLE STREET PHASE 2, STALYBRIDGE		
Drawn	PROPOSED ELEVATIONS		
Drawn No.	4654-01_1006	Rev	
Scale	1:1006		
Date	07/12/17	Scale	As indicated
Drawn	EMc	Approved	TW
Project	Planning	Tender	Construction
	Design	Contract	As Built
pozzoni			
London House 115 Golden Lane London EC1Y 0TJ		Woodville House 2 Woodville Road Altrincham WA14 2FH	
+44 20 7078 0079 architects@pozzoni.co.uk		+44 161 928 7848 www.pozzoni.co.uk	

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1:200	mm	0	1000	2000	3000	4000	5000
1:500	m	0	10	20	30	40	50
1:1250	m	0	10	20	30	40	50

SCHEME SUMMARY

BLOCK A (PHASE 1 - APPROVED):

21 x 1 Bedroom Apartments
17 x 2 Bedroom Apartments
TOTAL = 38 Apartments

BLOCK B (PHASE 2 - RESERVED MATTERS):

3 x 1 Bedroom Apartments
9 x 2 Bedroom Apartments
TOTAL = 12 Apartments

BLOCK C (PHASE 2 - RESERVED MATTERS):

6 x 2 Bedroom Apartments
TOTAL = 6 Apartments

PARKING PROVISION

Block A - 17 spaces
Blocks B & C - 15 spaces

CYCLE STORAGE

Shared covered store for 20 bicycles

MATERIALS (RESERVED MATTERS)

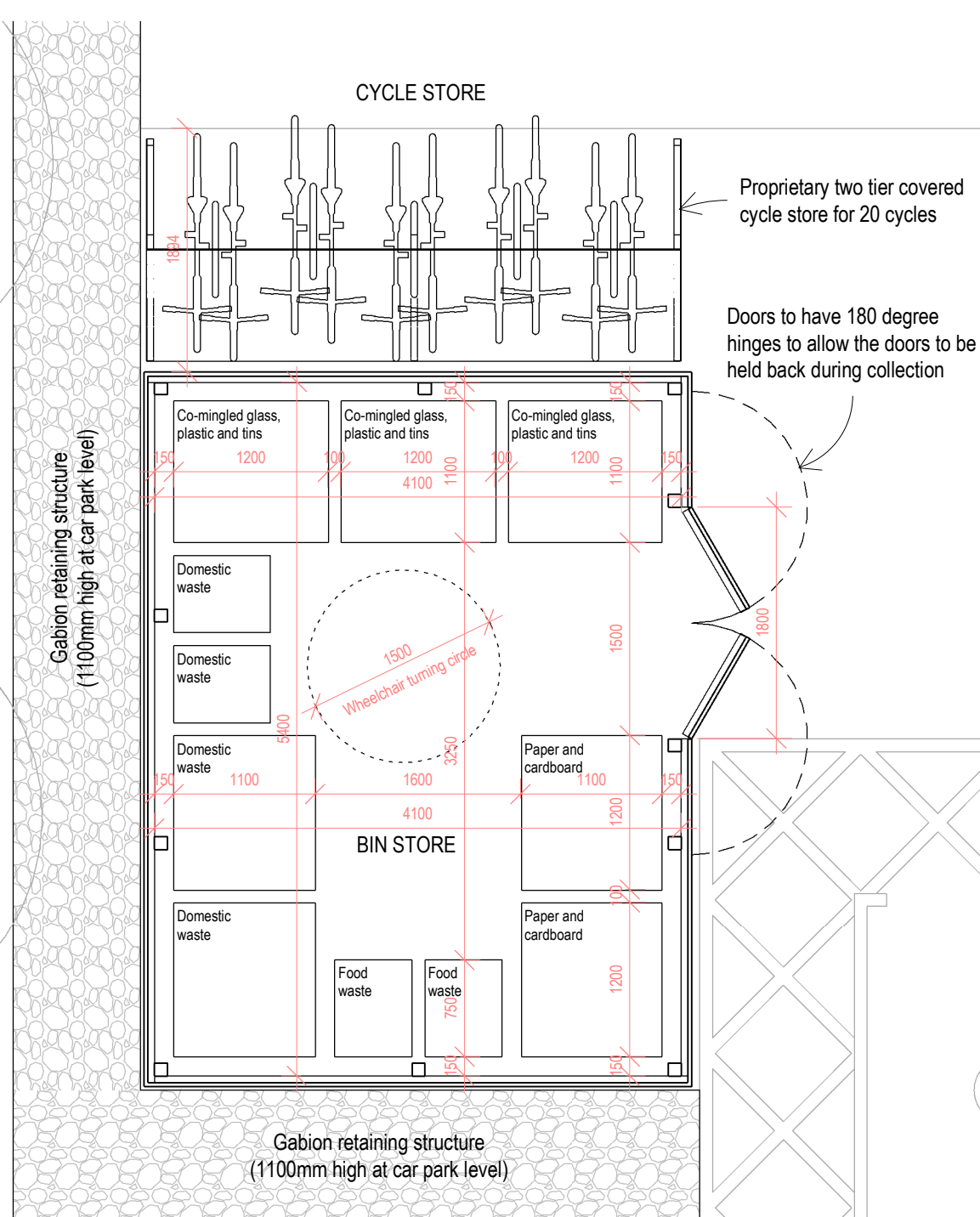
APARTMENTS:

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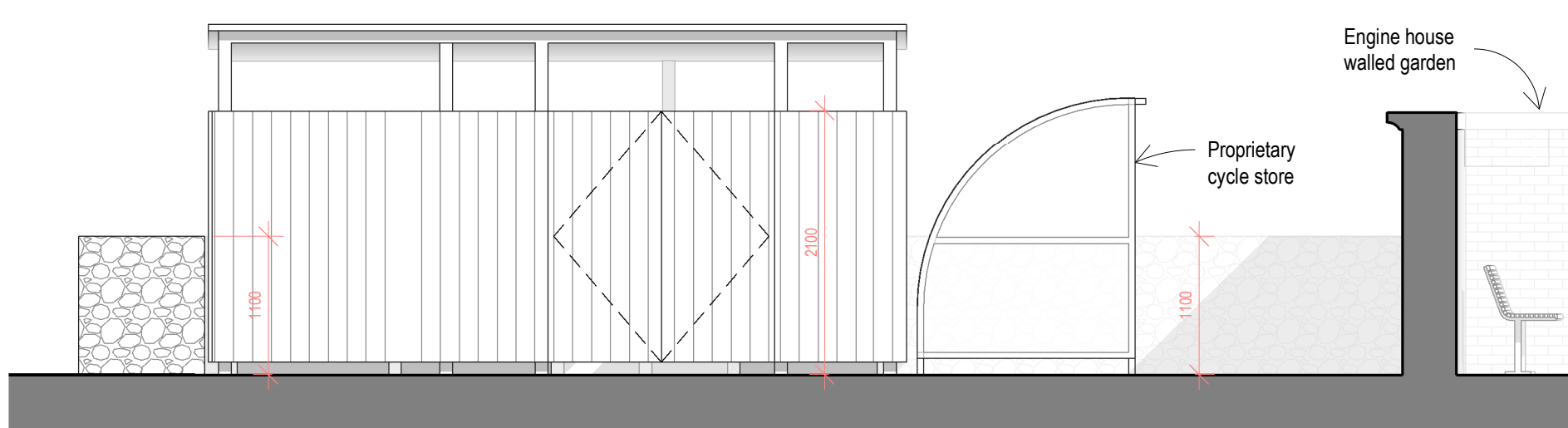
HARD LANDSCAPING & BOUNDARIES:

6. Car Park and Parking bays - Macadam, black
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8. Railings - Polyester powder coated metal, black
9. Bin store - treated timber clad structure with metal roof

PROPOSED SITE LAYOUT PLAN
1 : 200



BOUNDARY TYPE A ELEVATION
1 : 50



BIN & CYCLE STORE ELEVATION
1 : 50

Rev	Description	EMc	By	Date
A	Cycle storage provision clarified	EMc		10/05/18

Client: MOSSCARE ST VINCENTS' HOUSING GROUP
 Job: CASTLE STREET PHASE 2, STALYBRIDGE
 Drawing: PROPOSED SITE PLAN
 Ref: 4654-01_1002
 Scale: As indicated
 Date: 07/12/17
 Approved: TW
 Planning: EMc
 Design: EMc
 Tender: EMc
 Contract: TW
 Construction: TW
 As Built: TW

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1:200	mm	0 1000 2000	0 5000 10000
1:500	m	0 10 20	0 25 50
1:1250	m	0 10 20	0 20 40

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VIEW FROM CASTLE STREET, WEST OF THE SITE



VIEW FROM THE CAROLINE STREET BRIDGE



AERIAL VIEW FROM THE SOUTH WEST



AERIAL VIEW FROM THE SOUTH EAST



VIEW INTO THE SITE FROM THE NORTH



VIEW FROM THE ENGINE HOUSE GARDEN



VIEW FROM THE CAR PARK

Rev	Description	By	Date
Client	MOSSCARE ST VINCENTS' HOUSING GROUP		
Use	CASTLE STREET PHASE 2, STALYBRIDGE		
Drawn	PROPOSED STREET & AERIAL VIEWS		
Project No.	4654-01_1007	Rev	
Scale	1:200	Sheet	@ A1
Date	07/12/17	Scale	1:200
Drawn	EMc	Scale	EMc
Approved	TW	Approved	TW
Phase	Planning	Tender	Construction
Design	Contract	As Built	

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Application Number 18/00102/FUL

Proposal Proposed new one-bedroom bungalow, garden and detached outbuilding.

Site Land off Winton Avenue, Audenshaw

Applicant Mr James Ward

Recommendation Grant planning permission, subject to the completion of a Section 106 agreement, and subject to conditions

Reason for report A Speakers Panel decision is required because, in accordance with the Council's Constitution the application has been called-in by councillor Ryan and includes the requirement of a Section 106 legal agreement.

REPORT

1. APPLICATION DESCRIPTION

- 1.1 The application seeks full planning permission for the development of a detached bungalow with a detached, outbuilding in the garden at the rear.
- 1.2 The bungalow would be brick-built and contain an open-plan kitchen/dining/sitting room, a bedroom and bathroom. A single parking space would be provided in front of the bungalow.
- 1.3 Outline planning permission (ref. 15/00240/OUT) was granted in May 2015 for a 2-bedroom bungalow with a separate double garage on the site. The site has since been subject to a topographic survey, including a trace of a public sewer and culvert within the site. An accurate location of these drainage features required an adjustment of the layout of the proposed development, including the reduction of what was an initially-proposed double garage to a detached outbuilding and an amendment to the footprint of the proposed bungalow.
- 1.4 The proposal includes the diversion of the culvert from a point outside the site. If the application is approved it is recommended that the works to divert the culvert be secured by means of a Section 106 legal agreement.

2. SITE AND SURROUNDINGS

- 2.1 The site is located at the northern end of Winton Avenue, which is a no-through road, before the embankment up to the M60 motorway and railway. The land is unkempt, overgrown and disused. A metal palisade fence running along the foot of the embankment forms the site's northern boundary. Winton Avenue forms the site's eastern boundary. A passageway behind houses in Spring Bank Avenue forms the site's southern boundary whilst the disused land continues beyond the site's western boundary along the foot of the adjacent embankment. The site is enclosed to the east, south and west by metal, chain-link fences and is not publically accessible.
- 2.2 The surroundings are predominantly residential and characterised by well-spaced, two-storey terraced houses.

3 PLANNING HISTORY

- 3.1 05/00505/FUL - Erection of 3 no. 1 bed apartments, five parking spaces, external bin store and open space - Refused in June /2005. The reasons for refusal related to too great a coverage of hard areas, overlooking and loss of amenity to neighbouring residents, and traffic generation.
- 3.2 05/00507/FUL - Erection of two semi-detached dwelling houses - Refused in June 2005. The reasons for refusal related to the development having an unduly overbearing visual appearance, traffic generation and possible obstruction of the culvert.
- 3.3 14/01095/OUT - Erection of single domestic garages and open space – Outline permission granted, with conditions, in January 2015.
- 3.4 15/00240/OUT - Erection of single storey 2 bedroom bungalow with separate double garage – Outline permission, granted, with conditions, in May 2015

4. RELEVANT PLANNING POLICIES

- 4.1 National Planning Policy Framework (NPPF)
- 4.2 Planning Practice Guidance (PPG)
- 4.3 **Tameside Unitary Development Plan (UDP) Allocation**
Unallocated
- 4.4 **Part 1 Policies**
 - 1.3: Creating a Cleaner and Greener Environment.
 - 1.4: Providing More Choice and Quality Homes.
 - 1.5: Following the Principles of Sustainable Development
 - 1.10 Protecting and Enhancing the Natural Environment
 - 1.12: Ensuring an Accessible, Safe and Healthy Environment
- 4.5 **Part 2 Policies**
 - H2: Unallocated Sites.
 - H4: Type, size and affordability of dwellings
 - H10: Detailed Design of Housing Developments
 - OL4: Protected Green Space
 - T1: Highway Improvement and Traffic Management.
 - T10: Parking
 - C1: Townscape and Urban Form
 - N3: Nature Conservation Factors
 - MW11: Contaminated Land
- 4.6 **Other Policies**
 - Residential Design Supplementary Planning Document
- 4.7 **National Planning Policy Framework (NPPF)**
 - Section 1 Delivering sustainable development
 - Section 4. Promoting sustainable transport
 - Section 6 Delivering a wide choice of high quality homes
 - Section 7 Requiring good design
 - Section 8 Promoting healthy communities
 - Section 11. Conserving and enhancing the natural environment

4.8 Planning Practice Guidance (PPG)

This is intended to complement the NPPF and to provide a single resource for planning guidance, whilst rationalising and streamlining the material. Almost all previous planning Circulars and advice notes have been cancelled. Specific reference will be made to the PPG or other national advice in the Analysis section of the report, where appropriate.

5. PUBLICITY CARRIED OUT

- 5.1 In accordance with the requirements of the Town and Country Planning (Development Management Procedure) (England) Order 2015 and the Council's adopted Statement of Community Involvement neighbour notification letters were issued on 27th March 2018 to 15 addresses in Winton Avenue and Spring Bank Avenue.

6. RESPONSES FROM CONSULTEES

- 6.1 Head of Environmental Services (Public Protection): Raises no objections to the proposed development subject to the imposition of conditions limiting the hours of works and deliveries during the construction process, requiring a scheme for protecting the development from externally generated noise from the M60 motorway and the railway and requiring the submission and approval of an assessment into potential sources of contamination and a remediation strategy.
- 6.2 Head of Environmental Services (Highways): Raises no objections to the proposed development subject to the imposition of conditions requiring the provision of the parking space, a highways conditions survey and the culvert diversion works.
- 6.3 Borough Arboriculturalist: Raises no objections to the proposals there being no significant existing vegetation on the site and trees on the adjacent railway embankment should not be affected.
- 6.4 Highways England: Raises no objection to the proposed development.
- 6.5 United Utilities: Identifies an easement crossing the site but raises no objections to the proposed development subject to the imposition of conditions regarding drainage and requiring a Construction Risk Assessment.
- 6.6 Councillors - A representation offering objection to the proposals has been received from Councillor Ryan on behalf of local residents and requested for a determination at Speakers Panel. Concerns raised are the size of the plot being inappropriate, the bungalow being too close to existing houses in Spring Bank Avenue and Winton Avenue, the access to the garage being inappropriate and unreasonable and the change to the character of the Spring Bank Avenue alleyway.

7. SUMMARY OF THIRD PARTY RESPONSES RECEIVED

- 7.1 Six letters of objection have been received from neighbouring residents, raising the following concerns (summarised):
- the development would have a negative impact on property values;
 - loss of existing privacy and over-looking from existing houses in Spring Bank Avenue which are at a higher level;
 - loss of outlook from, and light to, the rear of houses in Spring Bank Avenue;

- the development being prejudicial to the safety of users of the alleyway due to vehicles
- possible damage to the culverted watercourse causing a flood risk
- disruption during the period of construction;
- damage to existing trees on the embankment
- loss of the wildlife habitat the site provides;
- access to the rear of houses in Spring Bank Avenue being restricted, including for emergency vehicles; and
- the traffic generated exacerbating parking problems and restricting the ability of vehicles to manoeuvre and turn in Winton Avenue which is a no-through road.

7.2 Concern is expressed also about local residents not being notified about the sale of the land.

8. ANALYSIS

8.1 The issue to be assessed in the determination of this planning application are:

- 1) The principle of development
- 2) The impact on the character of the surrounding area;
- 3) The impact upon the residential amenity of neighbouring properties;
- 4) The impact on highway safety
- 5) The impact on ecology and trees

9. PRINCIPLE OF DEVELOPMENT

9.1 The most recent of the previous planning permissions (ref. 15/00240/OUT) expired on 8th May 2018 and so carries significant weight in determining whether the principle of the development is acceptable. Nevertheless, although historic maps indicate that the site once accommodated houses all evidence of these has disappeared and the land is open and so must be considered against the policies of Section 8 of the NPPF and UDP policy OL4 and whether built development is acceptable on the area open space.

9.2 The contribution of the application site as open space to the community is mainly as a buffer between houses and the motorway and railway rather than it serving a functional purpose. The retention of the site solely for this purpose for residents immediately adjacent would represent the inefficient use of land within the urban area and would not comply with the Core Principles of NPPF. Having no specific recreational or amenity value, and there having been no material changes in circumstances since the grant of the previous permission to indicate otherwise, it is considered that the principle of development on this site remains acceptable and compliant with paragraph 77 of the NPPF and policies H2 and OL4 of the UDP.

10. CHARACTER OF THE SURROUNDING AREA

10.1 Again, the previous planning permission carries significant weight in determining whether the impact on the character of the surrounding area is acceptable. Whilst the area is characterised by well-spaced terraced houses the proposed bungalow would be located at the end of the no-through road and largely hidden from view from surrounding roads by the

existing houses in Spring Bank Avenue. Being brick-built with a tiled roof in keeping with the vernacular, as previously, it is considered that in this location the presence of the proposed bungalow would not result in an adverse impact on the character of the site or the surrounding area and so compliant with policies 1.3, 1.4, H4, H10 (a) and C1 of the UDP.

11. RESIDENTIAL AMENITY

- 11.1 At the nearest point there would be a distance of more than 16m between the front wall of the proposed bungalow and windows in the front of existing houses in Winton Street and a distance of approximately 18.5m between the sole habitable window in the front of the bungalow and these neighbouring houses. There would be a distance of approximately 10m between the side of the bungalow, where there would be no windows, and the rear of existing houses in Spring Bank Avenue.
- 11.2 In order to prevent undue over-looking or over-shadowing of neighbouring properties the council's normal policy is that a distance of at least 14m be maintained between habitable room windows in houses on street frontages. A distance of 10m is required normally between habitable room windows and a single-storey wall containing no windows.
- 11.3 In this arrangement it is considered that the layout of the proposed development would not have an adverse impact on the amenity of neighbouring residents, in terms of over-looking or over-shadowing and the proposal would accord with policy H10 (d) of the UDP, policy RD5 of the SPD.

12 HIGHWAY SAFETY

- 12.1 The proposed scheme includes one car parking space. According to the NPPF: Development should only be prevented or refused on transport grounds where the residual cumulative impacts of development are severe. In this case, neither the local highway authority nor Highways England have raised any objections to the amended proposals.
- 12.2 Whilst the concern of local residents is noted, and there would be an increase in traffic on the local highway network, this is a residential area where regular car movements occur and are expected. Additional traffic movements during peak periods would not be of a volume sufficient to result in a severe adverse impact on the capacity of the highway network and so the scheme is considered acceptable and compliant with policies H10 (b), T1 and T10 of the UDP and Section 4 of the NPPF.
- 12.3 In this instance, and in the absence of any objection by the Head of Environmental Services – Highways, it is considered that the impact of the loss car parking space would not be severe and so acceptable.

13. ECOLOGY AND TREES

- 13.1 The scrub on the site has the potential to support nesting birds. All birds, with the exception of certain pest species, and their nests are protected under the terms of the Wildlife and Countryside Act 1981 (as amended). It is therefore recommended that no vegetation clearance required to facilitate the scheme should take place during the optimum period for bird nesting (March to July inclusive), and that this requirement be secured by condition of any permission.
- 13.2 The Greater Manchester Local Record Centre hold no records for badgers within the locality of the proposed development. Badgers are however known to use railway embankments and are mobile in their habits. It is therefore recommended that if any large

holes are discovered during scrub clearance or at any other time during the works the applicant's attention be drawn to the need to cease work immediately and seek advice from a suitably qualified ecologist.

- 13.3 It is recommended also that that the applicant be made aware of the laws which are in place to protect biodiversity and that, if necessary, measures be taken to eradicate the invasive plant Himalayan Balsam from the site. Under the terms of the wildlife and Countryside Act 1981 it is an offence to cause this plant to spread. The development has potential to cause spread.
- 13.4 The council's Arboricultural Officer has confirmed that there are no trees of any significant amenity value on the site and that trees on the adjacent embankment should not be affected by the development.
- 13.5 Given the attachment of appropriate conditions to any permission it is considered that the impact of the development on the ecological value of the site, and on any trees, would be acceptable and compliant with policy N3 of the UDP and Section 11 of the NPPF.

14. OTHER ISSUES

- 14.1 The concerns expressed by local residents about any impact on house prices, and publicity of the sale of the land, are not a material consideration in the determination of the application and must not influence the decision.
- 14.2 A condition restricting the hours of construction, as suggested by the Head of Environmental Services (Public Protection), is considered inappropriate because the avoidance of nuisance, which is the object of the condition, would be to require compliance with other regulatory requirements. Another condition, suggested by Head of Environmental Services (Highways) is considered inappropriate also because again this would be to require compliance with other regulatory requirements.
- 14.3 The necessary diversion of the culvert can be secured via a Section 106 agreement and so the potential issue of flooding be avoided.

15. CONCLUSION

- 15.1 The principle of development on the site is considered to be acceptable given the predominantly residential character of the surrounding area and that the layout and scale of the development would not result in an unreasonable impact on the residential amenity of neighbouring properties.
- 15.2 The level of parking provision is considered to be acceptable and there are no objections to the proposal from consultees in relation to highway safety and the impact on trees and ecology.
- 15.3 The application is therefore considered to accord with the relevant national and local planning policies listed earlier in this report.

RECOMMENDATION

Grant planning permission, subject to the completion of a Section 106 Agreement to secure the necessary culvert diversion works and the following conditions:

1. The development must be begun not later than the expiration of three years beginning with the date of this permission.
2.

1068-01	Location Plan
1067 02	Existing Site Plan
1067 03 A	Proposed Site Plan
1067 04 A	Existing and Proposed Site Sections
1067-05 A	Proposed Plans and Elevations
3. Notwithstanding any description of materials in the application, no above ground construction works shall take place until samples and/or full specification of materials to be used: externally on the building; in the construction of all boundary walls and fences; and, in the finishes to all external hard-surfaces have been submitted to, and approved in writing by, the local planning authority. Such details shall include the type, colour and texture of the materials. Development shall be carried out in accordance with the approved details.
4. Foul and surface water shall be drained on separate systems.

Prior to the commencement of any development, a surface water drainage scheme, based on the hierarchy of drainage options in the National Planning Practice Guidance with evidence of an assessment of the site conditions shall be submitted to and approved in writing by the local planning authority.

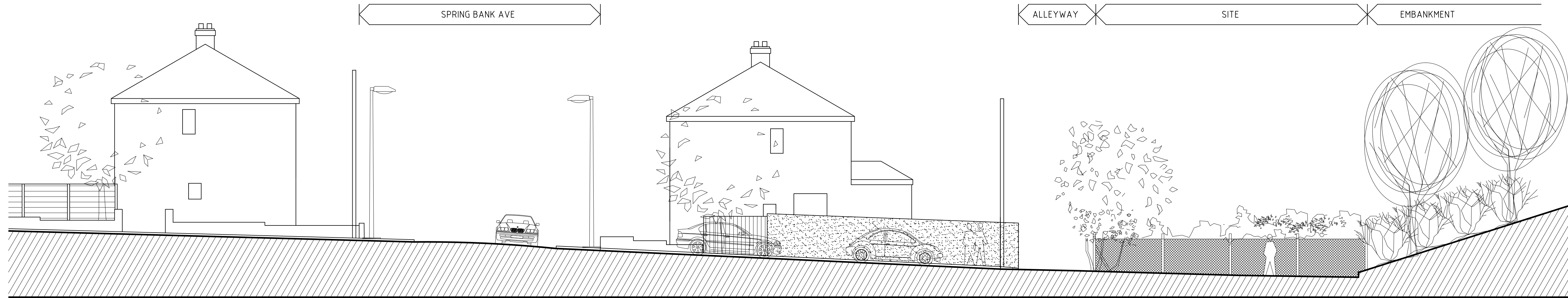
The surface water drainage scheme must be in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement national standards and unless otherwise agreed in writing by the local planning authority, no surface water shall discharge to the public sewerage system either directly or indirectly.

The development shall be completed in accordance with the approved details.
5. All boundary treatments shall be installed in accordance with the details as shown on the approved proposed site plan (drawing number 1067 03 A) and drawing number 169/120, Rev. P prior to the first occupation of any of the dwellings hereby approved.
6. The car parking spaces to serve the development hereby approved shall be laid out as shown on the approved proposed site plan (drawing number 1067 03 A) and the Existing and Proposed Site Sections plan ((drawing number 1067 04 A), prior to the first occupation of the bungalow hereby approved and shall be retained at all times thereafter.
7. No tree felling or vegetation removal shall take place during the optimum period for bird nesting (March to July inclusive) unless otherwise agreed in writing with the local planning authority.
8. No development shall take place unless and until a scheme for protecting the development from externally generated noise from the M60 motorway and the railway has been submitted to, and approved in writing by, the local planning authority and such approved works (which may include additional soundproofing and acoustic glazing and ventilation) shall be completed before the first occupation of the bungalow hereby approved.
9. No works other than the excavation of the foundations and/or piling works for the development shall be undertaken at the site until the CLS2A Contaminated Land Screening Form has been submitted to, and approved in writing by, the Councils

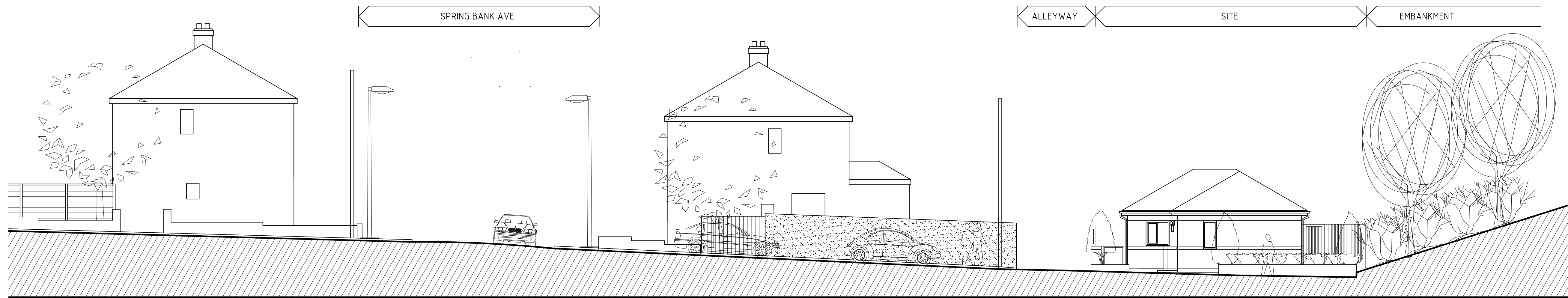
Environmental Protection Unit (EPU). Where necessary, a scheme to deal with any contamination / potential contamination shall be submitted to, and approved in writing by the EPU. The scheme shall be appropriately implemented and a completion report demonstrating this and that the site is suitable for its intended use will be approved in writing by the EPU prior to occupation. The discharge of this planning condition will be given in writing by the Local Planning Authority (LPA) on completion of the development and once all information specified in this condition has been provided to the satisfaction of the EPU.

10. During construction no work (including vehicle and plant movements, deliveries, loading and unloading) shall take place outside the hours of 07:30 and 18:00 Mondays to Fridays and 08:00 to 13:00 Saturdays. No work shall take place on Sundays and Bank Holidays.
11. No development shall take place until a Construction Risk Assessment Method Statement (RAMS) for construction of the proposed development, is submitted to and approved by the local planning authority. The statement shall outline the potential impacts from all construction activities on infrastructure that crosses the site and identify mitigation measures to protect and prevent any damage to this infrastructure. The development shall be undertaken in accordance with the approved RAM.

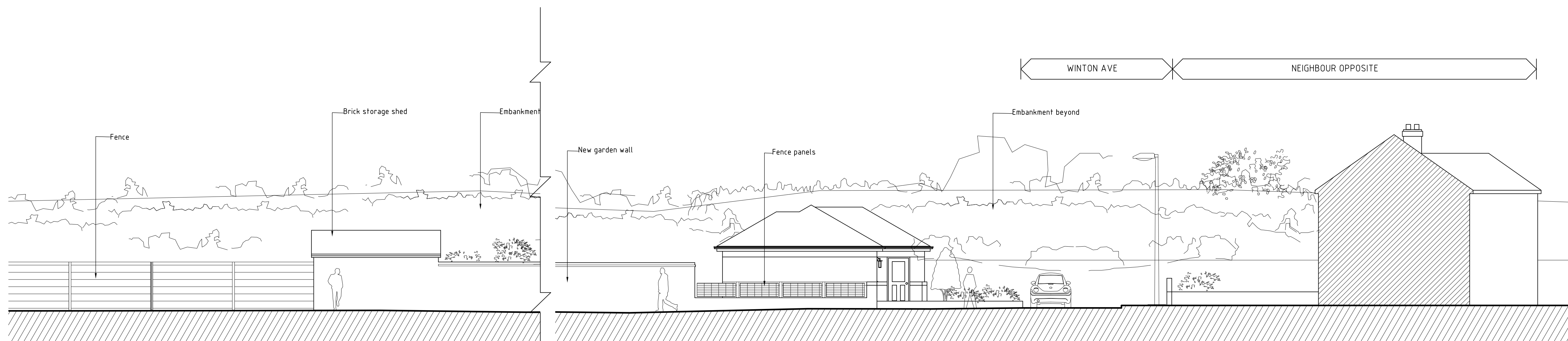
NOTES:



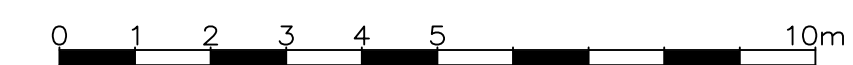
SITE SECTION A - A [EXISTING]



SITE SECTION A - A [PROPOSED]



SITE SECTION B - B [PROPOSED]



Rev	Date	Details Of Issue	Initials
A	12/04/18	Garage removed. Garden store added.	SE

Status

PLANNING

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Client	Mr James Ward 16 Appleton St, Awdenshaw		
Project	Proposed New Bungalow Land off Winton Avenue, Awdenshaw		
Title	Existing and Proposed Site Sections		

Drawn	Date:	Scale (at A1)
SE	26/01/18	1/100
Dwg no.	Revision No.	
1067 04	A	

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Application Number: 18/00102/FUL

Photo 1



Photo 2



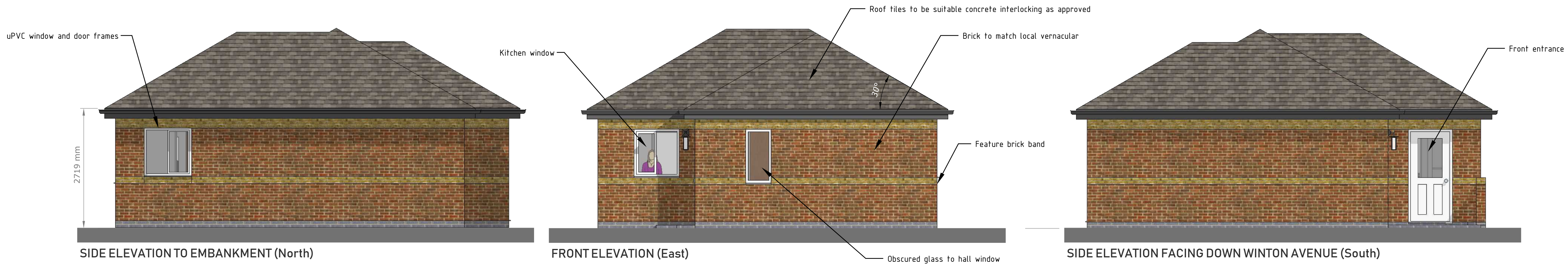
Photo 3



Photo 4



NOTES:



GARDEN STORE

Rev	Date	Details Of Issue	Initials
A	12/04/18	Garage removed. Garden store added	SE

Status

PLANNING



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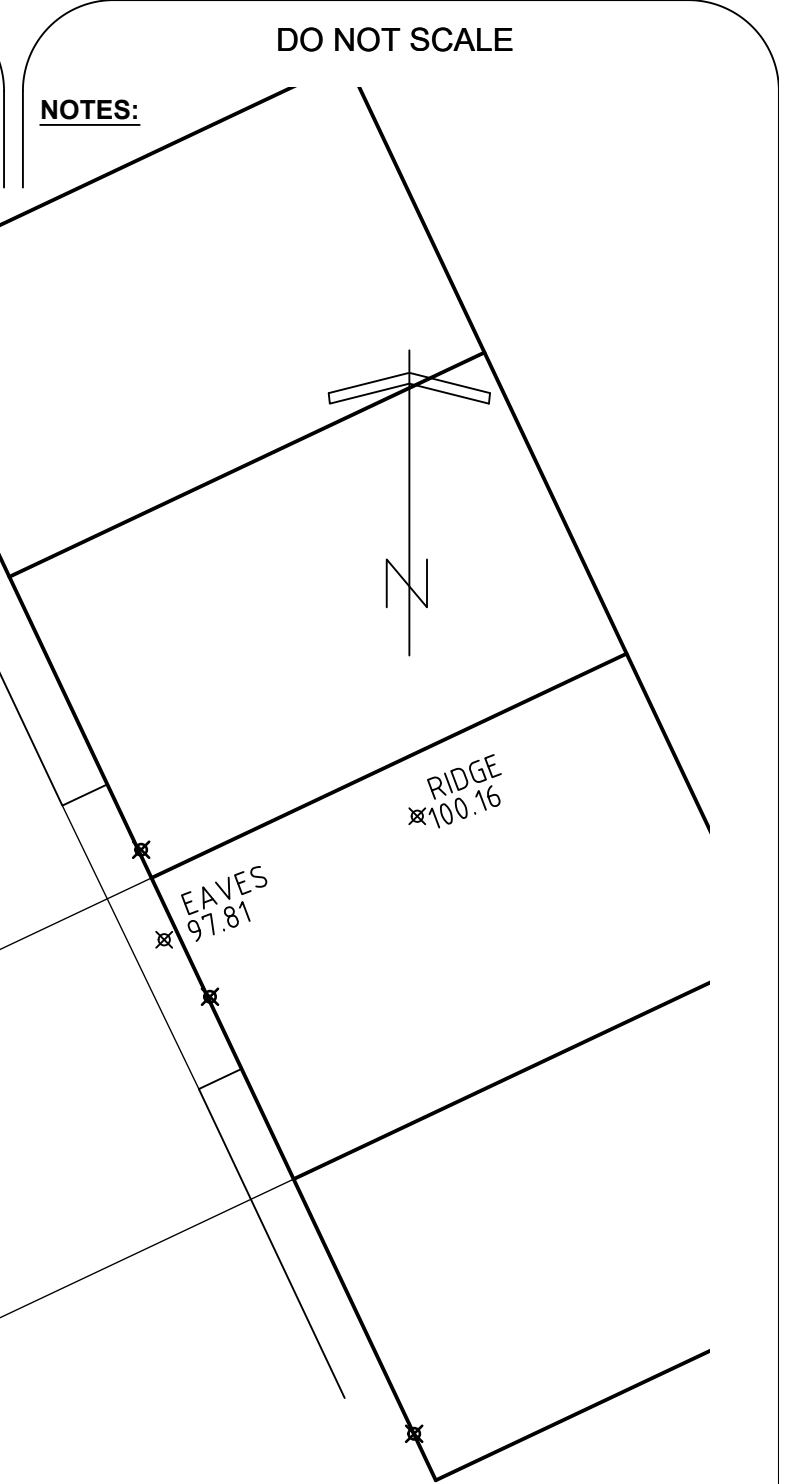
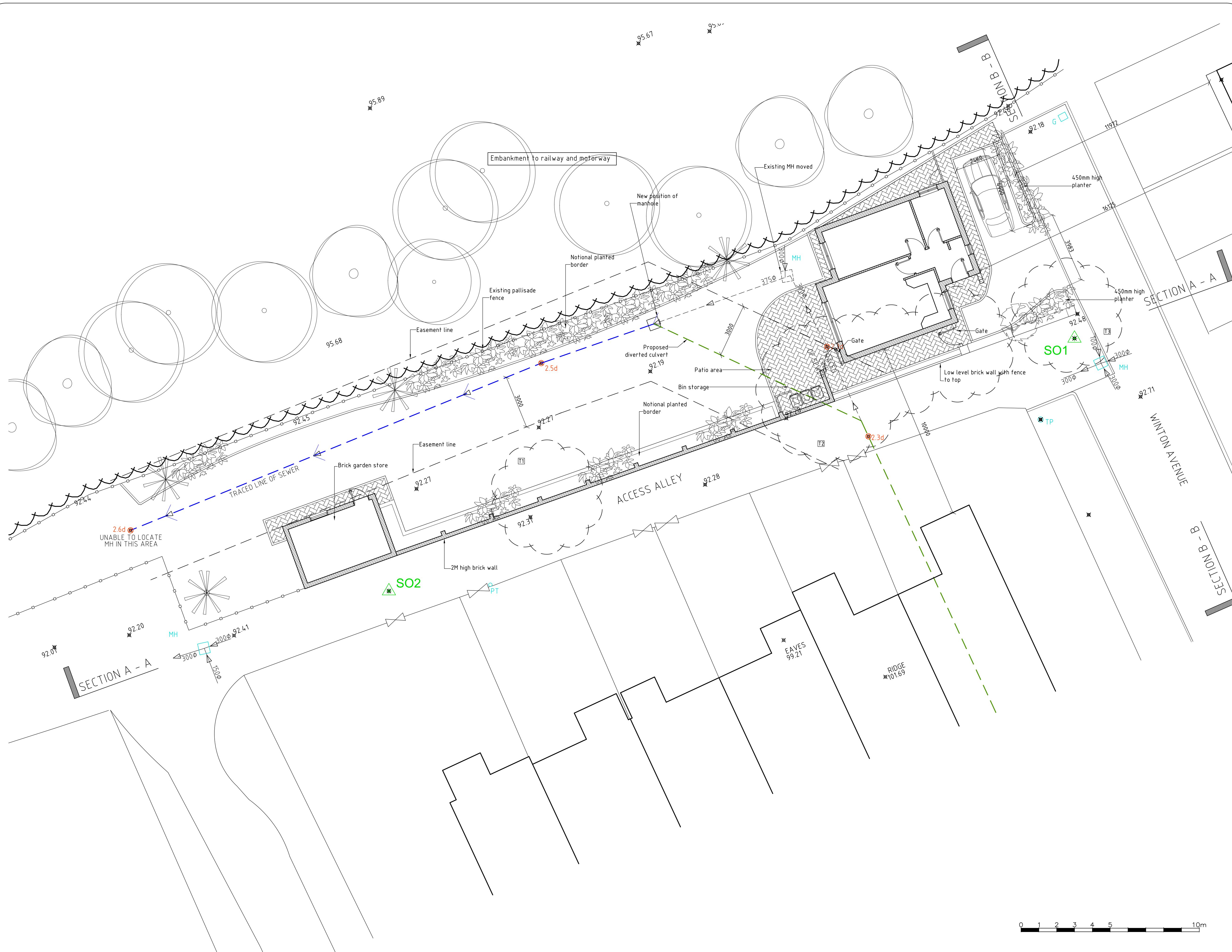
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Client: Mr James Ward
Project: Proposed new bungalow & garden store at land off Winton Avenue Audenshaw
Title: Proposed Plans and Elevations

Drawn	Date:	Scale (at A1)
SE	26/01/18	1/50
Orig no.	Revision No.	
1067 - 05	A	



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DO NOT SCALE

- NOTES:
- = Trees / bushes to be removed
 - Public sewer
 - Culvert

Rev	Date	Details Of Issue	Initials
A	12/04/18	Garage removed. Garden store added.	SE

Status

PLANNING

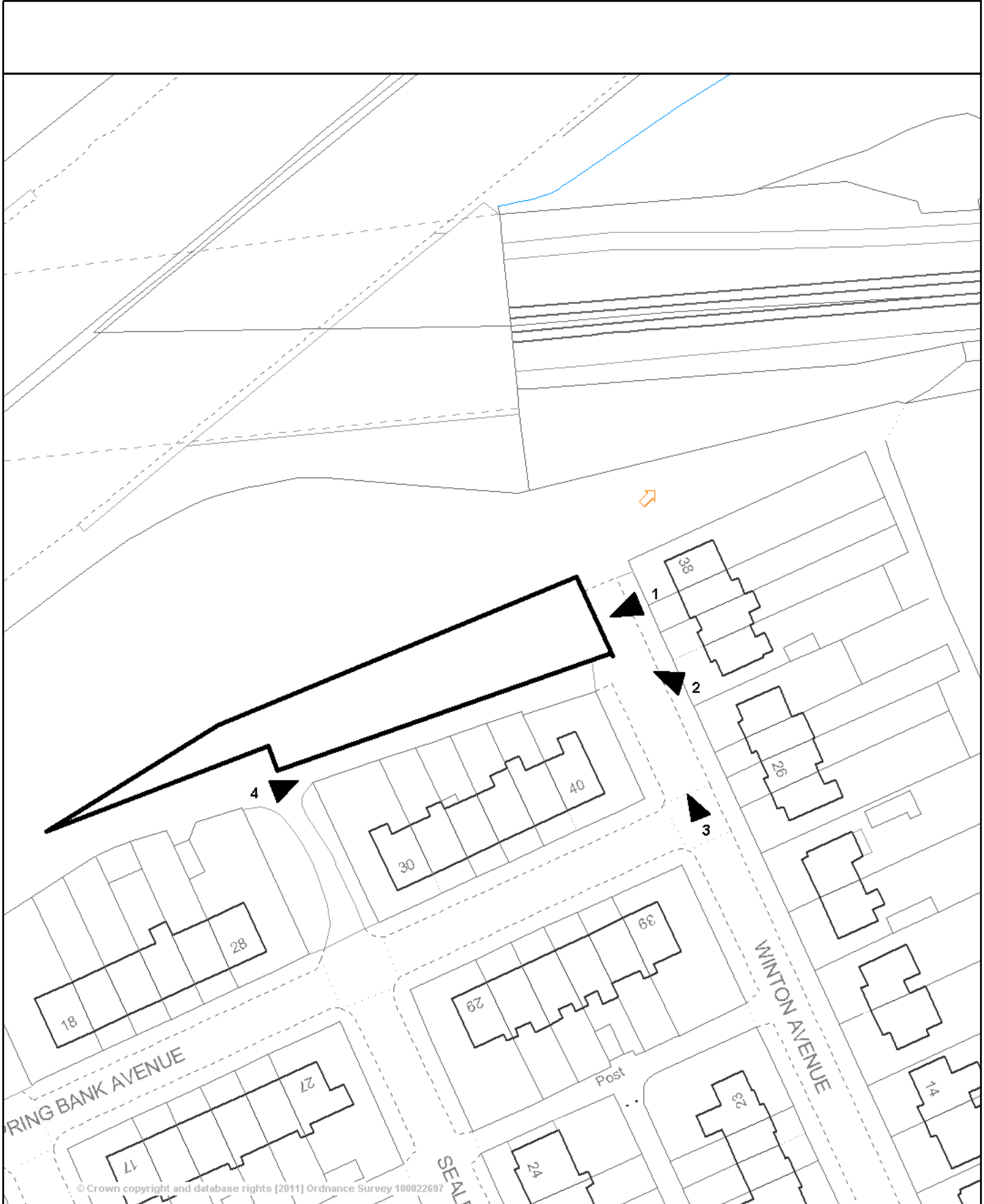
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Client	Mr James Ward 16 Appleton St, Awdenshaw
Project	Proposed New Bungalow Land off Winton Avenue, Awdenshaw
Title	Proposed Site Plan

Drawn	SE	Date:	26/01/18	Scale (at A1)	1/100
Drg no.	1067 03	Revision No.	A		

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Scale 1/750 Date 1/5/2018

Centre = 391633 E 397453 N

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